

DRAFT FOR CONSULTATION

# BAYS WEST URBAN DESIGN FRAMEWORK



IMAGE 0.1 Aerial view of Bays West from over Pyrmont / Brett Boardman  
Prepared by:

TERROIR

Prepared for :



Planning,  
Industry &  
Environment

**Issue:**  
FINAL EXHIBITION DRAFT  
MARCH 19, 2021



# Welcome to Country

The stretch of Country now known as Bays West has been known for millennia as **Gari Gurad/Nura** (Saltwater Country) and **Nattai Gurad/Nura** (Freshwater Country). This Country is celebrated for vast expanses of **garaban** (rock and sandstone) which in some places provides **gibbaragunya** (stone/cave shelters), and in other places creates **yiningmah** (steep cliffs) where ceremony can be performed privately without uninitiated onlookers.

For thousands of generations, local Aboriginal people have lived an abundant and sustainable lifestyle within a complex kinship system of numerous families and clans on this Country including the **D’harawal, Dharug, Eora, Gai-maragal, Gundangara and Guringai peoples**, among others. We pay our respects to their Ancestors and Elders past, present and emerging and acknowledge that through honouring Country, we also honour their timeless connections to Country.

It is also here on this Country that we recognise the changing and evolving nature of Country and the ways in which local communities and ecologies have responded and adapted to these changes throughout time. We acknowledge that Country is a living, breathing entity with an enduring **Duwee** or Spirit and it is this spirit that informs the work we undertake here today, and into the future.

## **Ngeeyinee bulima nandiritah**

(May you always see the beauty of this earth)

Welcome to Country provided by Shannon Foster

Government Registered Sydney Traditional Owner and D’harawal Knowledge Keeper

Within the *Bays West Draft Urban Design Framework* you will encounter stories of the Bays West location specifically. These are a small selection of the D’harawal stories of this place. They are shared by a contributor to this document, D’harawal Knowledge Keeper Shannon Foster, whose Ancestors kept these knowledges alive, and whose Elders and Knowledge Keepers still celebrate, live by and share them today.

The cultural Intellectual Property (IP) of all Aboriginal peoples, including the cultural IP of these stories, remains with the people they belong to and can never be vested or assigned. In this case the stories belong to the D’harawal people of the Sydney region who know themselves as Iyora here, and these stories may not be duplicated or used without the express permission of Sydney D’harawal Elders or Knowledge Keepers. The stories shared are just the starting point. There are other stories, and there are many layers of these stories that have not been unpacked in this document. There may also be other Ancestral stories of this location from other local peoples, and hearing them will involve the effort and time to learn in culturally appropriate ways.



# Table of Contents

Executive Summary	4
-------------------	---

## PART 1

### INTRODUCTION

1.1 Purpose	11
1.2 Relationships to other Frameworks	14
1.3 Vision and Directions	16
1.4 Document Structure	18

## PART 2

### PLACE PRINCIPLES

2.1 Structure Plan	21
2.2 Principles	26

## PART 3

### PRECINCT-WIDE FRAMEWORK

3.1 Introduction	56
3.2 Land Use and Function	58
3.3 Design of Places and Spaces	59
3.4 Transport & Movement	60
3.5 Heritage & Culture	61
3.6 Infrastructure Delivery & Governance	62
3.7 Design Excellence*	63

## PART 4

### SUB-PRECINCT FRAMEWORK

4.1 Sub-Precincts Overview	71
4.2 White Bay Power Station	74
4.3 Robert Street	102
4.4 Glebe Island Silos	105
4.5 Glebe Island Central	108
4.6 Glebe Island East	111
4.7 Rozelle Bay East	114
4.8 Rozelle Bay Central	117
4.9 Rozelle Bay West	120
4.10 White Bay	123
4.11 Rozelle Rail Yards	126

## PART 5

### IMPLEMENTATION

5.1 Implementation Actions & UDF Relationship	130
---	-----

## SUPPORTING FRAMEWORKS

S.1 Connecting with Country Framework (Authored by Bangawarra)	136 187
S.2 Sustainability Framework (Authored by Atelier Ten & Integral Group)	

## APPENDICES

A.1 Viewsheds	252
A.2 Public Access Parameters	268

We acknowledge the Traditional Custodians of the Country we work on and travel through and we pay our respects to Elders past, present and emerging.

We respect Country and seek to hear what she is telling us. We thank all those who have shared their knowledge and wisdom with us on this journey.

#### COPYRIGHT & CONFIDENTIALITY

The document is copyright of Terroir Pty Ltd and is not to be copied in any form without the written permission of Department of Planning, Industry & Environment NSW.

It is not to be used for any purpose other than that intended by the issuing party. The contents of this document are not to be conveyed to any person or organisation other than the person to whom it has been issued. This document is subject to controlled circulation. It is not to be circulated to any person or organisation other than the person it has been issued to without the written permission of Department of Planning, Industry & Environment NSW.

#### LIMITATIONS STATEMENT

In preparing this document, Terroir Pty Ltd and its team have relied upon information provided by employees of the various NSW Government departments and agencies and their consultants. Except as otherwise stated in this document, Terroir has not been able to independently verify the accuracy or completeness of any such information.

No warranty or guarantee, whether expressed or implied, is made with respect to the data reported or to the findings, observations, and conclusions expressed in this document.

This document has been prepared on behalf of and for the exclusive use of the Department of Planning, Industry & Environment NSW and is issued in connection with the provisions of the contract between Terroir Pty Ltd and Department of Planning, Industry & Environment NSW. Terroir accepts no liability or responsibility whatsoever for or in respect to any use of or reliance upon this report by any third party.

#### ACKNOWLEDGEMENTS

The work included within this report has built upon previous work on Bays West which was undertaken in collaboration with larger teams of consultants working with Terroir Pty Ltd as the head consultant. We gratefully acknowledge the inputs into the work presented within this report from the consultants noted below and the broader government and project teams.

Terroir gratefully acknowledges the new contributions from Bangawarra and Atelier Ten/Integral Group on the supporting Frameworks associated with this document. Terroir thanks our key team of collaborators on the Bays West Strategic Urban Design Report: Aspect Studios, SJB, Design5 Architects, Arup, Sleth, Latz+Partners and Egbert Stolk.

Terroir also thanks the team from Design 5 Architects and McGregorCoxall that assisted Terroir with the White Bay Power Station Urban Design Framework which included some of the initial concepts which informed the expanded Bays West approaches.

Date	Filepath	Prep By	Rev By	TERROIR Approv.	Client Approv.	Rev. no
19.03.2021	18303\3.07	FS/KA	TD	TD	AM	DRAFT FOR EXHIBITION v1.0

Prepared by :

**TERROIR**

Prepared for :



**Planning,  
Industry &  
Environment**

\* Section 3.7 Design Excellence has been prepared by Government Architect New South Wales



# Executive Summary

## INTRODUCTION

Bays West represents one of the last strategic piece of inner harbour land available for urban renewal in Sydney. Bays West also represents Sydney Harbour's last deep water and land interface, essential for port and working harbour functions. It constitutes renewal of a scale, time frame, and complexity well beyond that of a typical urban precinct. At 77 hectares, the Precinct encompasses White Bay, Glebe Island, Rozelle Bay, White Bay Power Station and Rozelle Rail Yards, along with 76 hectares of adjacent harbour waterways.

Bays West has a rich and layered history with deep songlines and ancestral connections to Country. It features an incredible piece of Sydney Harbour foreshore that is currently inaccessible, but has enormous potential for public access and engagement. The charming and diverse neighbouring suburbs, an authentic identity as a working harbour, and valued heritage and natural assets, make Bays West a unique place for Sydney and NSW.

The NSW Government *The Bays Precinct Urban Transformation Plan* set a vision for The Bays in 2015, founded on extensive community consultation through *The Bays Precinct, Sydney: Transformation Plan* – to make the area an iconic Sydney destination that would help drive an internationally competitive economy and deliver great outcomes for existing and new communities, to the benefit of Greater Sydney and NSW. The Bays West *Draft Place Strategy* builds on this ambition, and establishes a framework for government to deliver a landmark new piece of Sydney - a benchmark for responsive, sustainable, culturally enriched urban renewal.

## WHAT IS AN URBAN DESIGN FRAMEWORK?

An Urban Design Framework is a detailed document that provides a foundation for the future masterplanning of a site. It combines strategic directions and place-based responses, with an understanding of spatial implications

and delivery. An Urban Design Framework considers stakeholder and community collaboration, local character and identity, scenario testing informed by spatial analysis, proposed layout, built form and staging, and can be informed by a range of other documents and inputs. The Bays West *Urban Design Framework* will guide the masterplanning and rezoning process at Bays West and provide direction for ongoing urban renewal. It will inform the density, amenity and urban morphology of the precinct in a holistic and integrated way.

## PURPOSE OF THE URBAN DESIGN FRAMEWORK

The Bays West *Draft Urban Design Framework* supports the Bays West *Draft Place Strategy* and provides further detail on the delivery of the vision and directions for Bays West. The *Draft Urban Design Framework* expands upon the government position articulated in the Bays West *Draft Place Strategy* and establishes a series of principles and site-specific performance considerations to guide the masterplanning process and development of the planning framework for sub-precincts within Bays West. The *Draft Urban Design Framework* highlights opportunities for maximised public benefit whilst balancing the complex arrangement of land use, design, transport, heritage and governance.

The *Draft Urban Design Framework* serves a critical role, to bridge the high-level vision and directions in the *Draft Place Strategy* and *Draft Strategic Place Framework*, and the development of future masterplans. It will guide and inform the masterplanning and rezoning of the Precinct in an integrated way, enabling early stages of development and activation in concert with the delivery of the new metro station at Bays West.

The *Draft Urban Design Framework* is a 'live' document that will evolve and expand as further studies, investigations and masterplans are

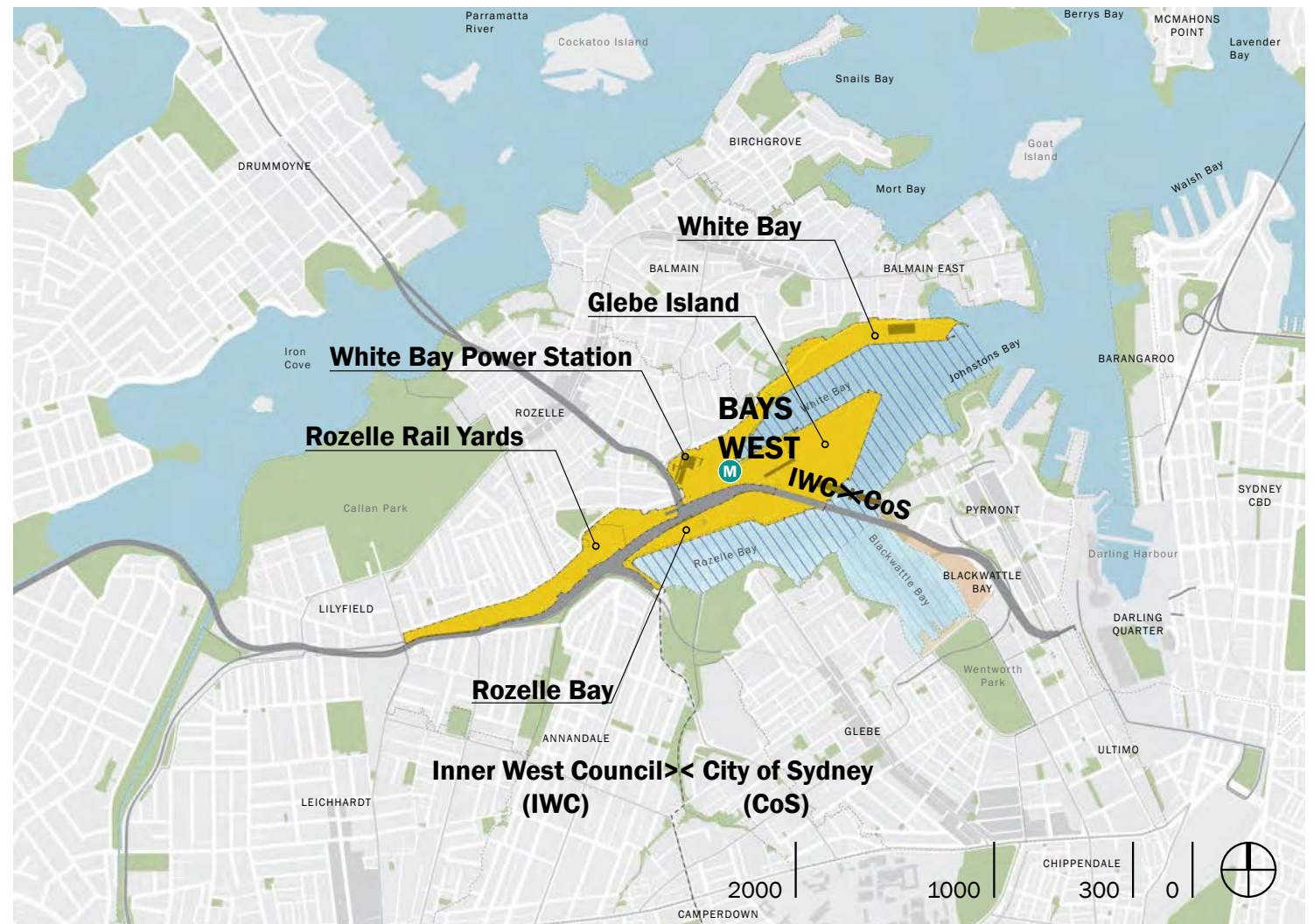


DIAGRAM 0.1 Bays West precinct location and extents

developed for the sub-precincts. Given that the White Bay Power Station (and Metro) sub-precinct is the initial stage for the precinct, greater detail and specific performance considerations are provided. The *Draft Urban Design Framework* will continue to guide and inform future built outcomes across the Precinct in line with the vision and directions, and be updated to reflect changes in strategic planning and policy positions.

## RELATIONSHIP TO OTHER DOCUMENTS

The Bays West *Draft Urban Design Framework* builds on the vision and directions of the *Draft Place Strategy* and *Draft Strategic Framework*, and provides a more detailed set of principles and criteria to guide future development. The *Draft Urban Design Framework* will continue to evolve with the *Place Strategy* as actions

are implemented and initial sub-precinct masterplans are developed.

The *Draft Connecting with Country Framework* and *Draft Sustainability Framework* have been developed as companion documents which have informed both of the frameworks and the Bays West *Draft Place Strategy*. These documents outline key considerations and site-specific opportunities across all future phases of the Precinct's masterplanning and redevelopment. The high-level objectives and key initiatives of both documents have been integrated into the *Draft Strategic Place Framework* and *Draft Place Strategy*. These documents are integrated within the Bays West *Draft Urban Design Framework*.



# Executive Summary

**VISION**

Building on the place analysis and consultation work, the vision outlines the NSW Government’s aspirations for the future of Bays West.

**THEMES & DIRECTIONS**

To support the vision for Bays West, 14 Directions have been established which are grouped into five Themes. These will guide future masterplanning and development processes across the Precinct.

**VISION FOR PLACE**

*Bays West will represent a new kind of Sydney urbanism that respects and celebrates **Country**.*

*It will build on its **natural, cultural, maritime and industrial stories** to shape an **innovative and sustainable** new place for **living, recreation and working**.*

*New **activities, places, connections and destinations** will enrich Bays West’s **character** and meaning over time through **built form and public spaces** that embrace its **natural and cultural heritage**.*

**LAND USE  
& FUNCTION**



DIAGRAM 0.2 Bays West land use and function factors

**Directions**

1. Deliver diverse employment spaces that can support knowledge intensive industries, which are a key contributor to the success of the innovation corridor
2. Deliver a range of housing, including affordable housing, to support the jobs created in the precinct and the ongoing growth of the Eastern Harbour City and metropolitan Sydney
3. Retain, manage and allow the essential strategic port and maritime industry uses to grow and evolve, to ensure they continue to support the NSW economy

**DESIGN OF  
PLACES & SPACES**

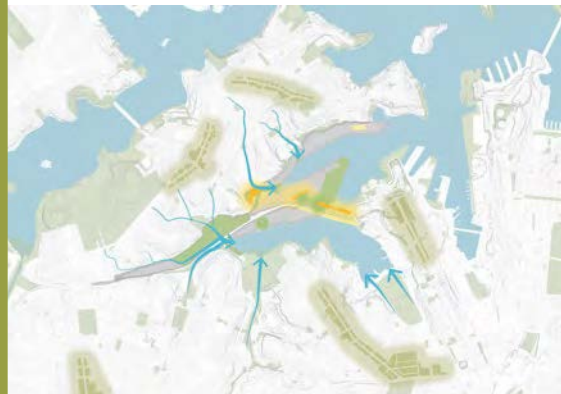


DIAGRAM 0.3 Bays West design of places and spaces factors

**Directions**

4. A key focus of the precinct is the design of open space and social infrastructure, ensuring careful integration with the natural, industrial, maritime and cultural heritage
5. Promote design excellence and embed a people-focused approach to deliver high quality and diverse built form and amenity outcomes
6. Promote biodiversity and improve water quality in the harbour whilst restoring and expanding the green and blue natural systems
7. Deliver a world class sustainable precinct which is carbon neutral and delivers efficient management of energy and water, and the elimination of waste

**TRANSPORT  
& MOVEMENT**



DIAGRAM 0.4 Bays West transport and movement factors

**Directions**

8. Improve the precinct’s connectivity and integration into its locality and surrounding areas
9. Provide for new connections to existing places by removing existing barriers to allow connections through the site and convenient access to the new Metro station
10. Prioritise walking, cycling and public transport by capitalising on the new Metro station, creating more convenient and direct active transport connections and investigate the reinstatement of a crossing from Bays West to Pyrmont

**HERITAGE  
& CULTURE**



DIAGRAM 0.5 Bays West heritage and culture factors

**Directions**

11. Bring new life to existing diverse assets and uses, integrating rich layers of creativity, heritage and culture across the precinct
12. Ensure that future developments recognise, embrace and create opportunities for deeper understanding of our culture and stories

**INFRASTRUCTURE  
DELIVERY & GOVERNANCE**



DIAGRAM 0.6 Bays West infrastructure delivery and governance factors

**Directions**

13. Use a whole-of-government approach to deliver strong and coordinated place outcomes for Bays West over time
14. Provide services and infrastructure to support the needs of the existing and future community of Bays West and its surrounds as it grows over time



# Executive Summary

## PART 2: STRUCTURE PLAN

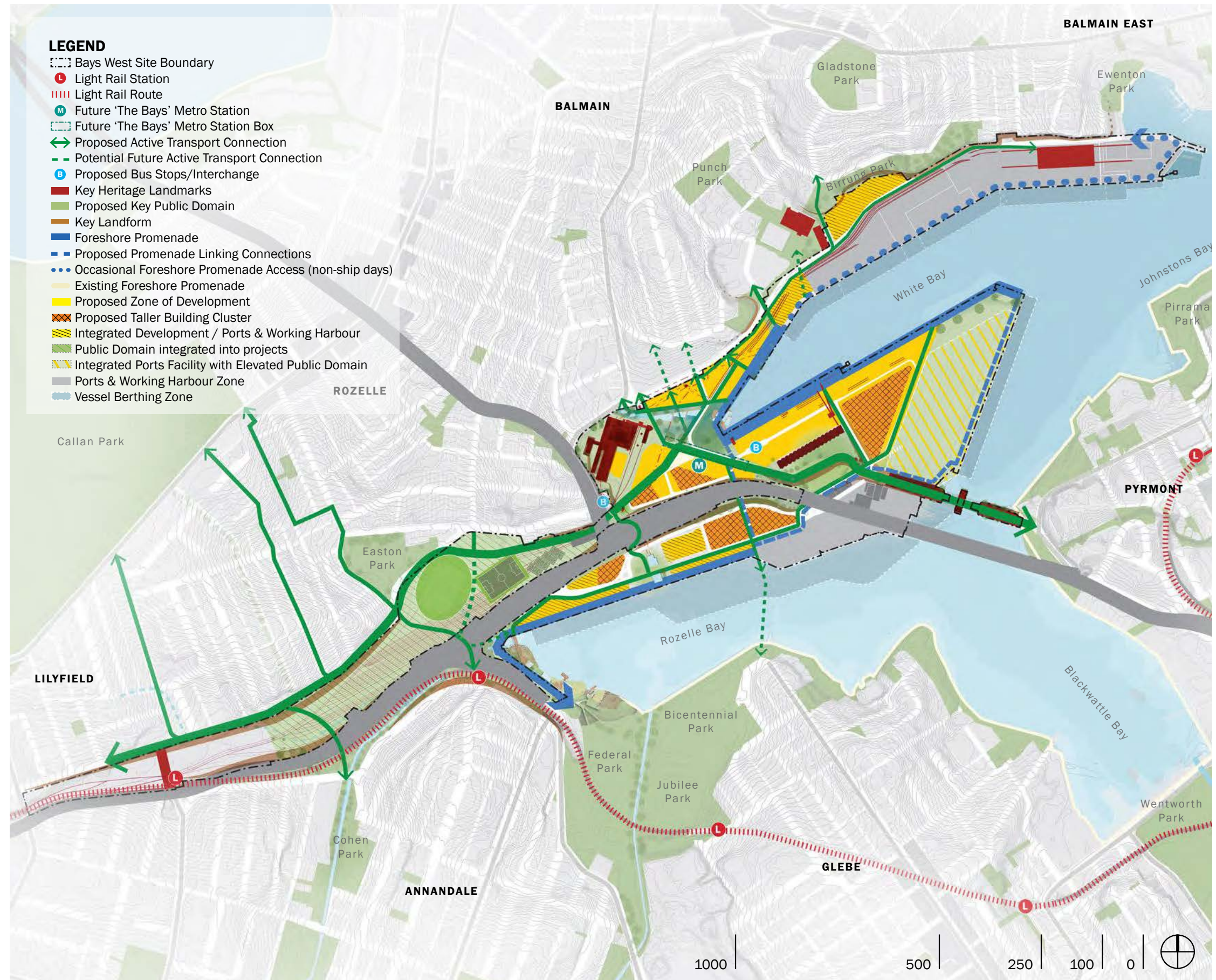
The Bays West Structure Plan sets out an overarching and integrated framework for the future of Bays West. It illustrates the primary land use, open space, and access and movement layout that have been developed for the precinct to capitalise on its existing and desired future place character and support its long-term renewal.

The structure plan represents the aspirational end-state of the Precinct's urban renewal. It is a broader framework for the precinct which will be explored and tested by ongoing and further detailed investigations and engagement with stakeholders.

Embedded within the Structure Plan are elements which respond to the strategies identified in the *Bays West Draft Connecting with Country Framework* which include:

- Caring for Stories is Caring for the Spirit of Country
- Minimise Damage to Country
- Co-Existing on Country
- Adaptation to Climate Change
- Care for Saltwater Country
- Care for Freshwater Country
- Care for Tucoerah/Tuggorah Country
- Creating Connections to Water
- Care for Complete Ecologies
- Care for all Remnants and Layers of Heritage
- Care for Sandstone Remnants
- Connecting up Country
- Country Appropriate Infrastructure
- Opportunities on Country
- Visual Connections to Country
- Cultural Expressions of Country
- Retained Connection to Country

Utilising this perspectives on these within the *Bays West Connecting with Country Framework* there are opportunities available to extend these approaches into the more detailed phases of design and delivery.





# Executive Summary

## PART 2: PRINCIPLES

The principles support the vision and directions in the Bays West *Draft Place Strategy*, and *Draft Strategic Place Framework*. They follow the five themes under which the directions have been classified to provide a clear linkages across the document. They have also been developed to support the Big Moves that the NSW Government has outlined in the *Draft Place Strategy*.

The Principles describe specific outcomes for Bays West, to enable a high amenity and place focused Precinct. These provide more specific guidance for the future planning and development of Bays West.

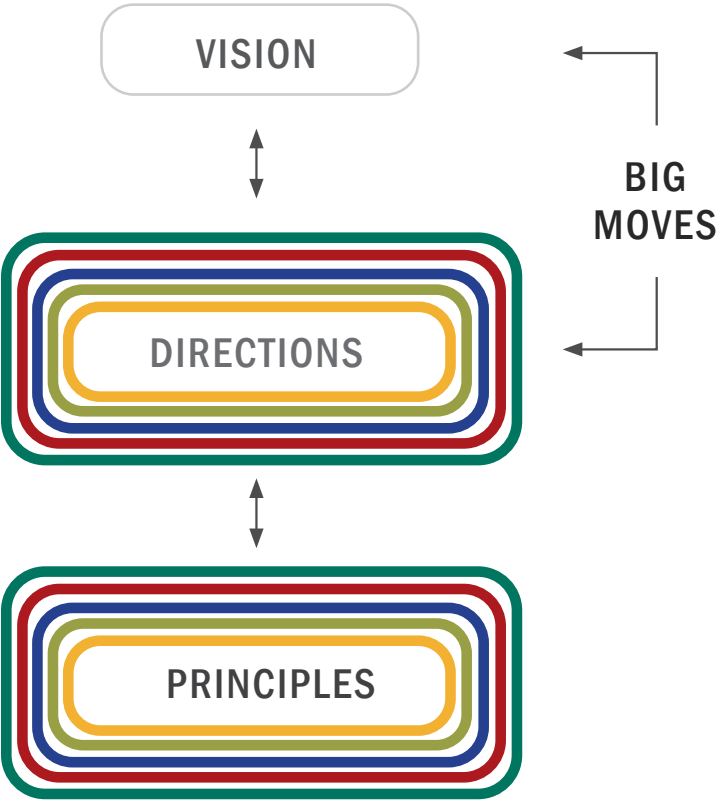


DIAGRAM 0.8 Relationship between Principles and Vision, Directions and Big Moves

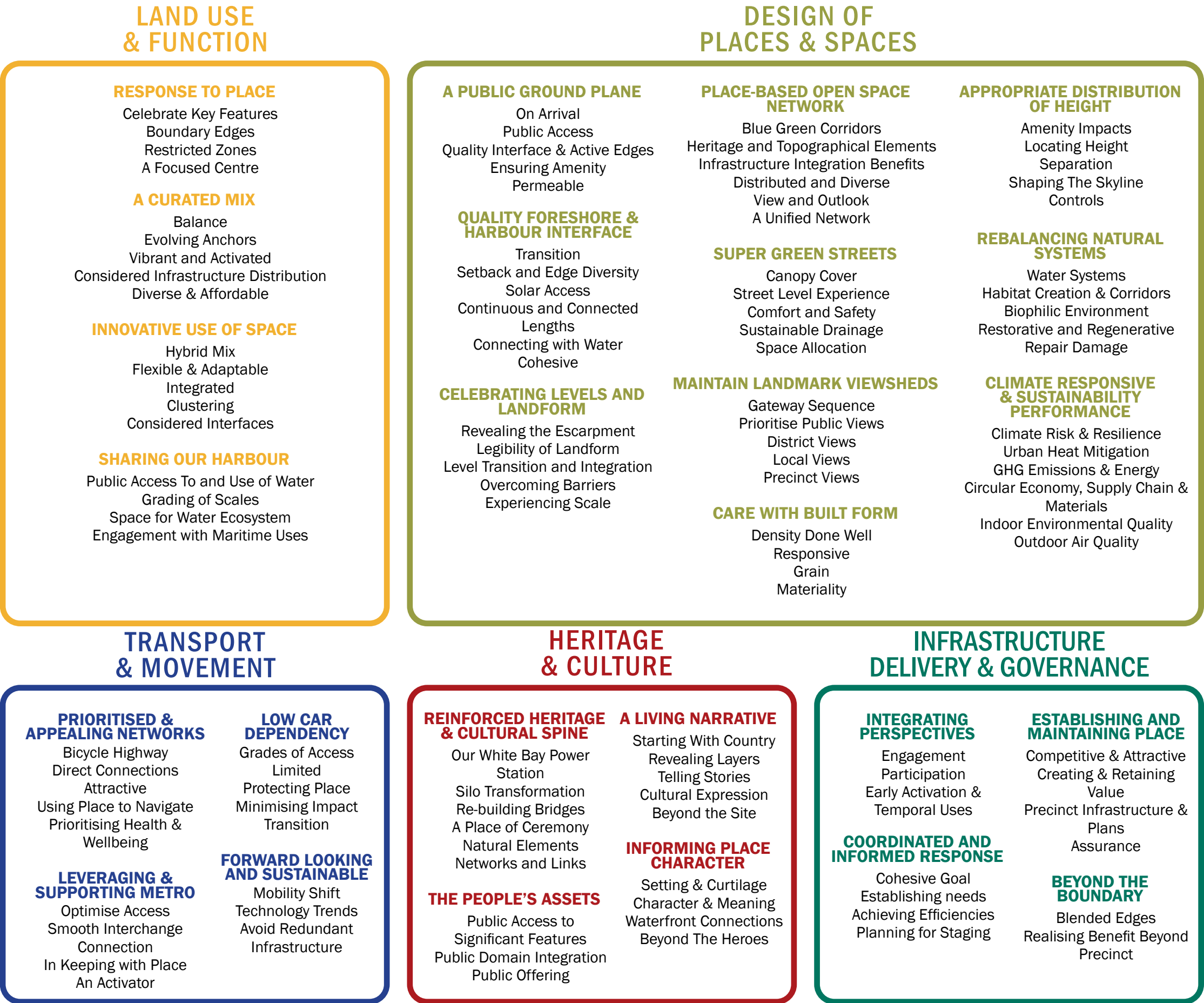


DIAGRAM 0.9 Bays West Principles



# Executive Summary

**PART 3: PRECINCT-WIDE FRAMEWORK**

This section will be completed as further detailed investigations into the precinct are undertaken including studies identified in the Bays West Precinct Wide Actions.

**DESIGN EXCELLENCE**

The Bays West Design Excellence Strategy outlines the key design processes, design related activities and policy context that will guide and promote design quality for Bays West. As part of the on-going collaboration between Government Architect NSW and the Department of Planning, Industry and Environment, the Strategy guides NSW Government agencies involved in the precinct’s development and delivery with an overview of the requirements and considerations for attaining design excellence. The Strategy is forward facing, and process-focused, setting expectations for design quality and providing guidance for the on-going development of future detailed design excellence strategies for individual projects and future statutory Design Excellence provisions under the relevant Environmental Planning Instruments.

**PART 4: SUB-PRECINCT FRAMEWORK**

The Sub-Precinct Framework divides the site into 10 sub-precincts to assist in defining site specific design parameters and performance measures. The division of the Precinct is based primarily on existing and future character zones, and is not representative of priority or staging. Future masterplanning and rezoning applications could encompass part of or multiple sub-precincts.

The 10 Sub-Precincts established for Bays West are:

- 1. White Bay Power Station (and Metro)
- 2. Robert Street
- 3. Glebe Island Silos
- 4. Glebe Island Central
- 5. Glebe Island East
- 6. Rozelle Bay East
- 7. Rozelle Bay Central
- 8. Rozelle Bay West
- 9. White Bay
- 10. Rozelle Rail Yards

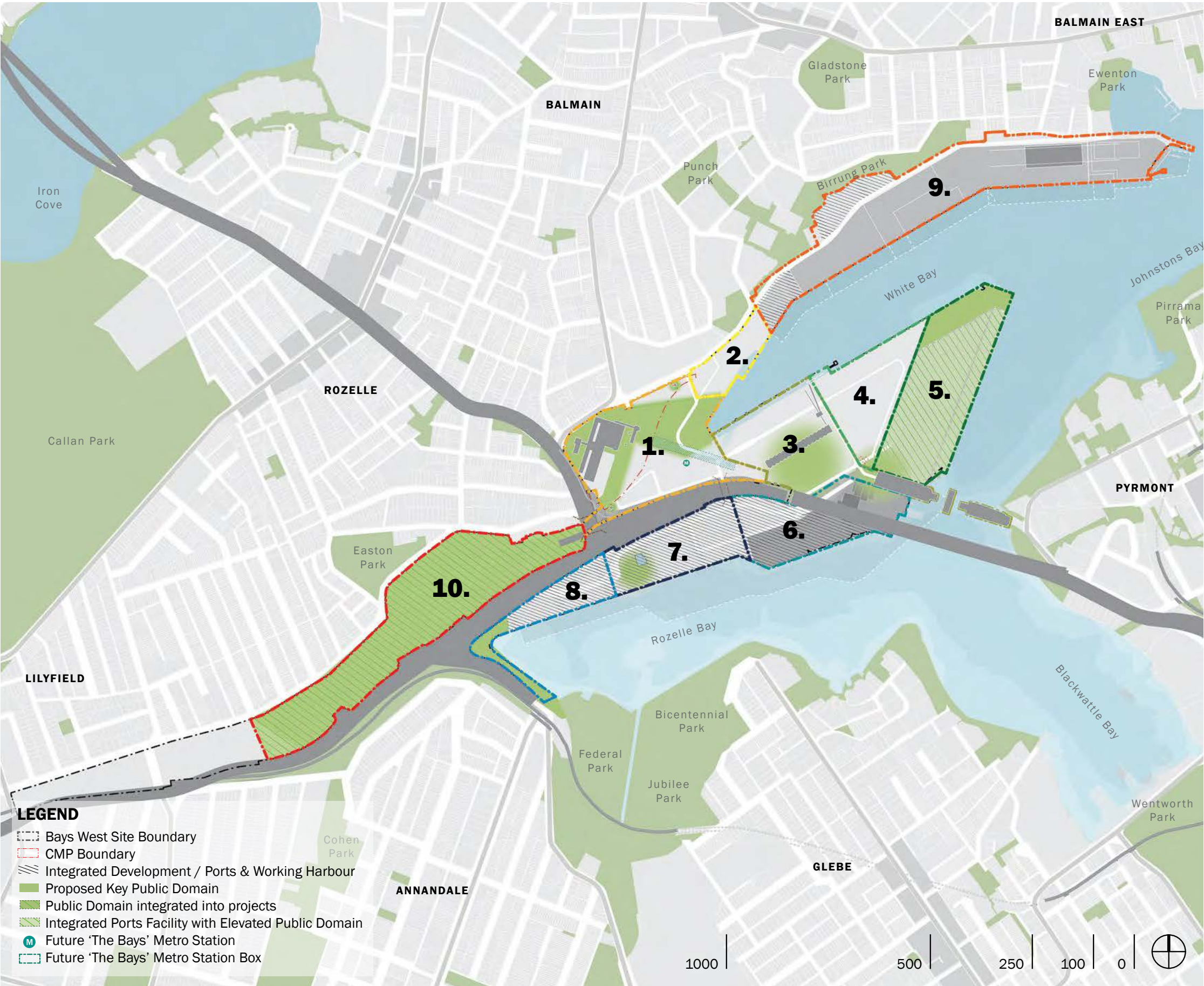


DIAGRAM 0.10 Bays West sub-precincts



# Executive Summary

**PART 4: SUB-PRECINCT FRAMEWORK (CONT.)**

Each of the sub-precincts have been explored in terms of their existing condition and the aspirational for their character in the future, with corresponding precedent images.

Additional detail is provided with regards to the White Bay Power Station (and Metro) sub-precinct against each of the Themes.

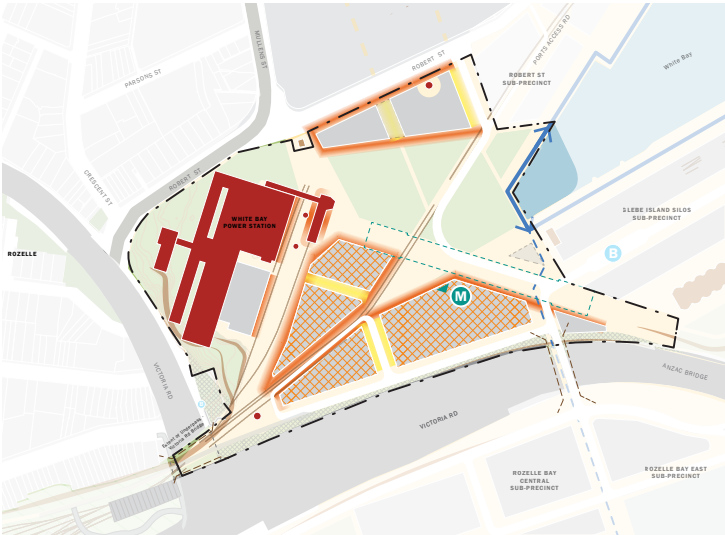


DIAGRAM 0.11 WBPS Sub-Precinct Land Use & Function Plan

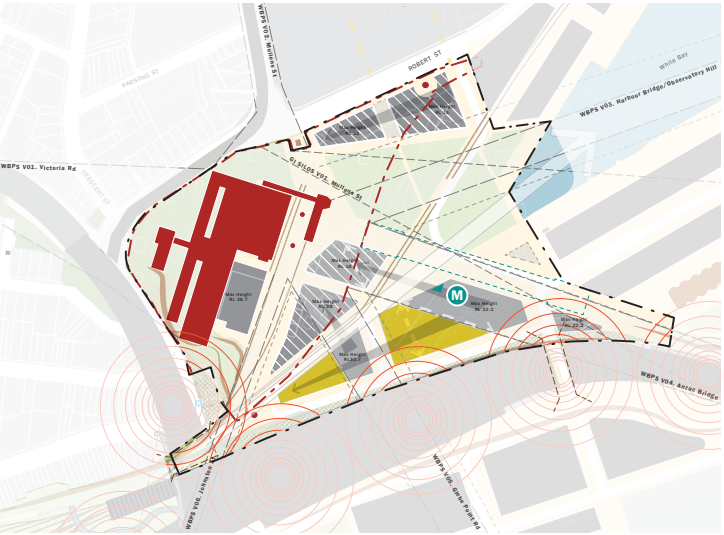


DIAGRAM 0.12 WBPS Sub-Precinct Built Form Plan

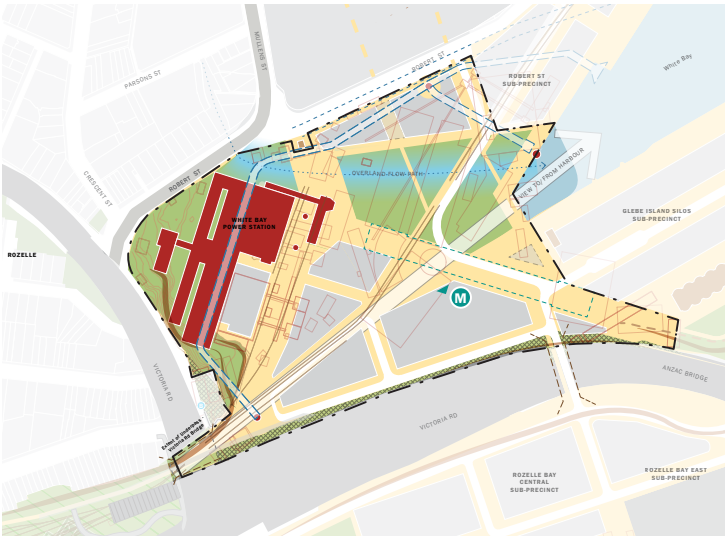


DIAGRAM 0.13 WBPS Sub-Precinct Public Domain Plan

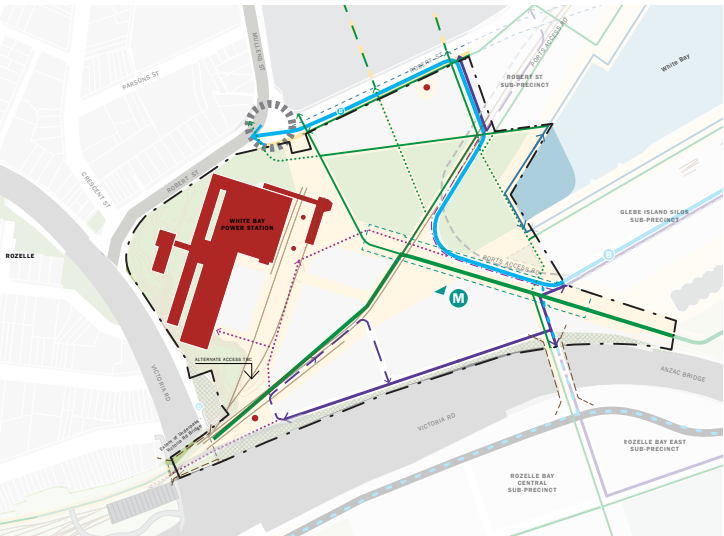


DIAGRAM 0.14 WBPS Sub-Precinct Transport & Movement Plan

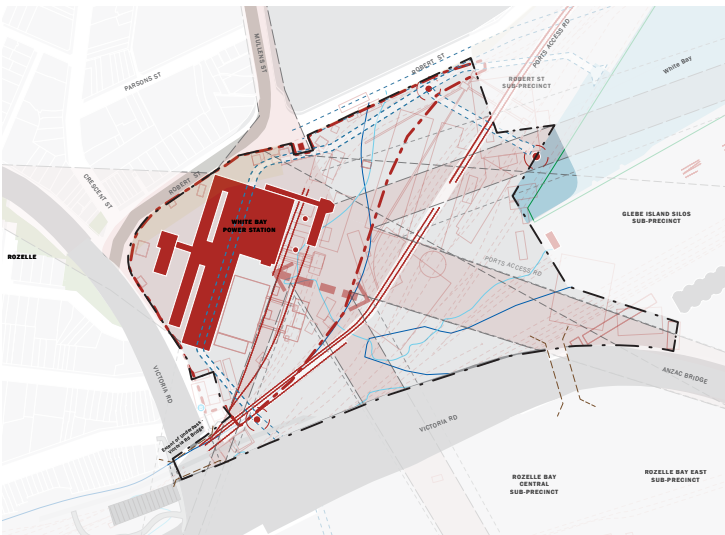


DIAGRAM 0.15 WBPS Sub-Precinct Heritage & Culture Plan

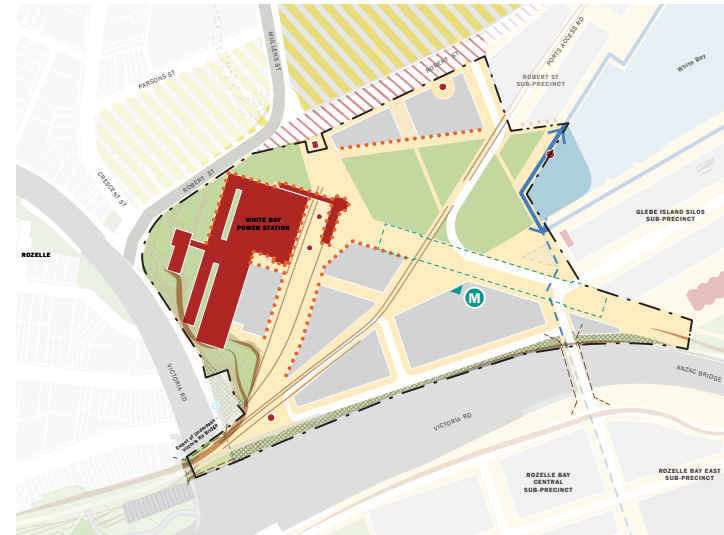


DIAGRAM 0.16 WBPS Sub-Precinct Infrastructure Delivery & Governance Plan

**PART 5: IMPLEMENTATION ACTIONS & UDF RELATIONSHIP**

The *Bays West Draft Place Strategy* has identified a number of Implementation Actions and initial staging considerations that are needed to facilitate the coordinated development of masterplans and associated rezoning of each stage. The *Bays West Draft Urban Design Framework* is a key part of this process and will be a ‘live’ document that will incorporate new information, further detailed studies and masterplanning details.

A suite of precinct-wide studies and strategies have been identified to ensure an integrated and coordinated approach to the long-term redevelopment of the Precinct. These will inform the staged masterplanning and rezoning of the Precinct. Where responsibility for major assets is already defined, feasibility studies and business case work will progress with regards to the future role of Glebe Island Bridge and the White Bay Power Station.

The first stage to undergo redevelopment will be the land around the new metro station. Sydney Metro and the DPIE will undertake masterplanning of the White Bay Power Station (and Metro) sub-precinct, which will inform the subsequent rezoning of the site and allow state-significant development applications to be lodged by future proponents. As information is received, the *Draft Urban Design Framework* will continue to be updated to ensure that the vision, directions and principles are relevant to the spatial outcomes that will shape and extraordinary place at Bays West.

The UDF will be updated regularly to integrate findings from the detailed studies identified in the Implementation actions. The anticipated updates triggered from the Implementation Actions into sections of the framework have been identified in detail,



DRAFT FOR CONSULTATION

# 1

## INTRODUCTION

IMAGE 1.1 Aerial View of Balmain and Rozelle with White Bay Power Station in foreground / DPIE



# 1.1

## PURPOSE



# Urban Design Framework Purpose

1.1 PURPOSE

BACKGROUND

The Department of Planning, Industry and Environment (DPIE) has been tasked with developing the Bays West *Draft Place Strategy* which sets the vision and place priorities for the Precinct. DPIE has drawn upon the earlier phases of work and community and stakeholder consultation undertaken by Infrastructure NSW (formerly Urban Growth Development Corporation). The *Bays West Draft Place Strategy* carries this work forward, leveraging the best outcome for NSW via large-scale renewal of the Precinct.

The Bays West *Draft Urban Design Framework* supports the Bays West *Draft Place Strategy* and provides further detail on the delivery of the vision and directions for Bays West. The *Draft Urban Design Framework* expands upon the government position articulated in the Bays West *Draft Place Strategy* and establishes a series of principles and site-specific performance considerations to guide the masterplanning process and development of the planning framework for sub-precincts within Bays West. The *Draft Urban Design Framework* highlights opportunities for maximised public benefit whilst balancing the complex arrangement of land use, design, transport, heritage and governance.

ROLE OF THE DOCUMENT

The Bays West *Draft Urban Design Framework* is a document that provides further detail on the directions in the Bays West *Draft Place Strategy* and the *Draft Strategic Place Framework*. It introduces a set of principles which describe tangible spatial and strategic outcomes at Bays West that future development will respond to. Based on the high-level vision and directions in the Bays West *Draft Place Strategy*, and the detailed analysis in the *Draft Strategic Place Framework*, it provides guidance for delivering the best outcomes for the Precinct, Sydney and the broader NSW economy. The *Draft Urban Design Framework* provides a set of spatial outcomes for the Precinct which will result in a highly integrated and cohesive urban design

response for the future of the site. It also draws upon the *Draft Connecting with Country Framework* and *Draft Sustainability Framework*, prescribing these as key considerations for place-based performance. The *Draft Urban Design Framework* serves a critical role, to bridge the high-level vision and directions in the *Draft Place Strategy* and *Draft Strategic Place Framework*, and the development of future masterplans. It will guide and inform the masterplanning and rezoning of the Precinct in an integrated way, enabling early stages of development and activation in concert with the delivery of the new metro station at Bays West.

The *Draft Urban Design Framework* is a ‘live’ document that will evolve and expand as further studies, investigations and masterplans are developed for the sub-precincts. The relationship between these documents will be iterative, noting that whilst the *Urban Design Framework* will guide these studies and masterplans, it will also evolve and be informed by them. In its current edition, the *Draft Urban Design Framework* provides principles for the broader precinct, an overview of the future character ambitions and significant place opportunities for each sub-precinct. Part 3 includes an indicative list of various parts which are expected to evolve over time as the precinct is delivered. As part of this process the landholders and the community will be consulted in greater detail and have an opportunity to input into the future development of each sub-precinct.

Given that the White Bay Power Station (and Metro) sub-precinct is the initial stage for the precinct, greater detail and specific performance considerations are provided. As the area of initial development, aligned with the delivery of Metro West, the detailed masterplan for this sub-precinct will be informed by the *Draft Urban Design Framework*. The *Draft Urban Design Framework* will continue to guide and inform future built outcomes across the Precinct in line with the vision and directions,

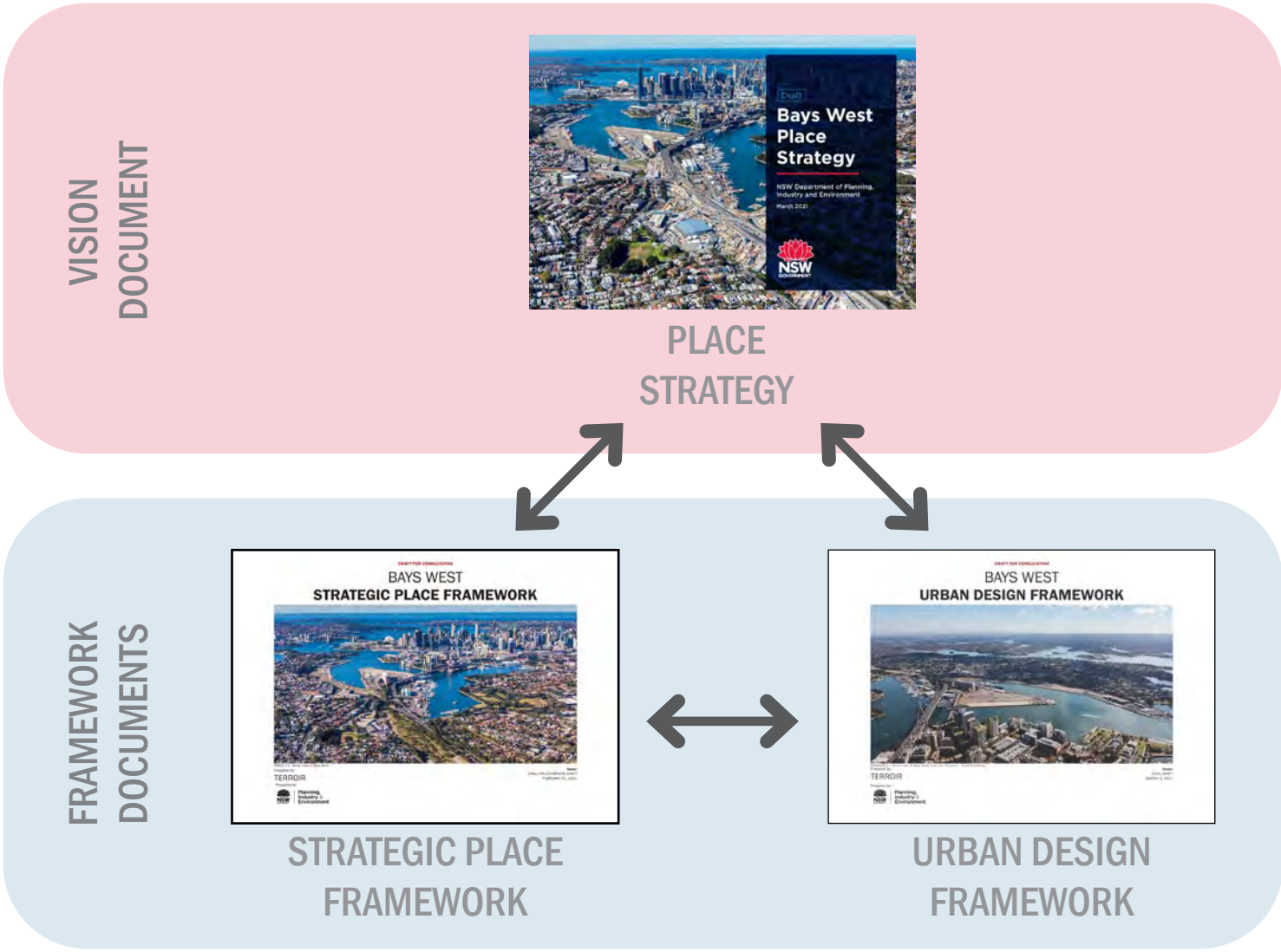


DIAGRAM 1.1 Relationship between Draft Place Strategy and Draft Framework Documents

and be updated to reflect changes in strategic planning and policy positions.

DOCUMENT OBJECTIVES

The Bays West *Draft Urban Design Framework*:

- translates strategic planning directions into spatial outcomes for the Precinct
- incorporates in the future the outcomes of initial sub-precinct masterplans, staging considerations and ongoing site investigations to update and refine the document as an iterative process
- integrates key community and stakeholder interests and concerns
- considers technical consultant inputs
- coordinates with other related projects
- provides clarity around realising the vision

as it relates to the site, including embedding shared values for urban quality and place

- allows for flexibility around how the vision is achieved over time
- establishes further detail on urban design parameters to deliver on the *Draft Place Strategy* and *Draft Strategic Place Framework*
- integrates findings of initial investigations, with the intention to include new inputs from subsequent



# Urban Design Framework Purpose

## 1.1 PURPOSE

### AN EVOLVING DOCUMENT

The Bays West *Draft Urban Design Framework* will be updated in response to ongoing inputs from the more detailed work that has been identified to be undertaken. The newer inputs which will inform the evolution of the framework include the precinct-wide strategies and studies and the masterplanning itself. The *Urban Design Framework* will be updated with these inputs to provide more detail for the following masterplans and be utilised as part of the rezoning process as it is progressively undertaken.

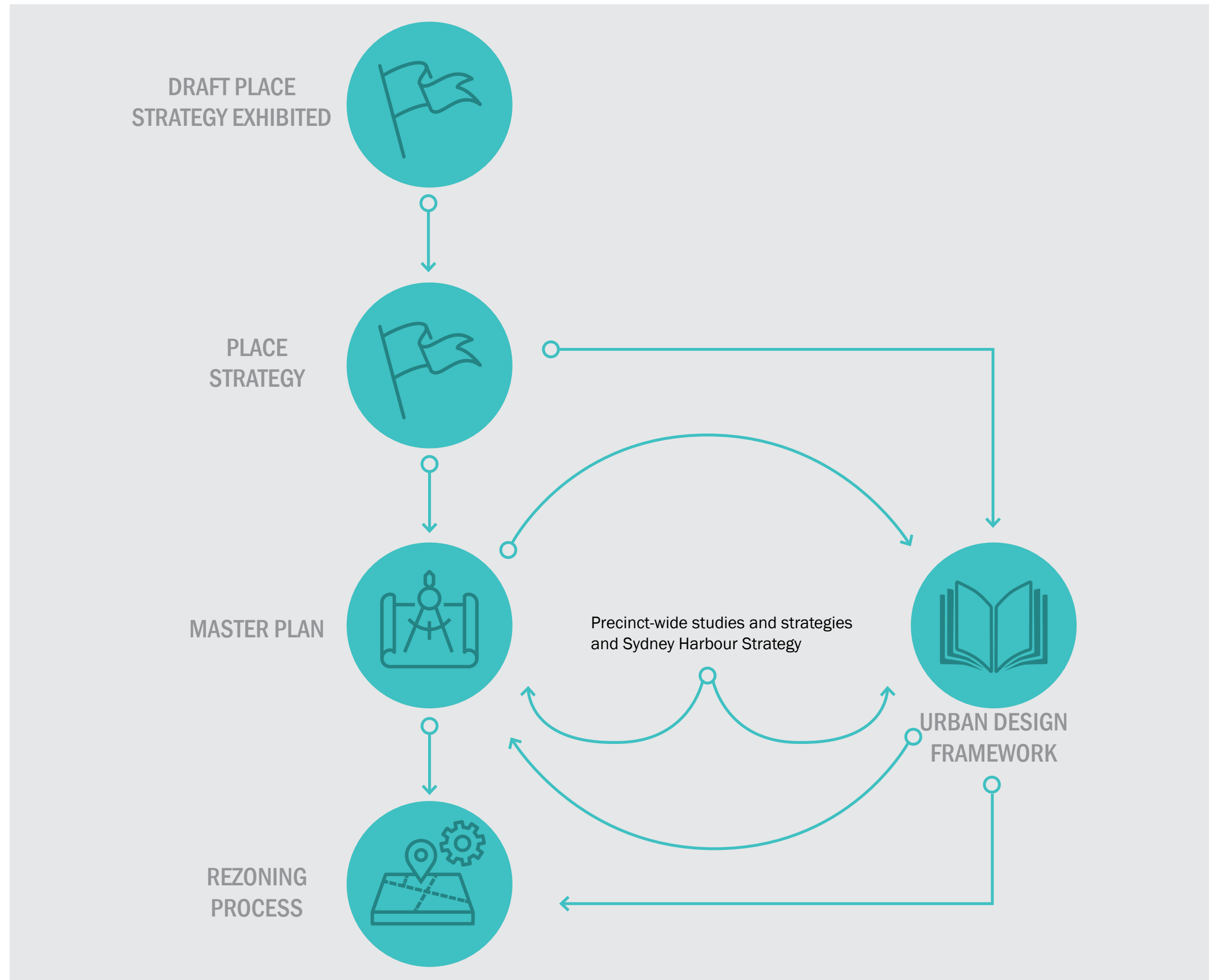


DIAGRAM 1.2 Implementation Flow Chart & Relationships to other documents



# 1.2

## RELATIONSHIP TO OTHER FRAMEWORKS



# Relationship to Other Frameworks

## 1.2 RELATIONSHIP TO OTHER FRAMEWORKS

The Bays West *Draft Urban Design Framework* forms one part in a coordinated set of framework documents which have been developed to inform the Bays West *Draft Place Strategy*. The *Draft Place Strategy* has been compiled by the Department of Planning, Industry and Environment, and exists as the overarching positioning document for the Precinct.

The *Draft Place Strategy* presents the NSW Government's vision and key directions for Bays West moving forward. The *Draft Strategic Place Framework* further explains the foundations for this approach. It frames the strategic policy context for the Precinct, and highlights the existing site structures and significant place attributes that have influenced its future direction and offer unique renewal opportunities that speak directly to its place narrative.

The *Draft Urban Design Framework* builds on the vision and directions of the *Draft Place Strategy* and *Draft Strategic Framework*, and provides a more detailed set of principles and criteria to guide future development. The *Draft Urban Design Framework* exists as a live document, which will continue to evolve with the *Place Strategy* as actions are implemented and initial sub-precinct masterplans are developed.

The *Draft Connecting with Country Framework* and *Draft Sustainability Framework* have been developed as companion documents which have informed both of the frameworks and the Bays West *Draft Place Strategy*. These documents outline key considerations and site-specific opportunities across all future phases of the Precinct's masterplanning and redevelopment. The high-level objectives and key initiatives of both documents have been integrated into the *Draft Strategic Place Framework* and *Draft Place Strategy*. These documents are integrated within the Bays West *Draft Urban Design Framework*.

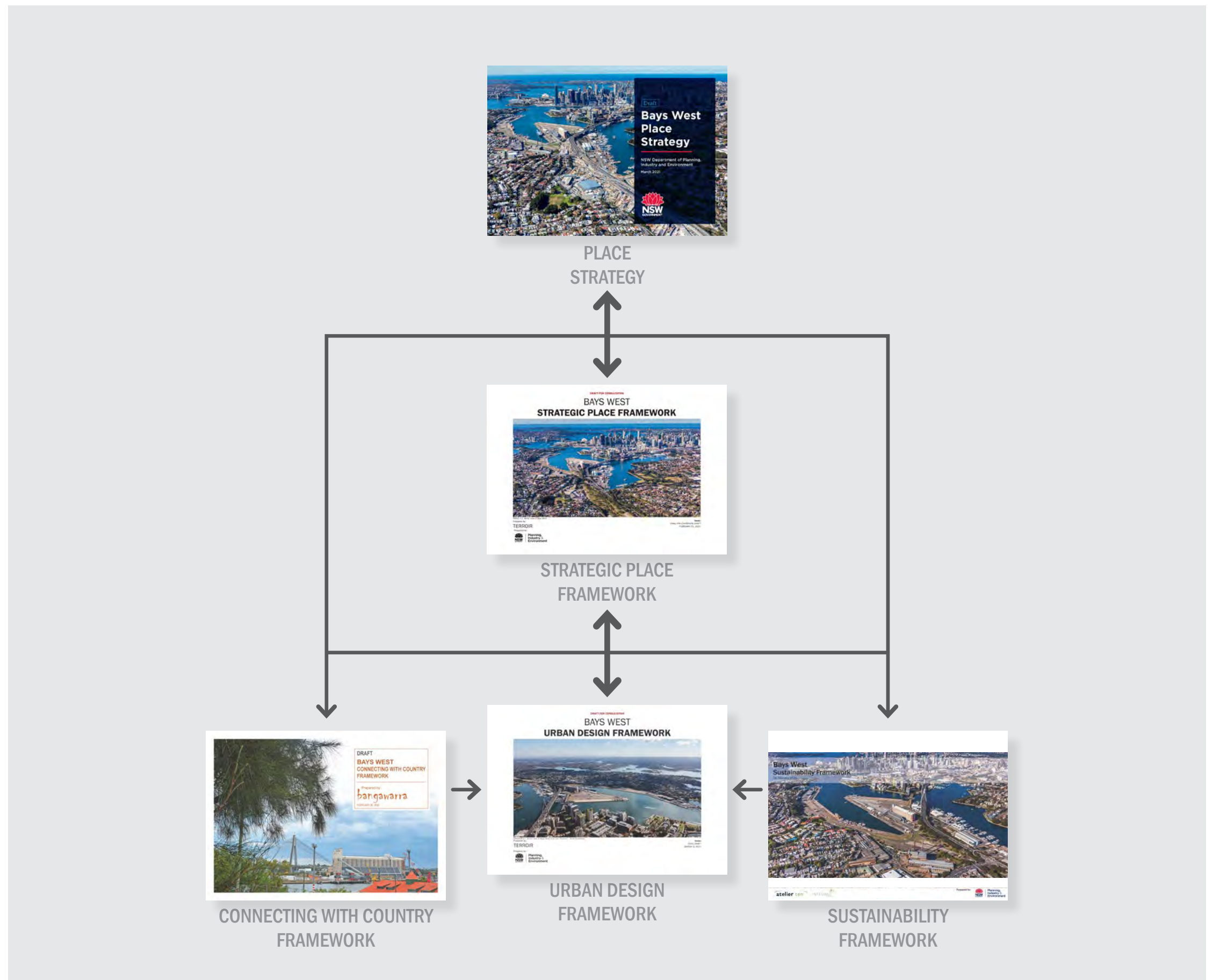


DIAGRAM 1.3 The relationship of the different framework documents that inform the Bays West Place Strategy



# 1.3

## VISION & DIRECTIONS



# Vision & Directions

**VISION FOR PLACE**

Bays West will represent a new kind of Sydney urbanism that respects and celebrates **Country**.

It will build on its **natural, cultural, maritime and industrial stories** to shape an **innovative and sustainable** new place for **living, recreation and working**.

New **activities, places, connections** and **destinations** will enrich Bays West’s **character** and meaning over time through **built form and public spaces** that embrace its **natural and cultural heritage**.

**DIRECTIONS**

The 14 directions that have been established for Bays West address connectivity, productivity, liveability and sustainability matters that will guide the growth and change of the precinct over time. The directions are grouped into five enabling themes.

These directions, together with the accompanying big moves and structure plan, establish how development and growth will occur in the Bays West precinct and transform its current use to deliver the vision for the area. The directions focus on five themes that ensure that the vision for Bays West is successfully delivered through the precinct’s renewal.

**LAND USE & FUNCTION**

Addresses the future land uses of Bays West and what role it will play in Sydney’s future.

**Directions**

- 1. Deliver diverse employment spaces that can support knowledge intensive industries, which are a key contributor to the success of the innovation corridor
- 2. Deliver a range of housing, including affordable housing, to support the jobs created in the precinct and the ongoing growth of the Eastern Harbour City and metropolitan Sydney
- 3. Retain, manage and allow the essential strategic port and maritime industry uses to grow and evolve, to ensure they continue to support the NSW economy

**DESIGN OF PLACES & SPACES**

Provides guidance on how Bays West will feel to people and what is important in the design of buildings, open spaces and public domain.

**Directions**

- 4. A key focus of the precinct is the design of open space and social infrastructure, ensuring careful integration with the natural, industrial, maritime and cultural heritage
- 5. Promote design excellence and embed a people-focused approach to deliver high quality and diverse built form and amenity outcomes
- 6. Promote biodiversity and improve water quality in the harbour whilst restoring and expanding the green and blue natural systems
- 7. Deliver a world class sustainable precinct which is carbon neutral and delivers efficient management of energy and water, and the elimination of waste

**TRANSPORT & MOVEMENT**

Acknowledges the constrained nature of Bays West and establish how the people and goods will move to/from and through the Precinct.

**Directions**

- 8. Improve the precinct’s connectivity and integration into its locality and surrounding areas
- 9. Provide for new connections to existing places by removing existing barriers to allow connections through the site and convenient access to the new Metro station
- 10. Prioritise walking, cycling and public transport by capitalising on the new Metro station , creating more convenient and direct active transport connections and investigate the reinstatement of a crossing from Bays West to Pyrmont

**HERITAGE & CULTURE**

Recognises the importance of the past and how understanding history and culture is critical to creating a place with meaning.

**Directions**

- 11. Bring new life to existing diverse assets and uses, integrating rich layers of creativity, heritage and culture across the precinct
- 12. Ensure that future developments recognise, embrace and create opportunities for deeper understanding of our culture and stories

**INFRASTRUCTURE DELIVERY & GOVERNANCE**

Acknowledges the precinct will evolve over time and that multiple stakeholders are required to ensure that Bays West is successfully delivered.

**Directions**

- 13. Use a whole-of-government approach to deliver strong and coordinated place outcomes for Bays West over time
- 14. Provide services and infrastructure to support the needs of the existing and future community of Bays West and its surrounds as it grows over time



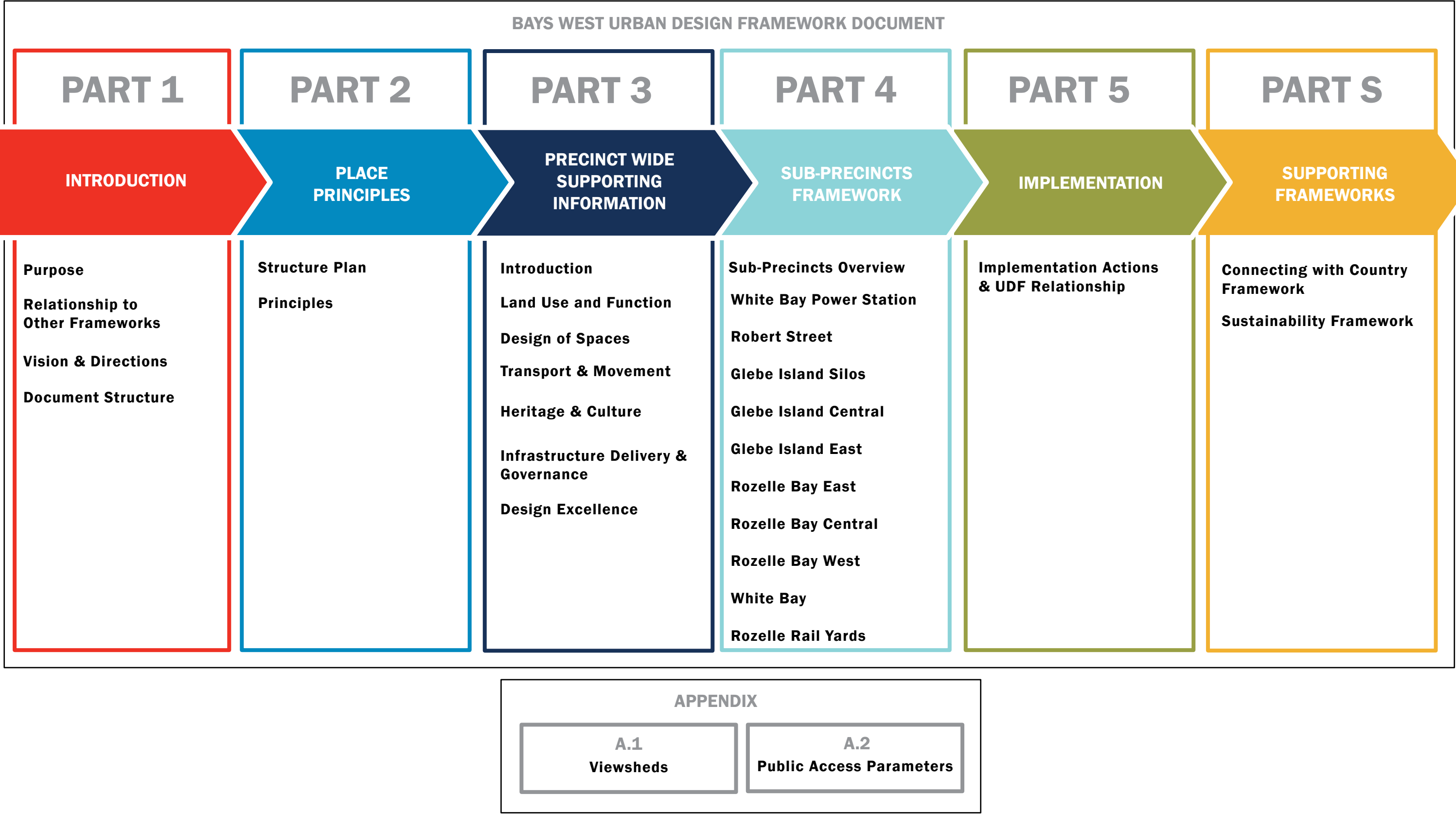
# 1.4

## DOCUMENT STRUCTURE



# Urban Design Framework Structure

The *Draft Urban Design Framework* document is structured into five parts as outlined below. Included within this document are the supporting *Draft Connecting with Country Framework* and *Draft Sustainability Framework*





# 2

## PLACE PRINCIPLES

IMAGE 2.1 View of White Bay Power Station from top of Glebe Island Silos / Port Authority of NSW



# 2.1

## STRUCTURE PLAN



# Draft Structure Plan

The Bays West Structure Plan sets out an overarching and integrated system framework for the future of Bays West. It articulates the primary land use, open space, and connectivity network structures that have been developed for the precinct to capitalise on its place character and support its long-term renewal.

The structure plan is informed by the vision and directions established for the precinct. It identifies the key strategic elements that will drive the transformation of Bays West, while allowing a staged delivery. The structure plan demonstrates how the precinct can achieve its potential.

The structure plan connects the precinct with its adjacent neighbourhoods and will facilitate access to water and travel through the precinct. The plan encourages a diversity of land use, high public amenity, and embedded infrastructure to support adjacent and future communities.

The structure plan is presented as an aspirational end-state representation of the Precinct's urban renewal. It is a broader framework for the precinct on which further, more detailed investigations, into for example, optimal land uses and urban form, will be based.

## Key elements of the Structure Plan:

### PORTS & WORKING HARBOUR LAND ZONES

The structure plan outlines a transition of existing ports and maritime industries within the precinct into consolidated land and water zones. The location and arrangement of these zones are best suited to retain, optimise and grow existing operations unlocking alternative use opportunities for the precinct.

### PORTS & WORKING HARBOUR WATER ZONES

The strategically important deep-water berthing zones within the precinct have been largely retained for use at White Bay, adjacent to the dedicated land zones. In Rozelle Bay, the water zones offer shallower berthing, facilitating a mix of recreational vessels along with other maritime service and contractor vessels.

### INTEGRATED PORTS FACILITY

The eastern end of Glebe Island has some of the deepest water berths available and are essential for vessels with a deep draught. An important port waterfront operational interface will be maintained at this location. Opportunities to create innovative solutions for open space to be developed above port infrastructure will be investigated.

### NEW DEVELOPMENT ZONES

The structure plan marks out areas within the precinct capable of supporting new development and envisages a mixed-use renewal with vibrant and diverse building outcomes.

### INTEGRATED DEVELOPMENT ZONES

A number of the port and working harbour and critical Transport for NSW (TfNSW) operations in Rozelle Bay and Port Authority of NSW (PANSW) zones of White Bay can be retained and integrated into the proposed development.

### DEVELOPMENT ZONE WITH GREATER HEIGHT POTENTIAL

The development scale and intensity is responsive to existing site characteristics, calibrated to consider amenity impacts to adjacent neighbourhoods and preserve key views, while embedding a layer of flexibility to facilitate the evolving needs of the local community and wider Sydney region.

## 2.1 STRUCTURE PLAN

### ROAD STRUCTURES

A primary road structure is set out in the structure plan, which promotes a street network that aligns with the desired movement and place outcomes for Bays West. A hierarchy of street typologies has been established to support prioritised and local movements, minimise opportunities for through traffic, and deter resident and worker dependence on private vehicles.

### PUBLIC DOMAIN ZONES

An evenly distributed and interconnected series of key public domain zones have been established across the precinct. These are typically located to incorporate either natural features or heritage artefacts, maximising public benefit and amenity of these elements.

### FORESHORE PROMENADE

The precinct's harbour foreshore will be progressively unlocked, and a new foreshore promenade established, reclaiming public access and delivering recreation opportunities. The promenade will stitch together key public domain zones and connect into the broader harbour foreshore network. Where and when required, public access will be managed to give priority to port and maritime uses.

### WALKING & CYCLING NETWORK

A prioritised network of direct and desirable commuter links connect the precinct to its adjacent neighbourhoods. The network establishes local and regional connections with a series of informal pathways overlayed for recreational movements, promoting walking and cycling as the precinct's default mobility choices.

### HERITAGE LANDMARKS

There are a series of key heritage landmarks within the precinct, which act as destination markers and speak directly to the place narrative and history of Bays West. White Bay Power Station, Glebe Island Silos, and the Anzac and Glebe Island bridges set up a gateway sequence and provide significant opportunities for adaptive reuse and public access/interaction.

### HERITAGE TRACINGS

Embedded within the grain of the structure plan are a series of heritage tracings. These highlight key opportunities to integrate places stories and interpretations overlays into the precinct's renewal narrative, highlighting its former and current uses, and promoting the preservation of existing artefacts.



# Draft Bays West Structure Plan 2040 and beyond

## 2.1 STRUCTURE PLAN

### LEGEND

- Bays West Site Boundary
- Light Rail Station
- Light Rail Route
- M Future 'The Bays' Metro Station
- Future 'The Bays' Metro Station Box
- <--> Proposed Active Transport Connection
- Potential Future Active Transport Connection
- B Proposed Bus Stops/Interchange
- Key Heritage Landmarks
- Proposed Key Public Domain
- Key Landform
- Foreshore Promenade
- Proposed Promenade Linking Connections
- ... Occasional Foreshore Promenade Access (non-ship days)
- Existing Foreshore Promenade
- Proposed Zone of Development
- Proposed Taller Building Cluster
- Integrated Development / Ports & Working Harbour
- Public Domain integrated into projects
- Integrated Ports Facility with Elevated Public Domain
- Ports & Working Harbour Zone
- Vessel Berthing Zone

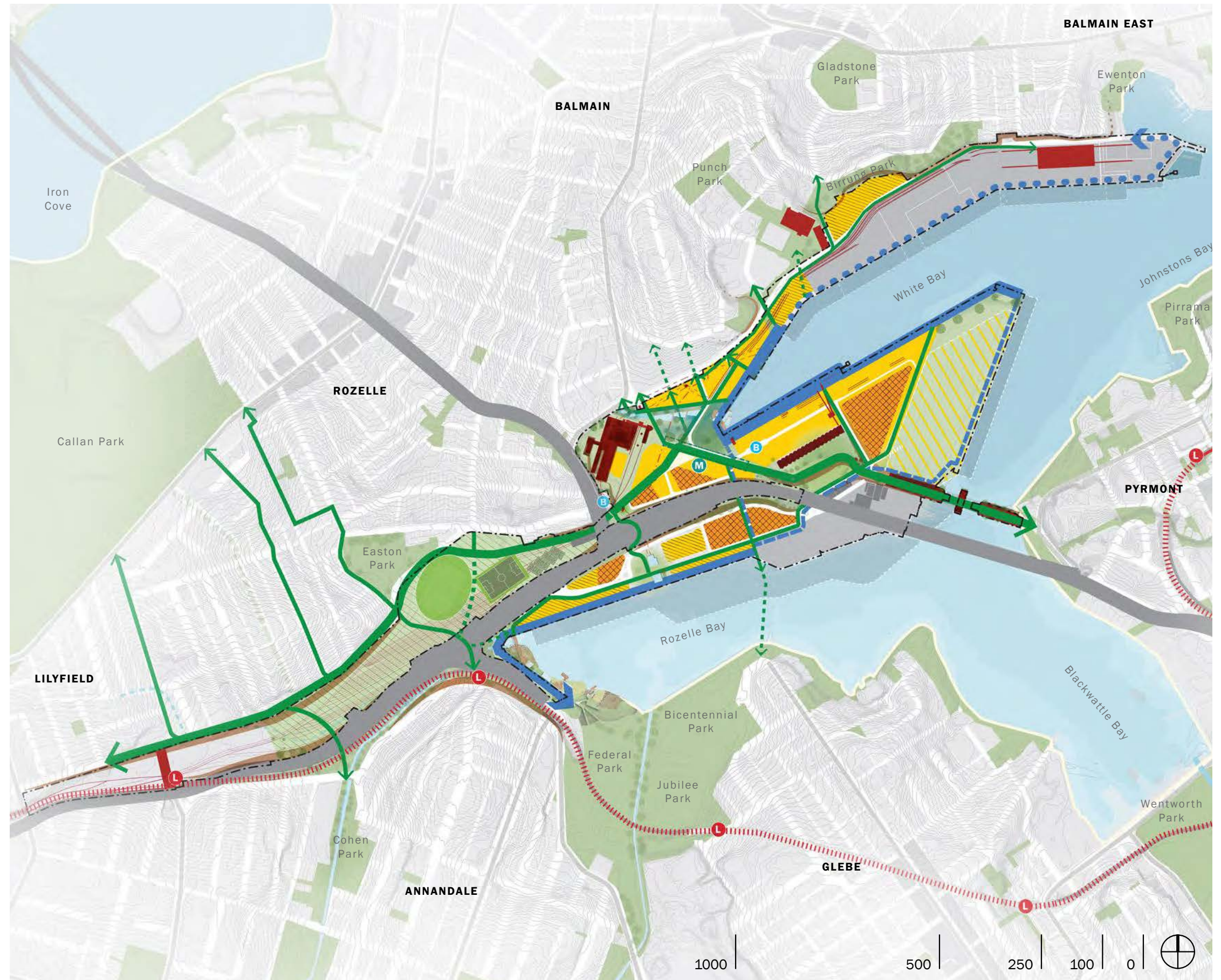


DIAGRAM 2.1 Bays West Structure Plan

Scale 1:10,000 @ A3  
23



# On Country

Embedded within the Structure Plan are elements which respond to the strategies identified in the *Bays West Draft Connecting with Country Framework*. The diagram represents some of the major spatial features which are included within the structure plan and align with the Designing with Country Strategy & Directions for Bays West which include:

- Caring for Stories is Caring for the Spirit of Country
- Minimise Damage to Country
- Co-Existing on Country
- Adaptation to Climate Change
- Care for Saltwater Country
- Care for Freshwater Country
- Care for Tucoerah/Tuggorah Country
- Creating Connections to Water
- Care for Complete Ecologies
- Care for all Remnants and Layers of Heritage
- Care for Sandstone Remnants
- Connecting up Country
- Country Appropriate Infrastructure
- Opportunities on Country
- Visual Connections to Country
- Cultural Expressions of Country
- Retained Connection to Country

Connection to Country extends across all of the directions and five themes these have been grouped into. Utilising this perspectives on these within the *Bays West Connecting with Country Framework* there are opportunities available to extend these approaches into the more detailed phases of design and delivery. Note that Country perspectives extend beyond the Heritage and Culture themes and Country has been considered more holistically within the document suite that has been developed.

For a summary of the dialogue on Place Stories between Country and Post-colonial Era refer to the *Draft Strategic Place Framework*.

Refer to the *Bays West Draft Connecting with Country Framework* document in **Supporting Frameworks S.01**.

## Country driven spatial strategies:



Place of cultural sharing, truth telling (learning from connecting with Country) and ceremony on top of Glebe Island sandstone with outlook to water.



Welcome/language gateways to enable an understanding of the Spirit of Country you are entering.



Naturalised watercourse and water treatment zones within public domain spaces that slow and clean the water contained within to improve water quality.



Celebrate use of small craft within bays featuring elements to stories of water crafts.



Celebrate water crossings and bridges featuring references elements to stories of water.



Water level change interpretation at key points.



Views and outlook to harbour from key public domain spaces (and major connection structures).



Shoreline change interpretation embedded throughout via markers.



Green streets and foreshore links between public domain spaces.



Sandstone expressed and celebrated/integrated into public domain spaces.



Retention and adaptive re-use of all heritage assets and prioritise view from public domain zones.



## Head of White Bay:

- Interventions at White Bay and water zones to establish a rocky edge habitat.
- Incorporate special seahorse protection zones.
- Critical interface points where ecosystems and uses are adjacent to be considered to determine best possible form of co-existence:
  - land and water boundary
  - ports and working harbour with development zone
- Water access and engagement points built into the new rocky edge habitat intervention.



## Head of Rozelle Bay:

- Celebrate water crossings and bridges featuring references elements to stories of water.
- Interventions at Rozelle Bay and water zones to establish a bitterwater habitat.
- Incorporate special seahorse protection zones including sea grass beds.
- Critical interface points where ecosystems and uses are adjacent to be considered to determine best possible form of co-existence:
  - land and water boundary
  - ports and working harbour with development zone
- Water access and engagement points built into the foreshore with possible boardwalks through and over new ecology zones.
- Improved access and protection for small vessels to utilise



LEGEND

- Bays West Site Boundary
- Light Rail Station
- Light Rail Route
- Future 'The Bays' Metro Station
- Future 'The Bays' Metro Station Box
- Proposed Active Transport Connection
- Potential Future Active Transport Connection
- Proposed Bus Stops/Interchange
- Key Heritage Landmarks
- Proposed Key Public Domain
- Key Landform
- Foreshore Promenade
- Proposed Promenade Linking Connections
- Occasional Foreshore Promenade Access (non-ship days)
- Existing Foreshore Promenade
- Proposed Zone of Development
- Proposed Taller Building Cluster
- Integrated Development / Ports & Working Harbour
- Public Domain integrated into projects
- Integrated Ports Facility with Elevated Public Domain
- Ports & Working Harbour Zone
- Vessel Berthing Zone
- Place of Cultural Sharing
- Welcome/Language Gateways
- Naturalised Watercourse & Water Treatment Xones
- Celebrate use of Small Craft
- Celebrate Water Crossings & Bridges
- Water Level Change Interpretation
- Views and outlook to harbour
- Shoreline Change Interpretation Markers
- Green Streets & Foreshore Links
- Sandstone Expressed & Celebrated / Integrated
- Retention & Adaptive Re-Use of All Heritage Assets
- # Head of White Bay
- \* Head of Rozelle Bay

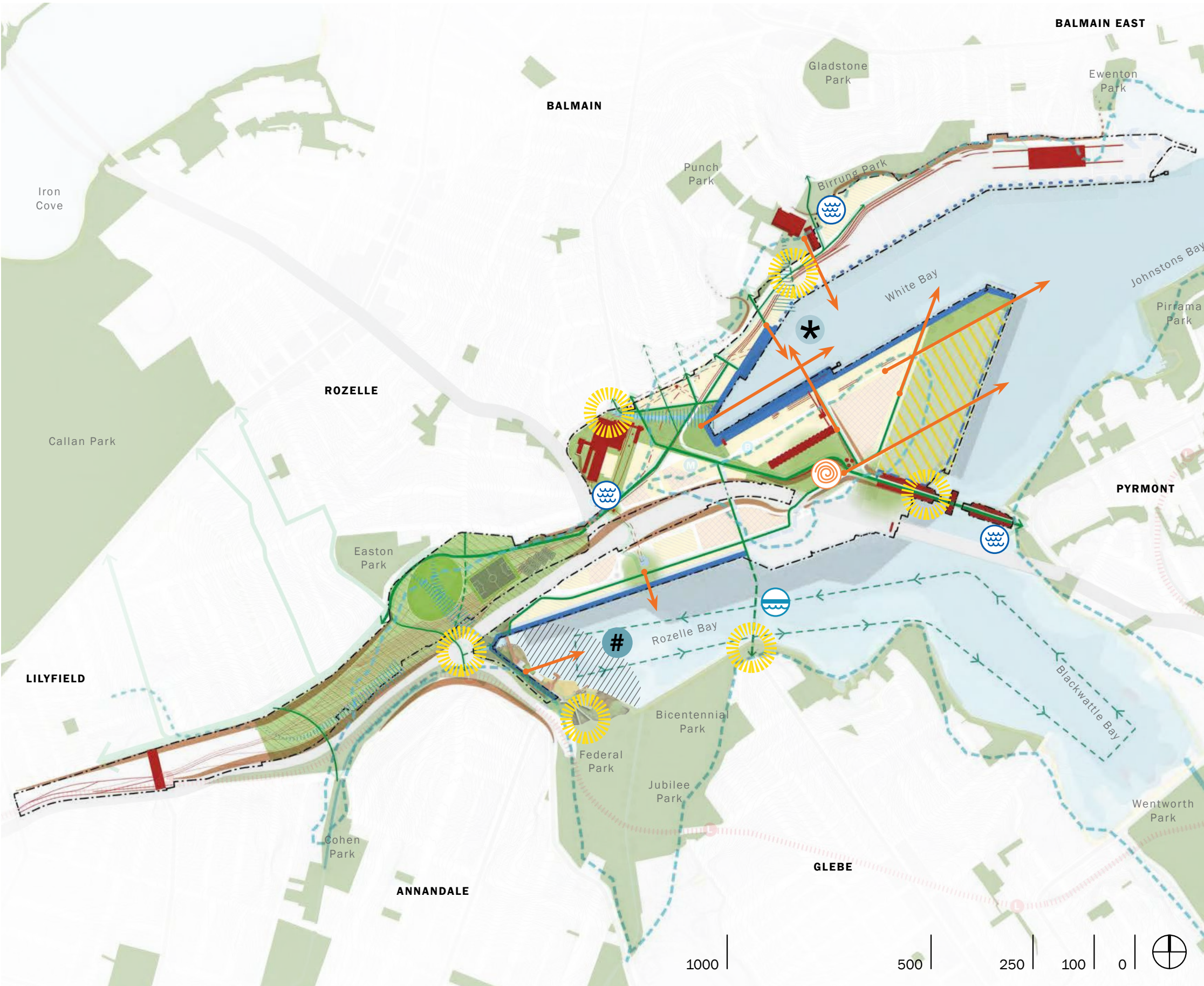


DIAGRAM 2.2 Overlay of zones of Bays West Structure Plan with embedded Connecting with Country features

Scale 1:10,000 @ A3  
25



# 2.2

## PRINCIPLES



# Introduction

The principles support the vision and directions in the Bays West *Draft Place Strategy*, and *Draft Strategic Place Framework*. They follow the five themes under which the directions have been classified to provide a clear linkages across the document. They have also been developed to support the Big Moves that the NSW Government has outlined in the *Draft Place Strategy*.

The Principles describe specific outcomes for Bays West, to enable a high amenity and place focused Precinct. These provide more specific guidance for the future planning and development of Bays West.

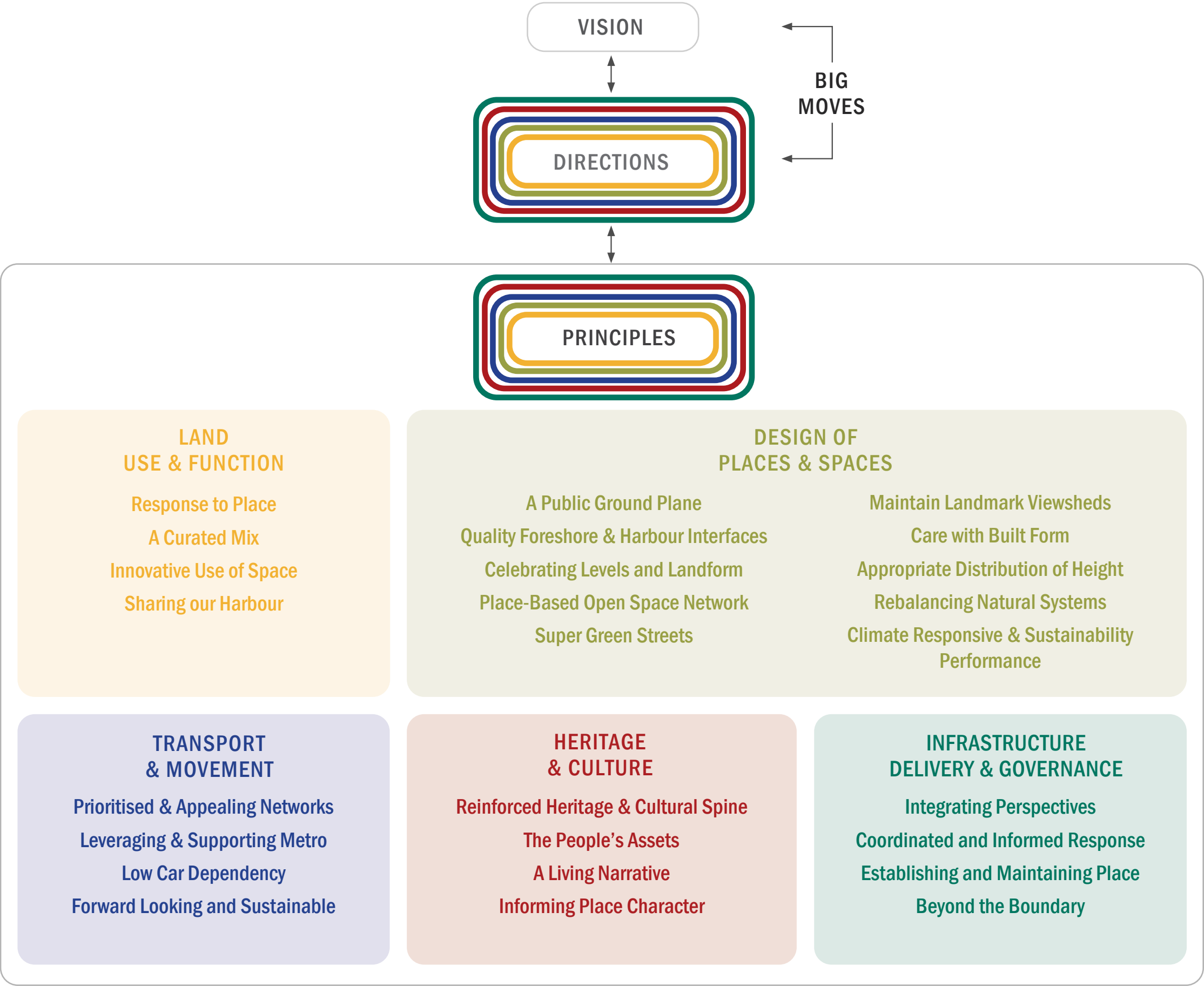


DIAGRAM 2.3 Overview of Bays West Principles







# Land Use & Function

## 2.2 PRINCIPLES

### Response to Place

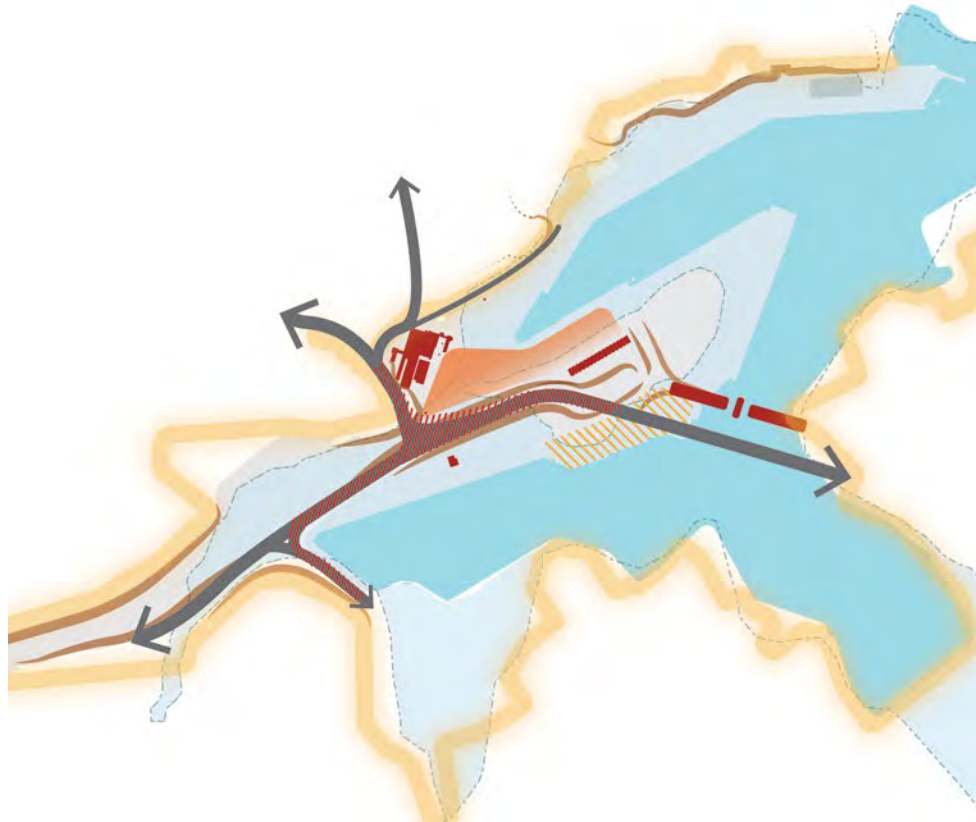
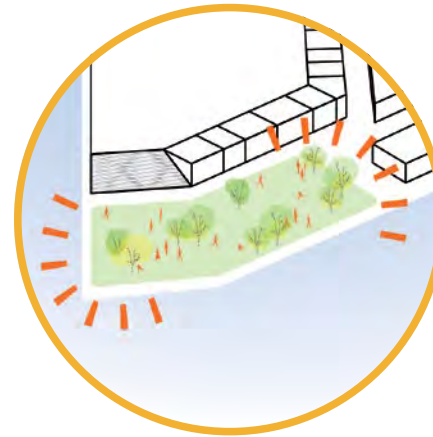


DIAGRAM 2.5 Distribution in response to place

*The unique Country characteristics and songlines, the existing morphology, activities and community engagement with Bays West should drive decisions on appropriate siting of uses.*



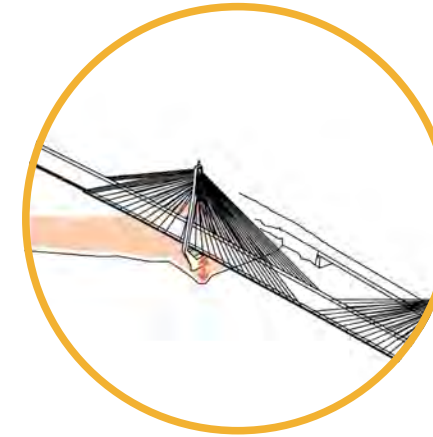
#### Celebrate Key Features

Underpin the structure of the site with the unique features at Bays West, including the heads of bays and peninsulas, heritage assets and landform. Locate people-focused uses at these key features to optimise community enjoyment whilst balancing ports and working harbour needs.



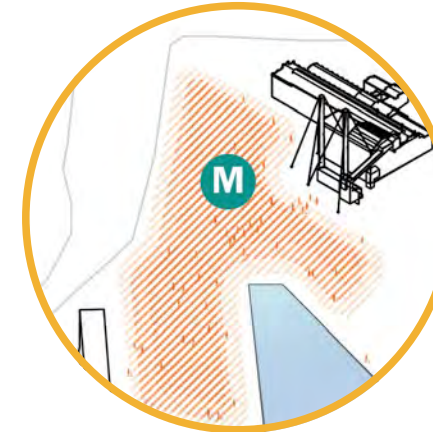
#### Boundary Edges

Position uses to create good transitions and integration with surrounding areas. Support existing and new communities by delivering community infrastructure that is well-connected to surrounding areas and situated in clear logical locations which assist in unlocking the Precinct boundary.



#### Restricted Zones

Ensure restricted areas (under bridges, level changes, overshadowed and noisy areas) are well integrated, and accommodate non people-focused uses. Locate high-use and people-focused places are in the areas of highest amenity.



#### A Focused Centre

Deliver a considered volume and intensity of residential, employment and other uses in proximity to the metro and head of White Bay (and associated open space) to create a centre with 24/7 activity and vibrancy from the initial stage. This creates strong opportunities in proximity to heritage landmarks, the head of the bay and significant open space.



# Land Use & Function

## 2.2 PRINCIPLES

### A Curated Mix

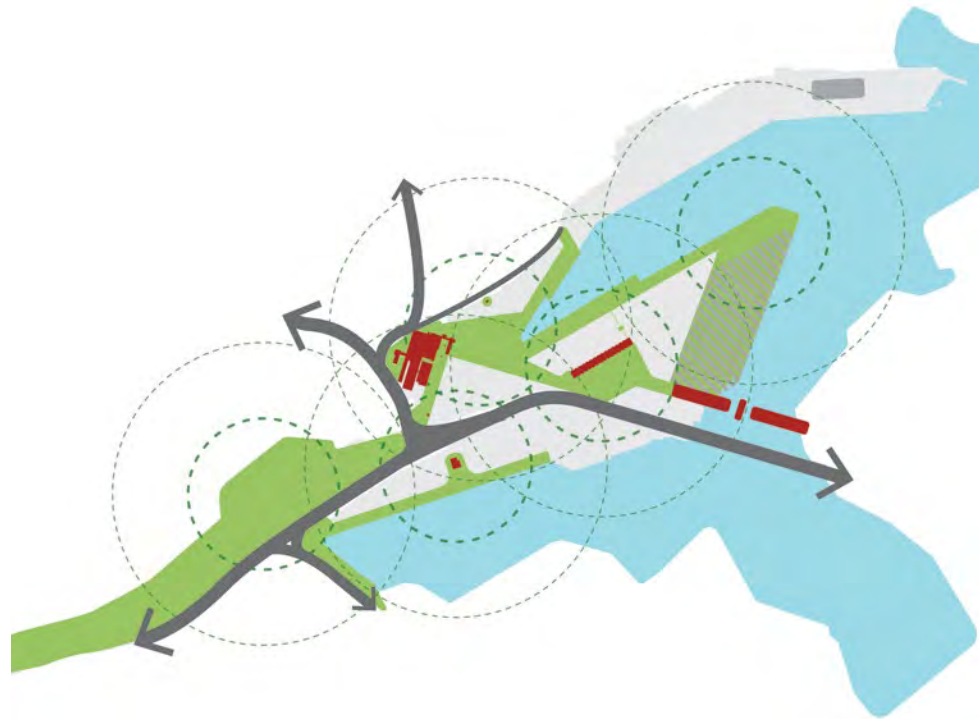
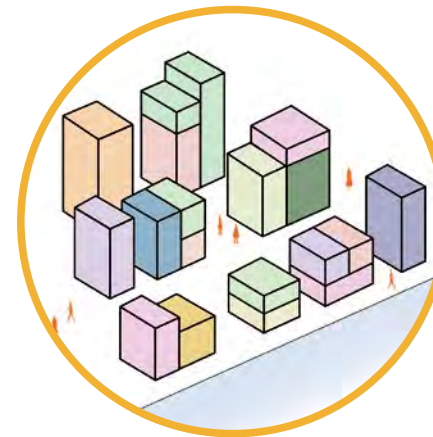


DIAGRAM 2.6 A curated mix

*Curate the mix of uses at Bays West to create a cohesive waterfront precinct on Country. Integrate identified strategic needs and their supporting components to ensure that the diversity of uses to foster a knowledge intensive precinct, including the broad range of employment and residential requirements are met.*



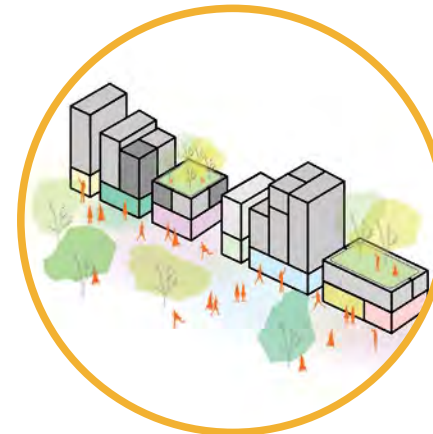
#### Balance

Deliver a wide variety of uses that balance the retention of ports and working harbour activity with residential, commercial, education, community and other uses, to support a highly desirable and thriving place that fosters community.



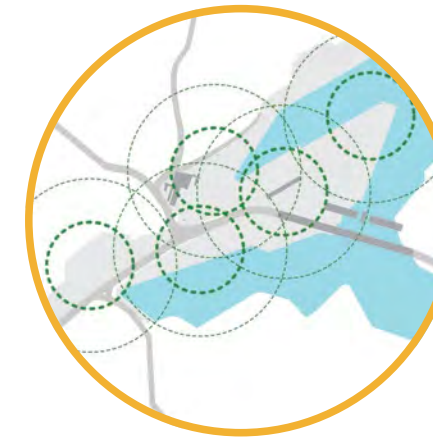
#### Evolving Anchors

Retain and evolve the role of the existing ports and working harbour uses as anchors of the Precinct. Foster innovation in the ports and maritime related economy, contributing to the overall desired place and strategic outcomes. This creates a unique setting for the precinct and one which has the potential to be a significant draw card, if managed appropriately.



#### Vibrant and Activated

Locate publicly accessible uses to interface with the ground plane and lower levels of all buildings to create a precinct with magnetic place quality and a 24/7 economy. Enable increased trading in the high activity precinct centre.



#### Considered Infrastructure Distribution

Equitably distribute social and cultural infrastructure to maximise public benefit. Position in areas of high amenity and adjacent to high-quality public domain to ensure engagement with and activation of these space.



#### Diverse & Affordable

Ensure diversity in the land use mix to create a place that will attract a broad range of users, residents and workers. Allow for affordable housing and employment spaces to add to the vitality and energy of the place.



# Land Use & Function

## 2.2 PRINCIPLES

### Innovative Use of Space

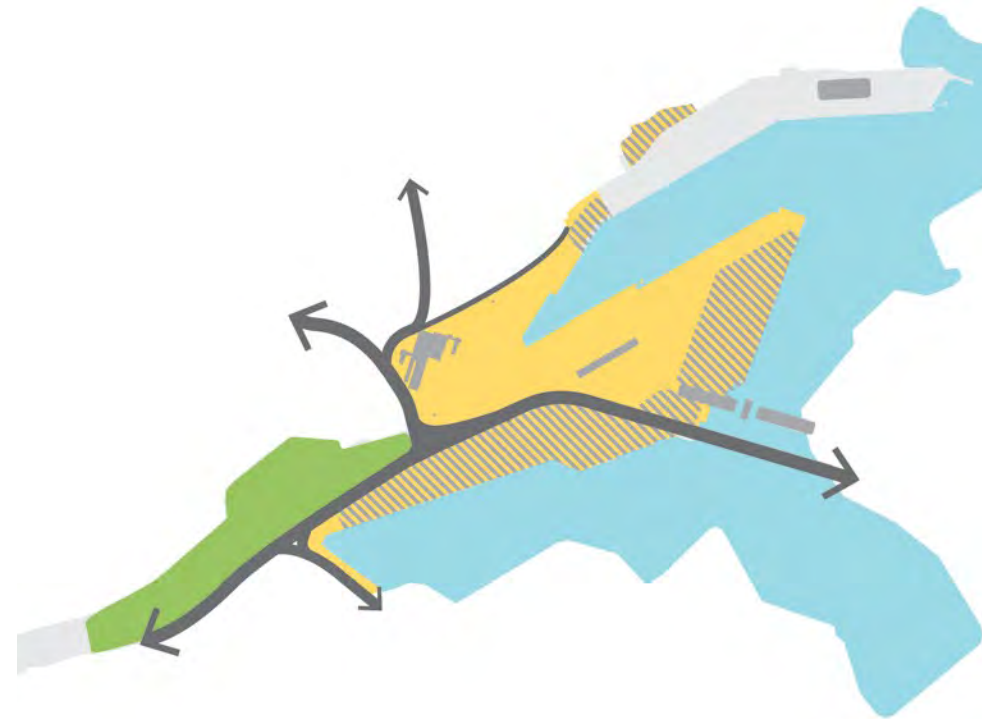
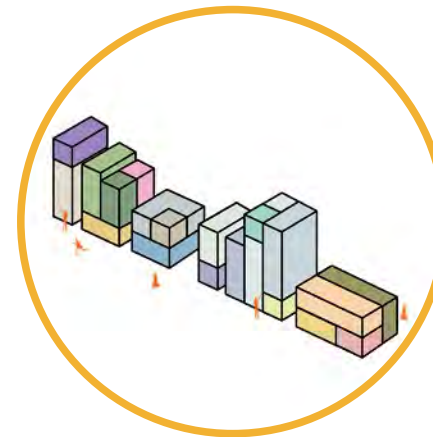


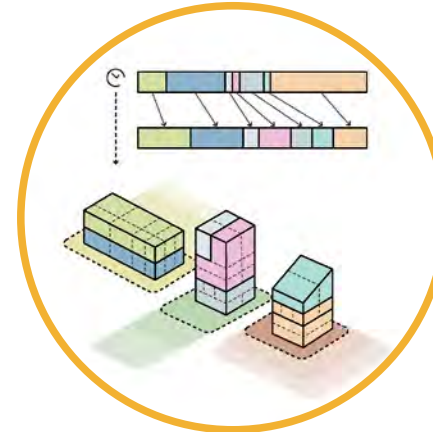
DIAGRAM 2.7 Innovative Use of Space

*Creatively integrate uses to optimise the available developable land and maximise delivery of strategic uses to support Country, the surroundings and Innovation Corridor.*



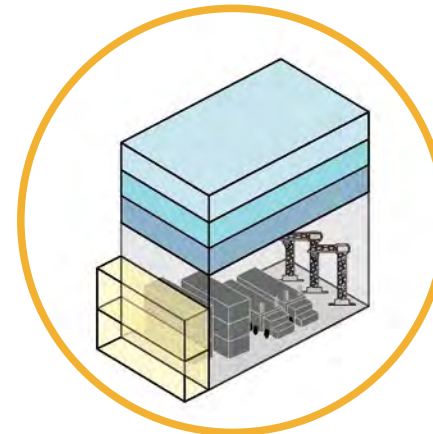
#### Hybrid Mix

Facilitate the delivery of diverse strategic needs at Bays West by accommodating non-typical and more diverse mixes within development proposals. Utilise the opportunity to create innovative and distinct building typologies that focus on public use and water access at the lower levels.



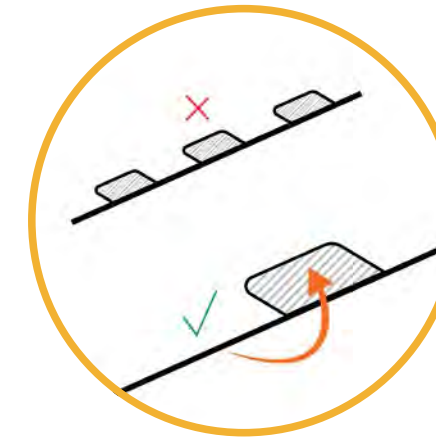
#### Flexible & Adaptable

Establish a flexible, adaptable and multi-functional approaches to land use, delivering space and building typologies to accommodate the variety of economic activities and the evolving needs of the precinct over time.



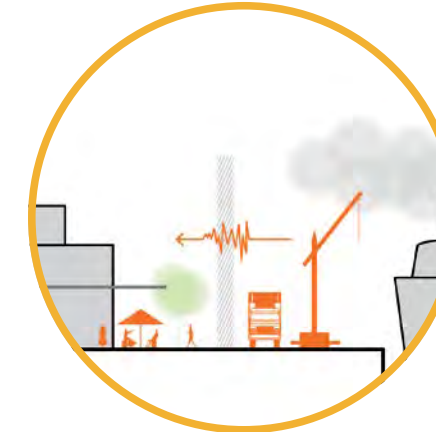
#### Integrated

Integrate a range of land uses in developments to deliver a truly mixed-use precinct. This includes the optimisation of ports and working harbour and other uses in a balanced relationship and integration into unified proposals where appropriate.



#### Clustering

Consider place features to determine position for clusters of strategically important ports and working harbour in locations which allow ongoing growth and evolution of the port and working harbour uses in harmony with the new. Clustering will assist in managing amenity impacts on other sensitive land uses and provide proximity and agglomeration benefits.



#### Considered Interfaces

Where development and ports and working harbour uses exist in integrated or separate clustered zones, it is essential to design buffers to mitigate the impacts of industry and protect their ongoing operation. Consider innovative solutions which address impacts such as noise, emissions and traffic and for public foreshore access can be provided.



# Land Use & Function

## Sharing Our Harbour

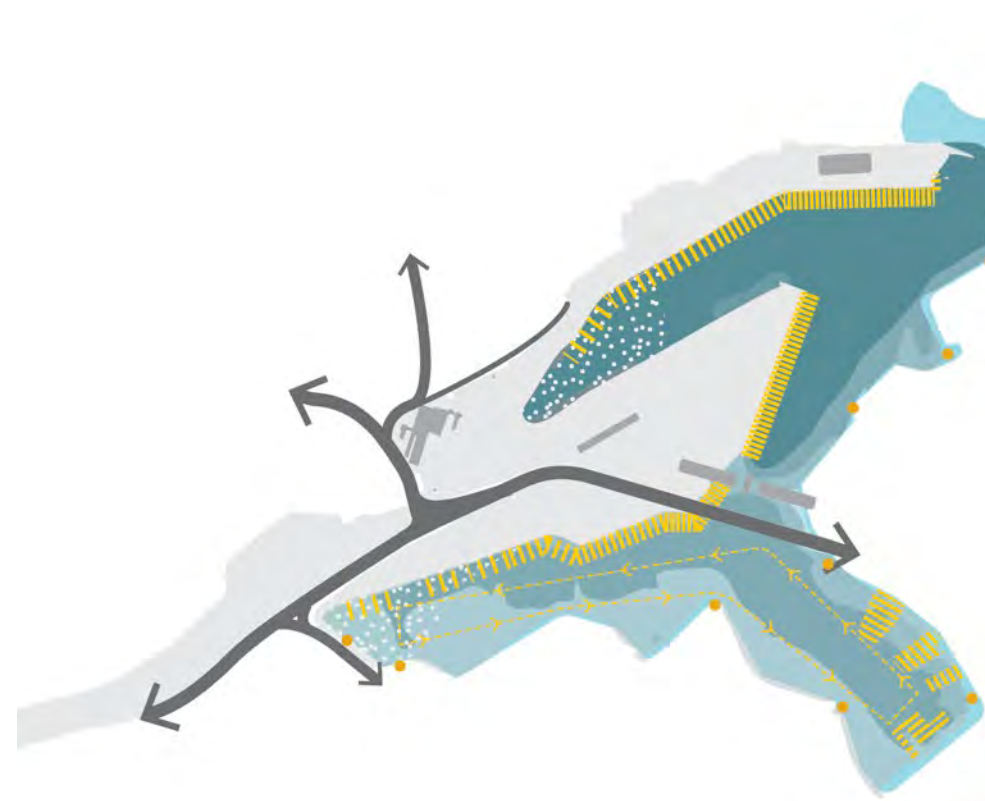


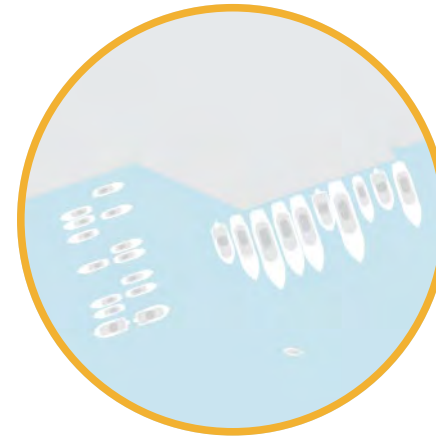
DIAGRAM 2.8 Sharing Our Harbour

*Sydney Harbour is one of the city's greatest assets and a key feature at Bays West. Connecting with Country includes connecting and caring for this water. Enable the creation of a place on the Sydney Harbour foreshore that opens up genuine access and engagement with the water in balance with ports and working harbour operations.*



### Public Access to and use of Water

Strive for balancing public access to the foreshore and use of the harbour with the ports and working harbour activities. Deliver an exemplar integrated public and working harbour for Sydney.



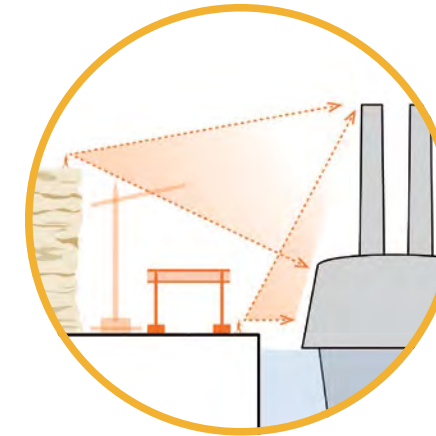
### Grading of Scales

Locate and protect larger scale strategic port and working harbour uses at deep-water berthing zones. Where possible consolidate smaller / lower impact vessels (e.g. recreational vessels; charter boats) along public sections of foreshore and in proximity to public domain areas.



### Space for Water Ecosystem

Demonstrate caring for Country in harmony with ports and working harbour uses. Establish ecology and biodiversity zones at the heads of bays connected to new blue-green corridors and including parameters for establishing sea horse protections.



### Engagement with maritime uses

Foster a sense of engagement with port and working harbour activity. Recognising these uses, create a distinctive place and places for people to interact with the port and working harbour.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### A Public Ground Plane

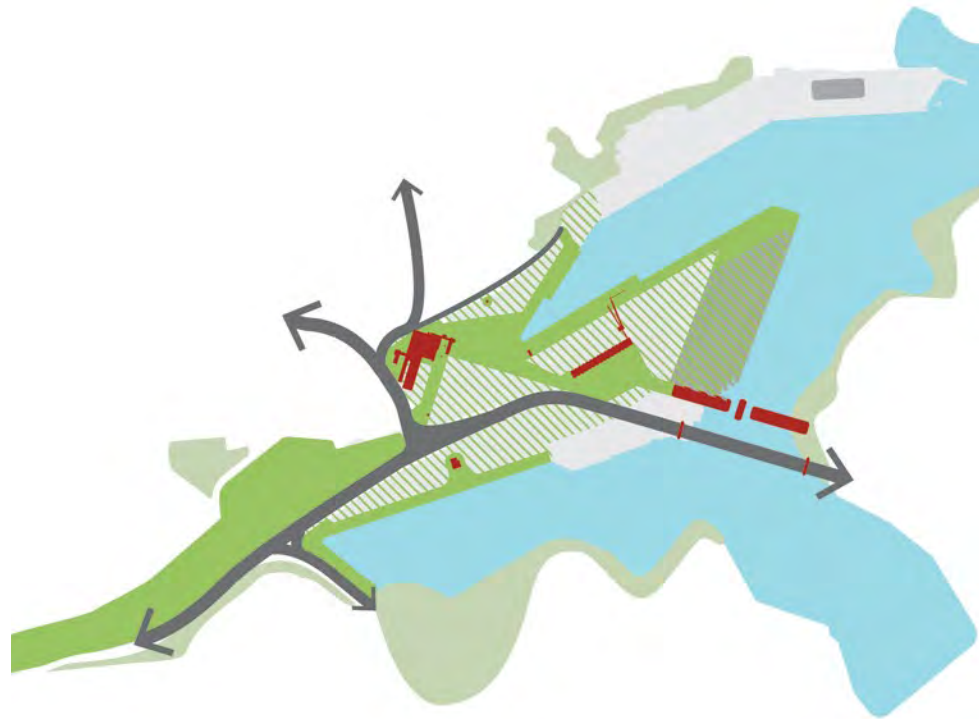
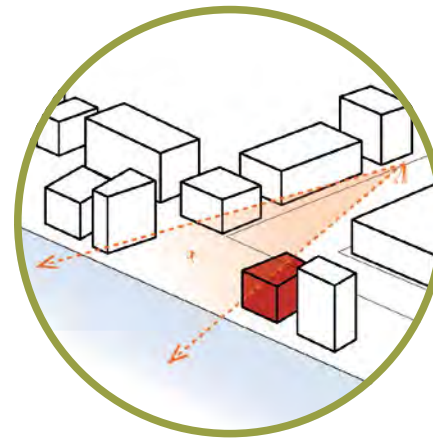


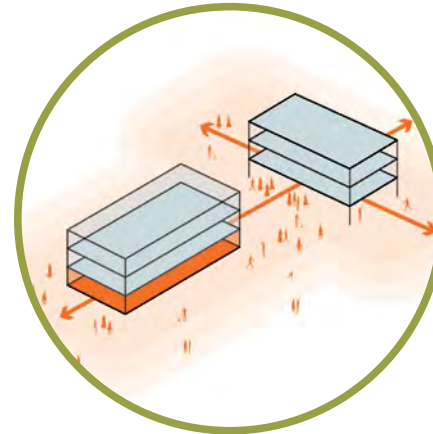
DIAGRAM 2.9 A Public Ground Plane

*Design and deliver a public ground plane where people can understand this Country, feel welcomed and have unimpeded movement throughout. The ground plane should have high permeability and amenity and perform as an activated and socially inclusive place.*



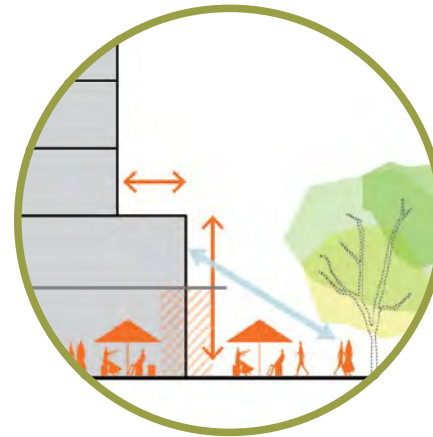
#### On Arrival

Unlock the Precincts thresholds and ensure entering the Precinct is a wonderful experience, discovering a place that shares the stories of Country, and the Harbour's maritime identity, is welcoming to all people and has views to significant features of Bays West.



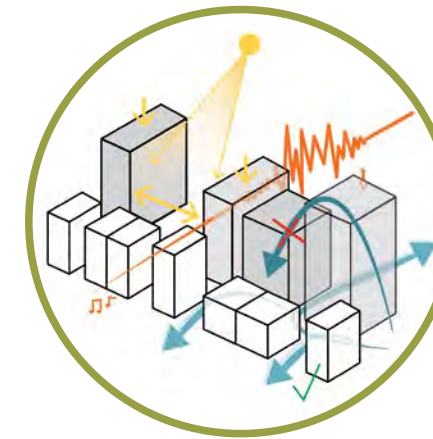
#### Public Access

Open the ground plane up to people, whilst considering the operational and safety factors constraints of ports and working harbour areas. In all other areas be open and public with free access in, around and through the built form and generous public domain, creating places to gather and foster a sense of community.



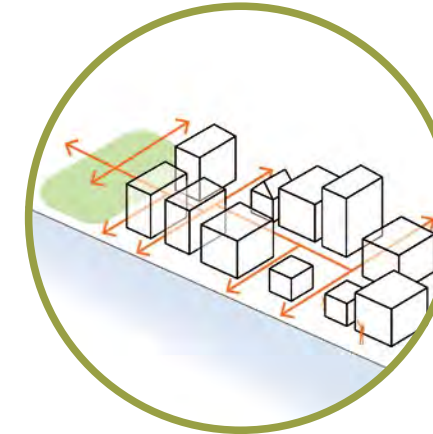
#### Quality Interface & Active Edges

Ensure the design of the ground and lower levels of buildings contribute to the amenity, character, passive surveillance and activation of adjacent streets and public spaces.



#### Ensuring Amenity

Ensure built forms contribute to comfortable streets and public spaces. This include consideration for solar, wind, acoustic and other micro-climatic conditions and protection of solar access to existing open space areas.



#### Permeable

Prioritise public access and accessibility for pedestrians and cyclists. Create high levels of permeability between and within buildings and direct and intuitive movement paths to the precinct's major features and services.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### Quality Foreshore & Harbour Interface

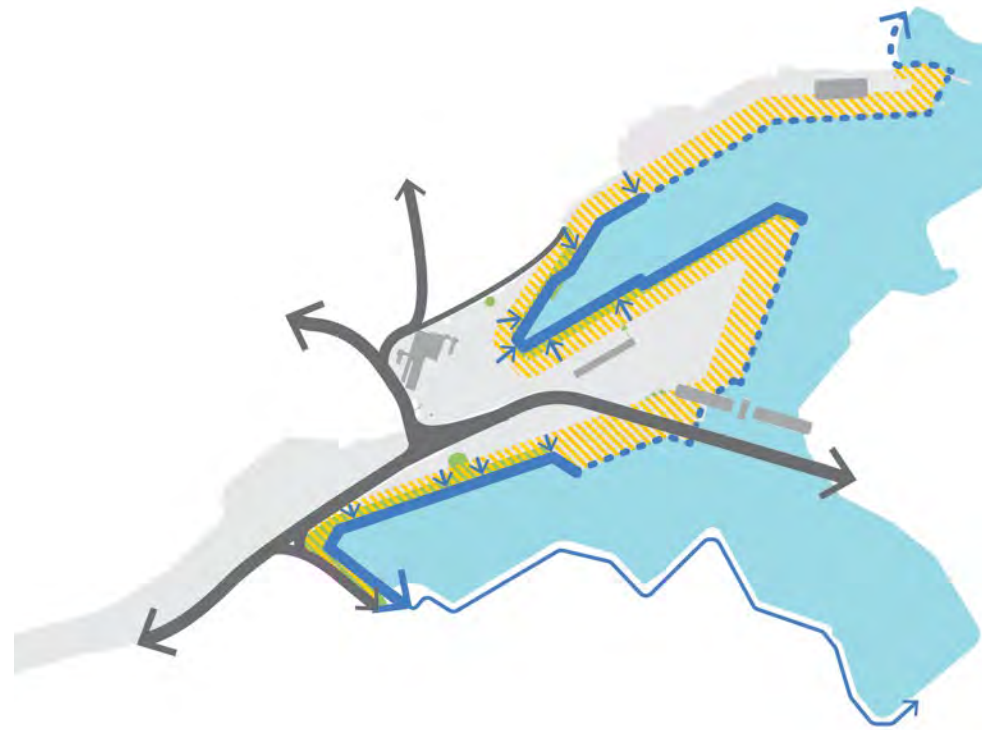
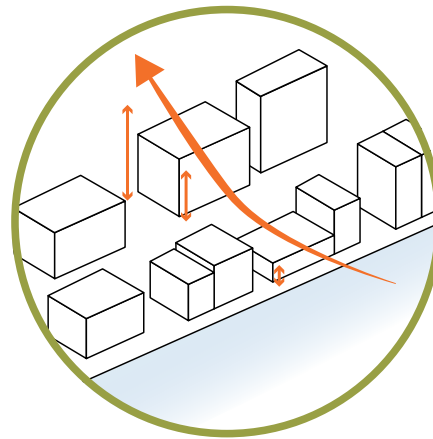


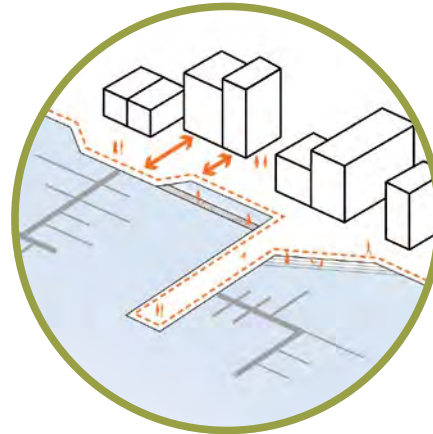
DIAGRAM 2.10 Quality Foreshore and Harbour Interface

*Extend and link existing foreshore connections through Bays West with a high amenity and publicly accessible foreshore promenade. Ensure that associated land and water zones are of equally high-quality. Through the design, demonstrate care for Country and link together new public domain zones and the various heritage and ports and working harbour experiences into a cohesive and integrated precinct.*



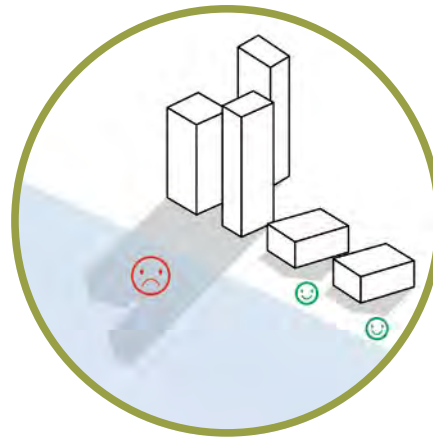
#### Transition

Create a quality relationship between the landform, built form, public domain and foreshore. Transition the height of built form down to the water and minimise adverse effects on the foreshore conditions.



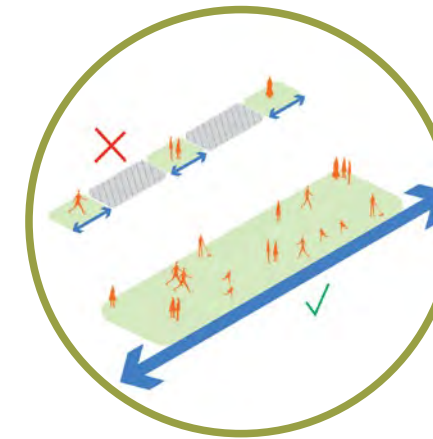
#### Setback & Edge Diversity

Provide substantive and varied setback from the water and a diverse set of water's edge conditions to set up a variety of waterfront character zones with opportunities for outlook to both water and ports and working harbour activities. A combination of boardwalks, steps to water, places to touch the water and heritage canals and inlets, will create a unique experience.



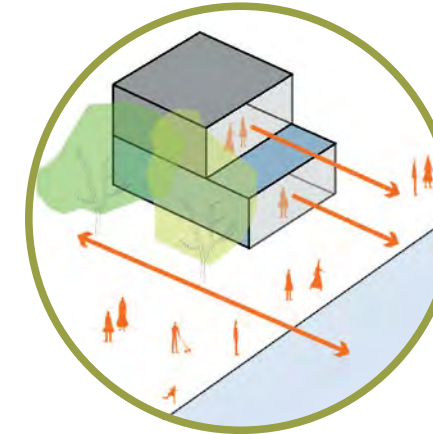
#### Solar Access

Maximise solar access to foreshore promenade must be considered at Bays West, prioritising high quality public domain areas, the heads of the bays, the foreshore promenade and the water to foster marine life. Overshadowing impacts are to be minimised but it is recognised that these may be unavoidable in limited circumstances.



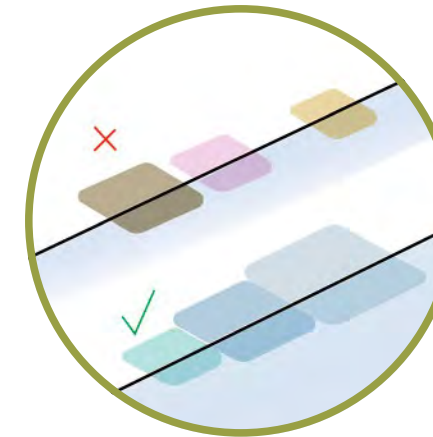
#### Continuous & Connected Lengths

Establish long, uninterrupted sections of public foreshore which connect to existing. Locations of larger public access lengths to be determined with consideration of the needs of the working harbour. Avoid small fragmented foreshore sections and where the ports and working harbour uses restrict public access at the waterfront, it is essential for high amenity connections joining these spaces together.



#### Connecting with Water

Enable direct physical engagement and access to water with prioritised permeability to foreshore. Ensure you can dip your toes in the water and marvel at the natural beauty of the precinct and its place in Sydney Harbour.



#### Cohesive

Ensure the foreshore does not feel like it is delivered in many different parts. There must be an integrated language that works across the whole precinct that blends into neighbouring zones. The cohesion and legibility of the foreshore design will be important to the delivery and success of the entire Precinct.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### Celebrating Levels and Landform

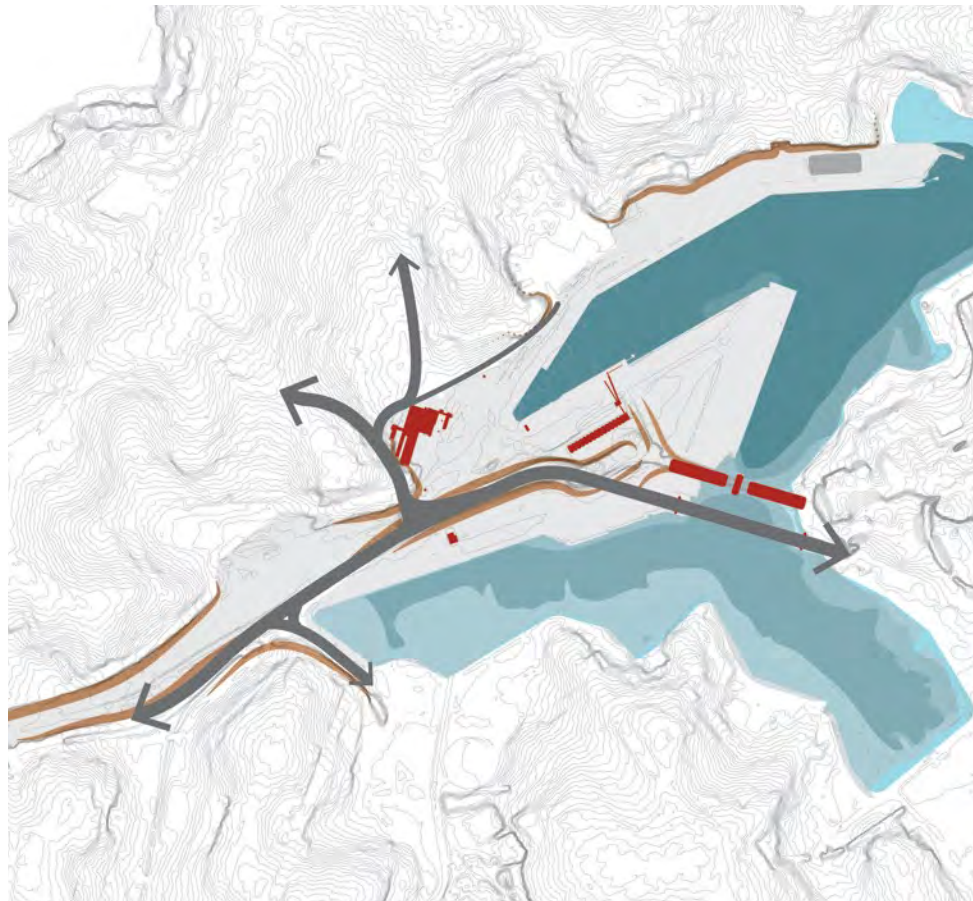
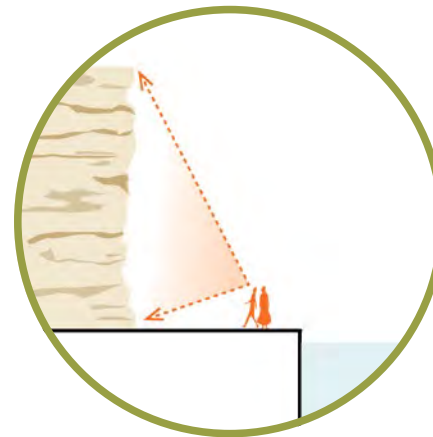


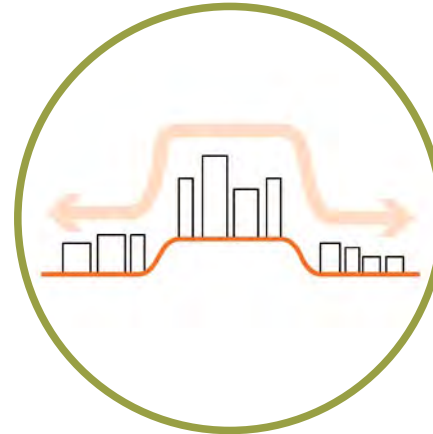
DIAGRAM 2.11 Celebrating Levels and Landform

*Retain the legibility of Country with the dramatic landform features of the original island, the escarpments, the reclaimed flats, and the dramatic scale of elements and experience that occur in the Bays West waterfront setting. Utilise Country appropriate solutions to overcome the barriers to access at key entry/exit points and within the site.*



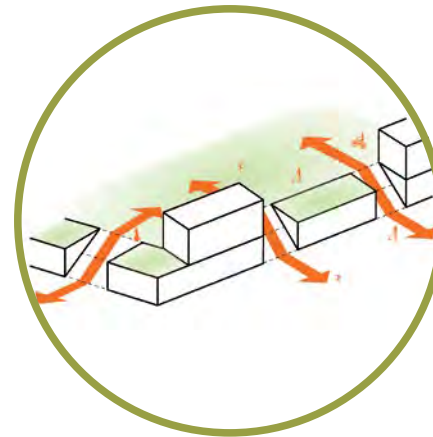
#### Revealing the Escarpment

Retain the escarpment and sandstone cuttings as features of Bays West that contribute to its drama and uniqueness. The escarpments at White Bay, Rozelle Rail Yards and Glebe Island are signifiers of the natural land formation and speak to changes over time that have reshaped this place.



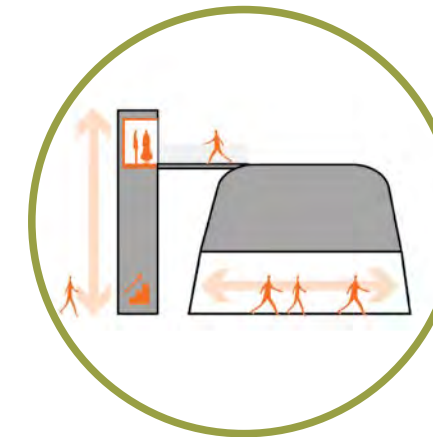
#### Legibility of Landform

Reveal the story of the site evolution through the landform and level changes which make each part of Bays West distinct. Maintain the legibility of the current landform and ensure that public places, and buildings, are sited and designed appropriately according to this unique landform.



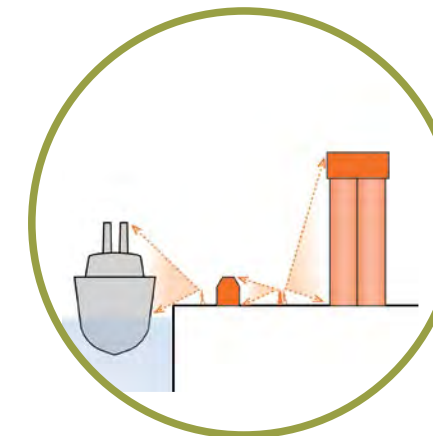
#### Level Transition and Integration

Provide design solutions to enable level change including built form integration, visibility between zones and highly visible access arrangements. Ensure convenient and legible public access provisions provided to all public use areas.



#### Overcoming Barriers

Use infrastructure creatively (tunnel links, bridges, stairs or lifts) to increase connectivity and access to and across the precinct and enhance the human experience of the precinct's landforms /topography /nature.



#### Experiencing Scale

Respond to the unique characteristics of the waterfront setting. Retain areas where the unusual experience of the being up close to both the grand scale of heritage buildings, large ships and landscape features and the finer grain features occur.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### Place-Based Open Space Network

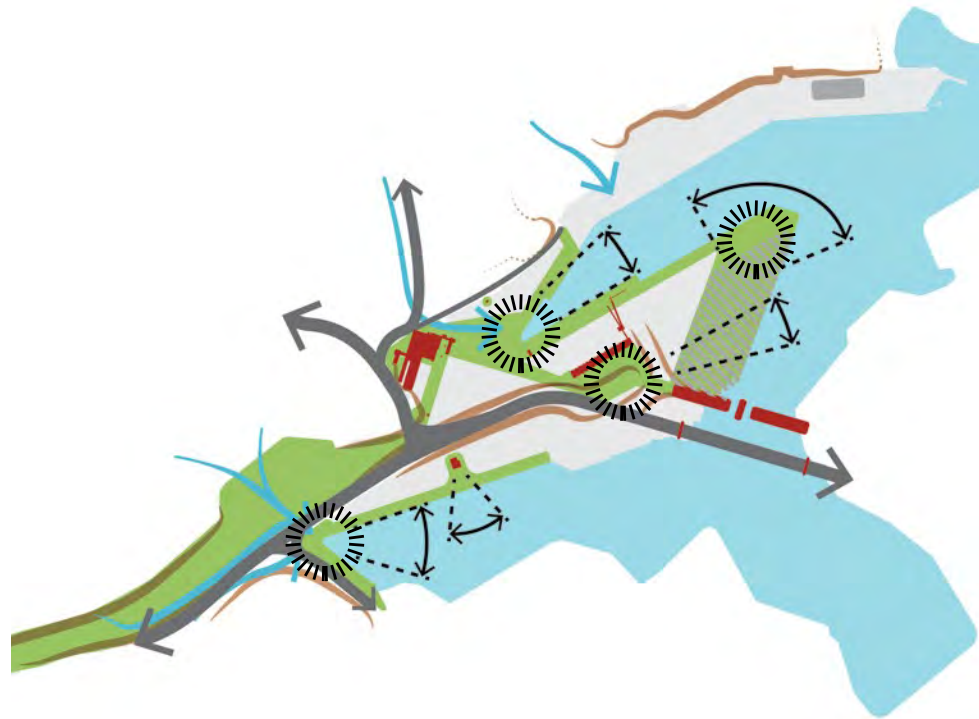
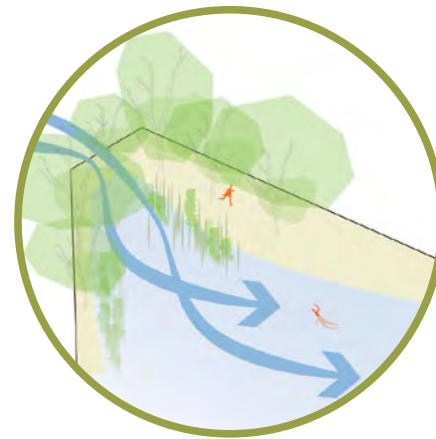


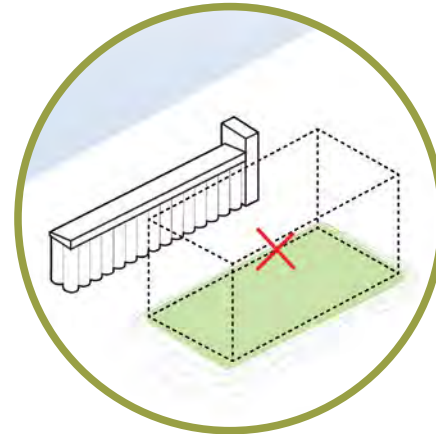
DIAGRAM 2.12 Place-based Open Space Network

*Deliver significant public places that focus around the attributes of this Country including the natural, cultural, industrial, maritime & infrastructural heritage artefacts, creating a series of diverse and inviting public spaces which give character and meaning to place and maximise public benefit.*



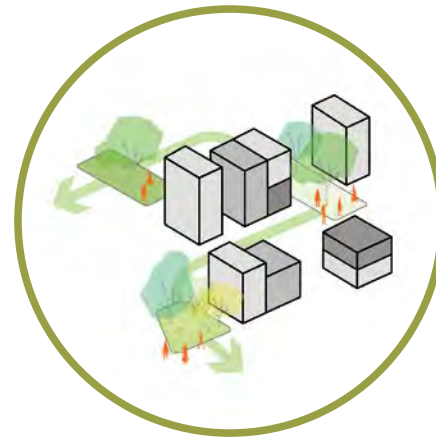
#### Blue Green Corridors

Reinstate and protect original creek corridors that provide logical links between the site and the surrounding areas with smooth level transitions. Extend the blue/green corridors through the site as the backbone of public domain linking existing open spaces to the harbour.



#### Heritage and Topographical Elements

Locate extended public domain spaces in response to heritage elements and dramatic landform locations, ensuring these elements are the heroes of these spaces. These should provide meaningful place anchors as the character focus of different spaces.



#### Infrastructure Integration Benefits

Optimise opportunities within larger scale infrastructure investment to create innovative new public domain spaces. Ensure they are well integrated and well-connected, and do not feel like 'left over' space.



#### Distributed and Diverse

Deliver a wide variety of landside and waterfront activities within the open space network, ranging from restorative, to passive and active, and integrate them into high-quality public domain spaces. Determine a hierarchy of parks, squares, streets and lanes and ensure they are well connected and distributed across the precinct.



#### View and Outlook

Provide uninterrupted outlook to sky, water and views of landmark features within and beyond the precinct at key locations. Key views must be visible and prioritised from the public domain via siting and orientation.



#### A Unified Network

Coordinate a unified materials and components language across the precinct to create a cohesive, integrated solution. Avoid fragmentation and variation across the suite of recurring elements such as seating, signage, utilities and amenities.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### Super Green Streets

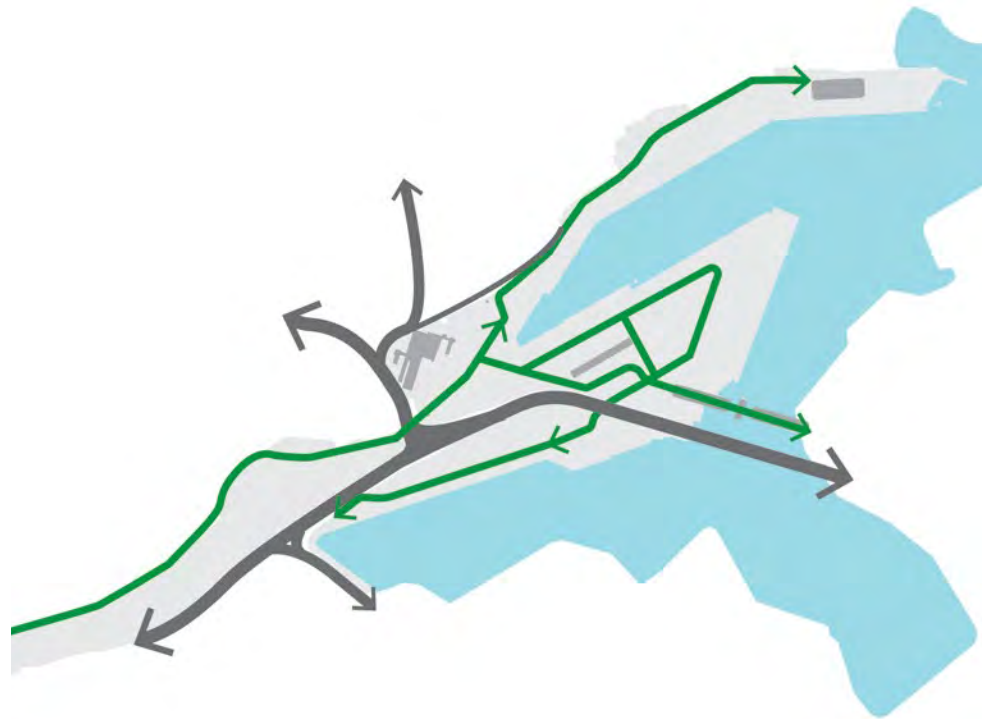
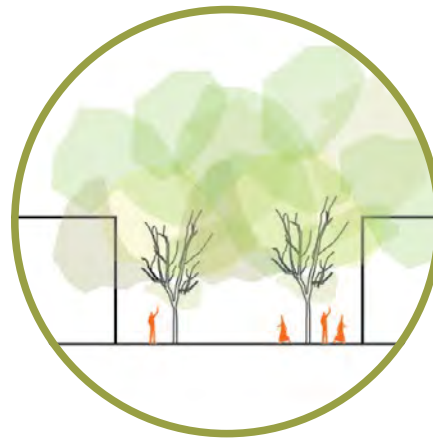


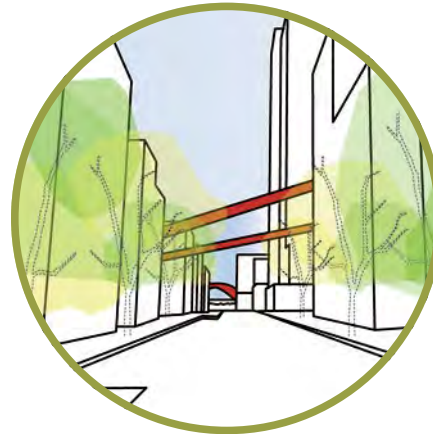
DIAGRAM 2.13 Super Green Streets

*Provide a direct and delightful network of street connections which connect up Country and tie together the public domain spaces. Maximise the value that indigenous trees provide within these links and consider the space needs and street level experience of people within them.*



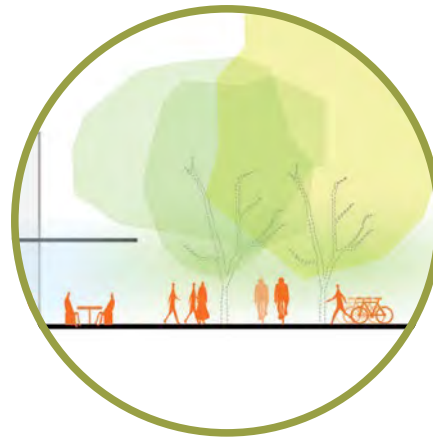
#### Canopy Cover

Promote green ecology and biodiversity throughout the Bays West Precinct via connected green street networks. Minimise the impacts of heat island effects through well shaded streets that will also offer amenity and comfort to users.



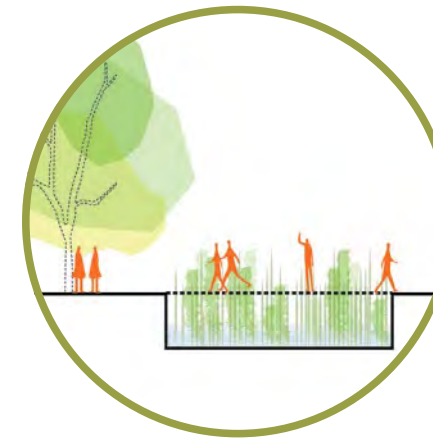
#### Street Level Experience

Create a great experience from the street level and understand the implications of scale, height, shadow and views. Optimise the ground plane experience by ensuring views to sky, place features and a balance of light and shade.



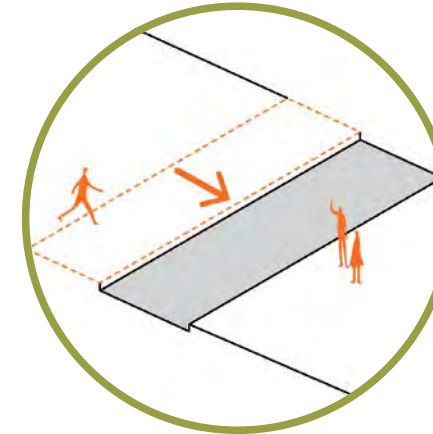
#### Comfort and Safety

Prioritise the comfort and safety of users in the Precinct providing shade, protection and shelter year-round, ensuring a positive experience for pedestrians, cyclists and those using the footpath in other ways.



#### Sustainable Drainage

Deliver integrated Water Sensitive Urban Design and integrate with urban water cycles throughout the precinct.



#### Space Allocation

Provide adequate space for each form of movement and activity needed along the street. Streets are crucial parts of the public domain and create opportunities for activation and inhabitation of the street. i.e. outdoor dining, parklets etc.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### Maintain Landmark Viewsheds

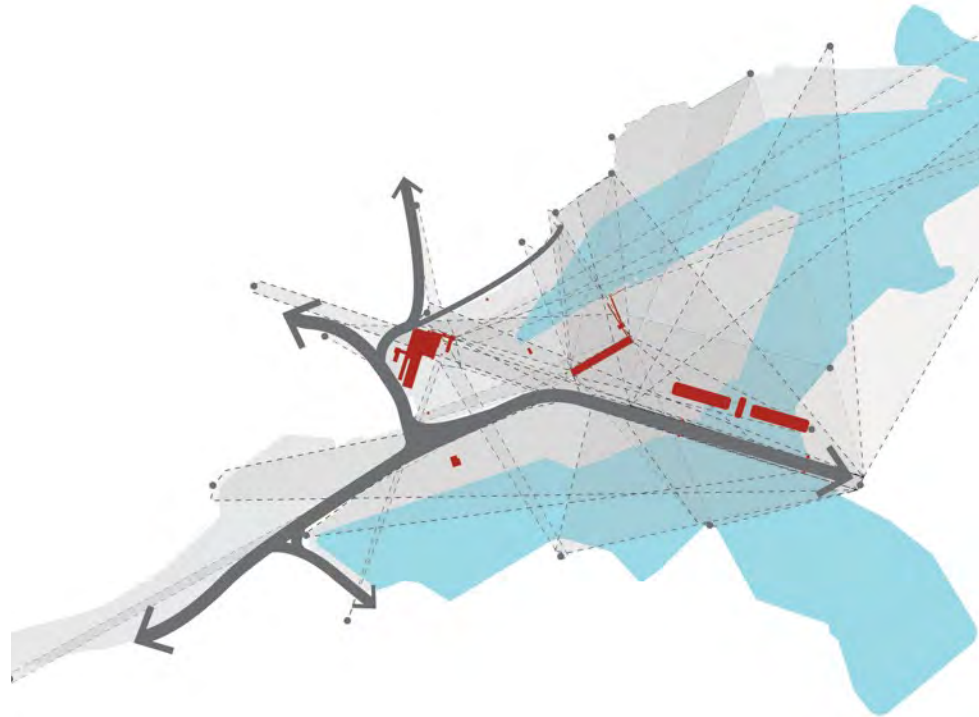
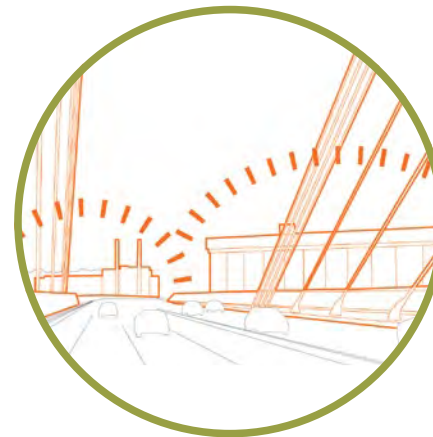


DIAGRAM 2.14 Maintain Landmark Viewsheds

*Maintain existing signature views to WBPS, Glebe Island Silos and Anzac Bridge as the key built landmarks of this Country and signify its industrial, maritime and infrastructural role. These views are key to the deep connection between the sites its immediate neighbourhoods, surrounding district and the broader city in terms of navigation, stories, memory and identity.*



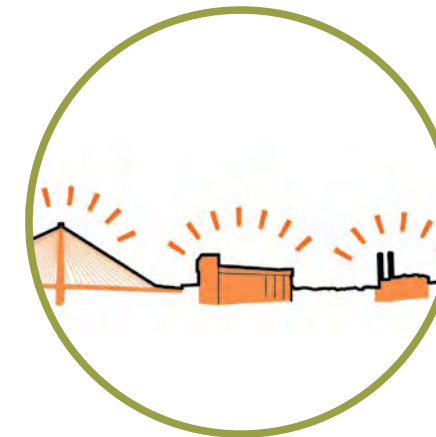
#### Gateway Sequence

Retain the role of the three major landmarks as visual markers of the gateway sequence between the Inner West and Pyrmont/ CBD by enabling these to be experienced together. Protect and retain important views along major movement corridors and consider new buildings impacts within this experience.



#### Prioritise Public Views

Prioritise the retention of public views to the precinct landmarks as well known and iconic features of the Bays West landscape. Public views to them from inside and outside the precinct must be maintained.



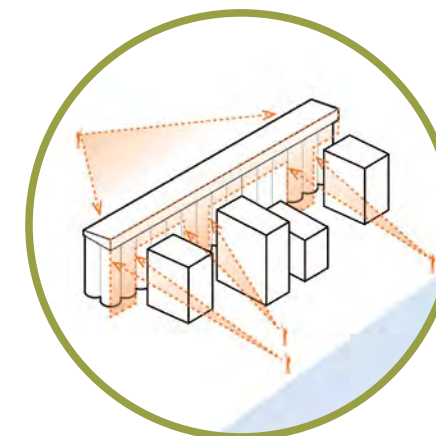
#### District Views

Protect the district views of landmark features which shift and change along public movement and open space areas. Retain their contribution to the place character and display symbolic wayfinding of the Sydney skyline. Protect the district views in line with the detailed viewshed studies and recommendations.



#### Local Views

Utilise views from surrounding suburbs to support the preservation of the character and grain of the place. Protect the local views in line with the detailed viewshed studies and recommendations.



#### Precinct Views

Preserve internal views to landmarks to maintain the clarity of these elements within the Precinct. Provide understanding of the size, scale, texture and materiality of these elements when viewed from within the precinct.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### Care with Built Form

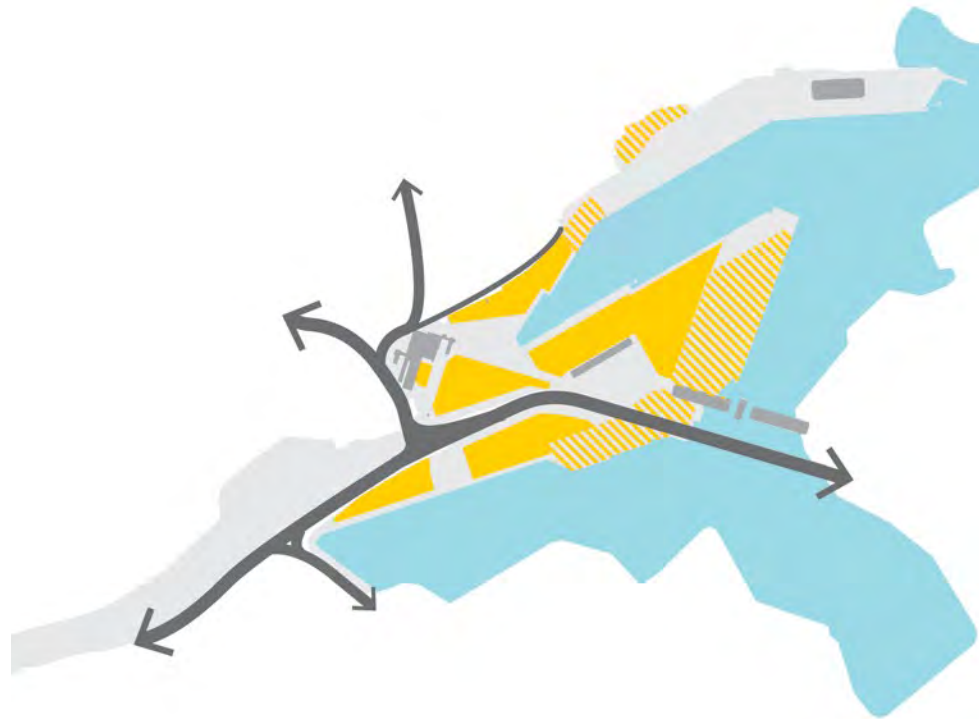


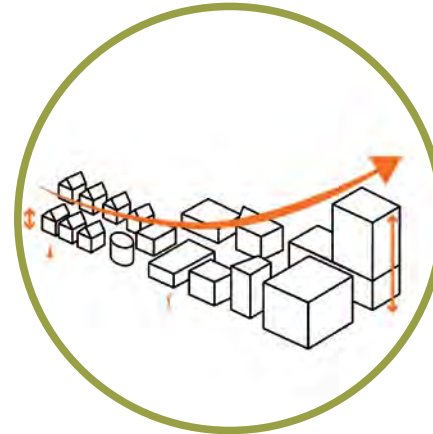
DIAGRAM 2.15 Care with Built Form

*Deliver a built form outcome at Bays West that harmonises with Country and ensures its suitability within its setting. Consider optimal built form outcomes that are responsive to the place and contribute to the human experience of the precinct and the broader city quality as a whole.*



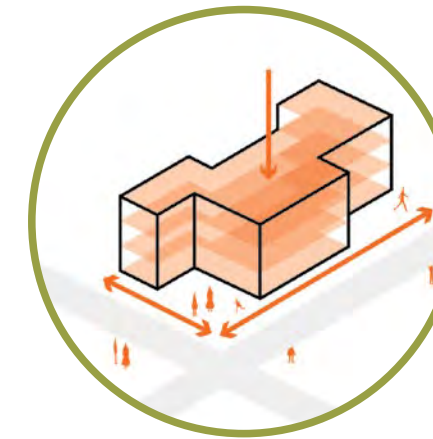
#### Density Done Well

Capitalise on the Precinct's strategic location and access to alternative modes of transport to deliver compact development with exceptional levels of urban amenity.



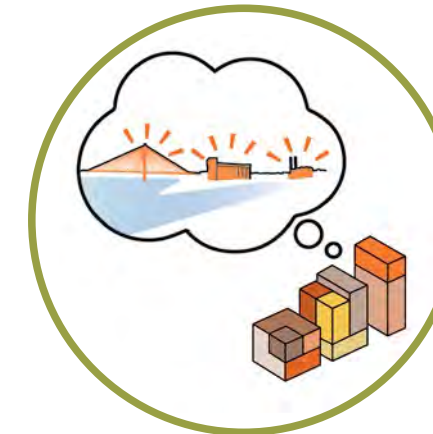
#### Responsive

Integrate new built form within the current and evolving context to ensure the suitability of proposals to its setting. Create an appropriate relationship between the new built form with the surrounding low scale suburbs and sensitively treat the transition to these.



#### Grain

Ensure that building bulk, mass, height and scale considers both the location within the precinct and relationship with the surroundings. Provide an appropriate human-scaled experience and contribute to a walkable Precinct.



#### Materiality

Consider built form materiality relative to the place context which combines the post-industrial attributes with stunning harbourfront. This uniqueness must be captured in the materiality which tells the story of the site's past and future but does not compete with these landmarks.



# Design of Places & Spaces

## 2.2 PRINCIPLES

### Appropriate Distribution of Height

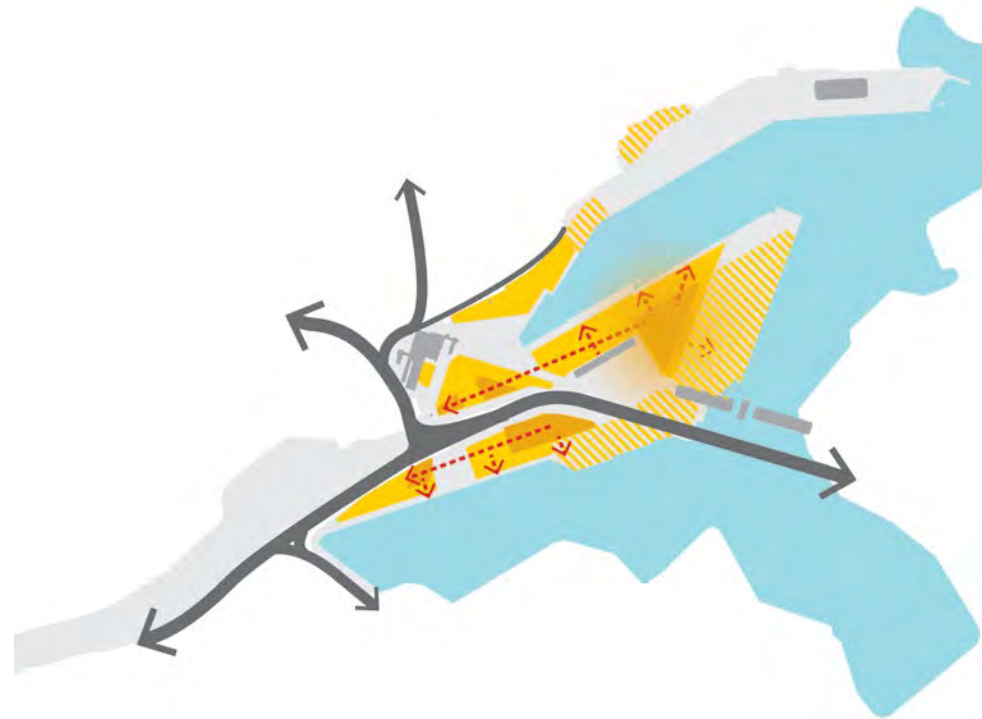
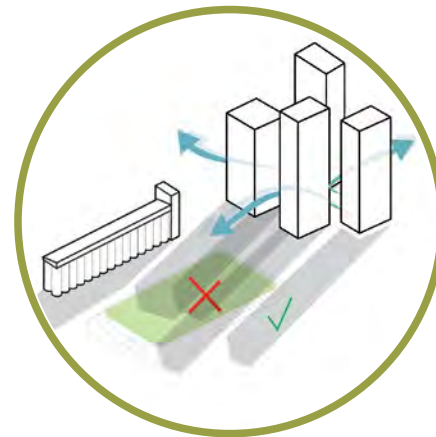


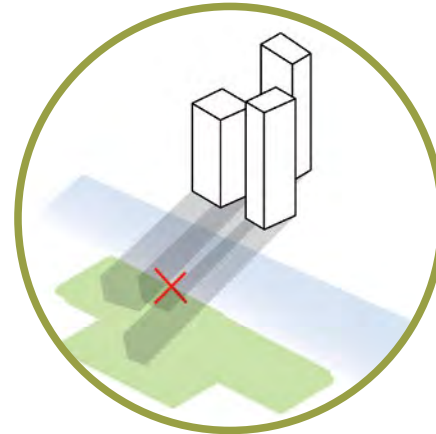
DIAGRAM 2.17 Appropriate Distribution of Height

*Position height and density at Bays West in a way that retains visual connections to Country whilst balancing strategic uses. Large-scale built form must be placed appropriately, and its physical and visual impact from within and outside the precinct must be considered.*



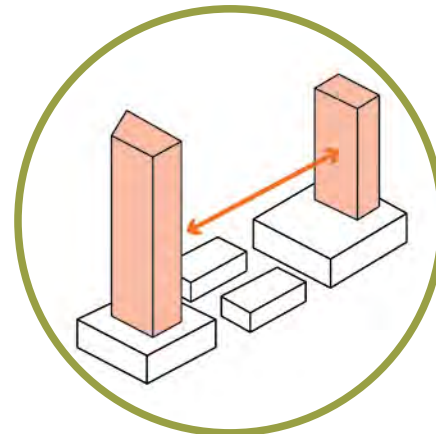
#### Amenity Impacts

Ensure built form is shaped to protect the amenity of the adjacent low-rise suburbs, the water along with the existing and new foreshore and public domain.



#### Locating Height

Locate height in the most appropriate zones and transition it across the site in a logical manner. Utilise elements of built form to assist as a buffer from noise and emissions created by arterial roads and ports and working harbour operations.



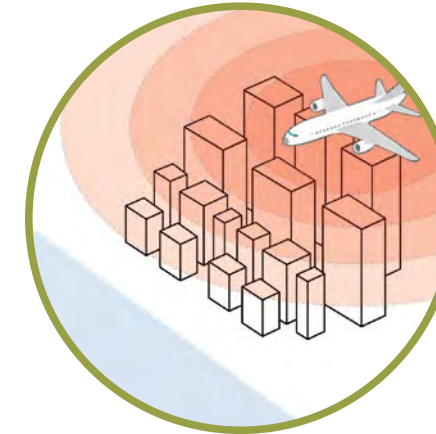
#### Separation

Provide appropriate separation between taller buildings to elements and avoid negative impacts on the pedestrian experience of streets and public spaces (induced winds, overshadowing). Also avoid the visual impacts of large walls of taller buildings.



#### Shaping The Skyline

Create a new skyline for Sydney that works as a unified whole and is sympathetic to its surroundings. Ensure the precinct landmarks to have space to breathe and remain features as part of its composition.



#### Controls

Ensure that known controls are considered and complied with as part of the holistic and place driven built form response. Known controls include the White Bay Power Station Conservation Management Plan and airspace protection regulations. Future controls will be defined through the rezoning process.



# Design of Places & Spaces

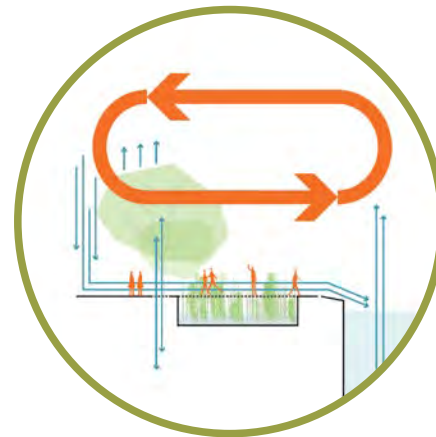
## 2.2 PRINCIPLES

### Rebalancing Natural Systems



DIAGRAM 2.18 Rebalancing natural systems

*Intentionally transform Bays West into a place where Country is once again in balance with natural systems and restored to a healthy state via a proactive approach. Repair past damage and create a healthy ecosystem that will have long-term positive influences on health, wellbeing and quality of life.*



#### Water Systems

Prioritise the naturalisation of water systems at Bays West. Reinstating natural water systems must be considered alongside capturing, treating and improving water health across the precinct. Meet ambitious targets in relation to water management and the preservation of non-renewable water sources.



#### Habitat Creation & Corridors

Provide habitats for animals, insects and sea life to thrive, and promote green ecology and biodiversity throughout the precinct. Connect new habitats to surrounding areas, creating corridors that do not stop at the site boundary.



#### Biophilic Environment

Establish a biophilic environment at Bays West that creates a strong connection between the thriving natural systems and the people who engage with it. The natural environment that is fostered at Bays West will contribute to its unique place character.



#### Restorative and Regenerative

Demonstrate an understanding and care for Country by restoring and regenerating complete local indigenous ecologies within the precinct.



#### Repair Damage

Repair the damage that has been caused throughout its long industrial history and integrate this as a learning experience. Reparation to this place will see quick and significant value in the improvement of the place, starting with the treatment and containment of contaminants and supported green initiatives.



# Design of Places & Spaces

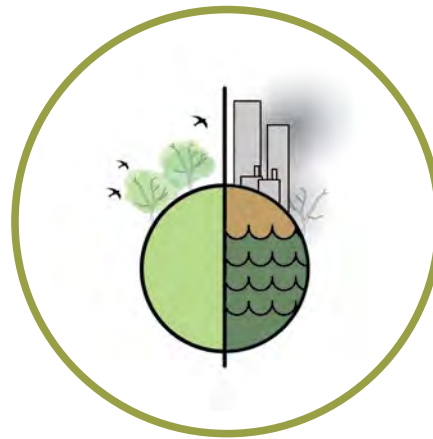
## 2.2 PRINCIPLES

### Climate Responsive & Sustainability Performance



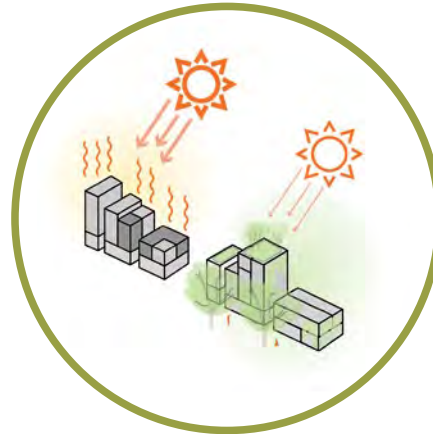
DIAGRAM 2.19 Climate Responsive and Sustainability Performance

*Establish an ambitious and evolving approach to the precinct design, embedding climate responsiveness and sustainability performance. Ensure care for Country in the adaptation of climate change in relation to both built form and public domain. Bays West will be in line with world's best practice which will contribute towards precinct competitiveness.*



#### Climate Risk and Resilience

Design to mitigate chronic stresses and acute climate risks particularly those that are critical due to the relationship with water. Plan for expected and evolving scenarios and design place-responsive solutions, including the intersecting flooding, storm surges and sea level rise.



#### Urban Heat Mitigation

Provide urban cooling through systems-based approaches that utilise both natural systems and materials-based approaches. Include canopy cover, water and air flow, cool building materials and approaches which increase the permeability and minimise amounts of hard ground finishes.



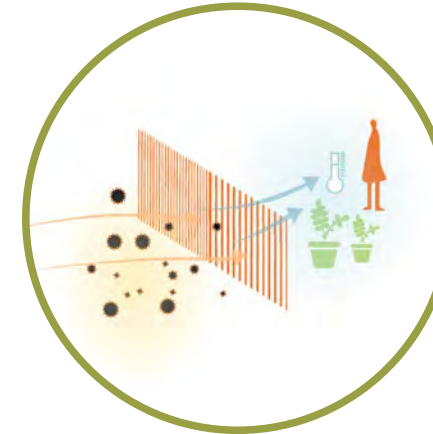
#### GHG Emissions & Energy

Target net zero emissions across energy, greenhouse gas and other port related emissions as an early ambition of the Bays West precinct in both construction and operation.



#### Circular Economy, Supply Chain & Materials

Establish processes and systems which are restorative and regenerative to eradicate the end-of-life concept. In doing so, work to design out waste, keep produce and materials in use in a closed loop with minimal finite resource inputs. Work in harmony with natural systems.



#### Indoor Environmental Quality

Provide high quality environments within buildings which address light, acoustics, thermal comfort and other best practice wellness approaches to support the experience of the users and contribute to liveability and productivity.



#### Outdoor air quality

Improve the outdoor air quality at Bays West through large-scale urban greening, the introduction of new public domain spaces and green streets. Innovative buffer zones can protect or filter air may be required at thresholds adjacent to arterial roads.



# Transport & Movement

## 2.2 PRINCIPLES

### Prioritised & Appealing Networks

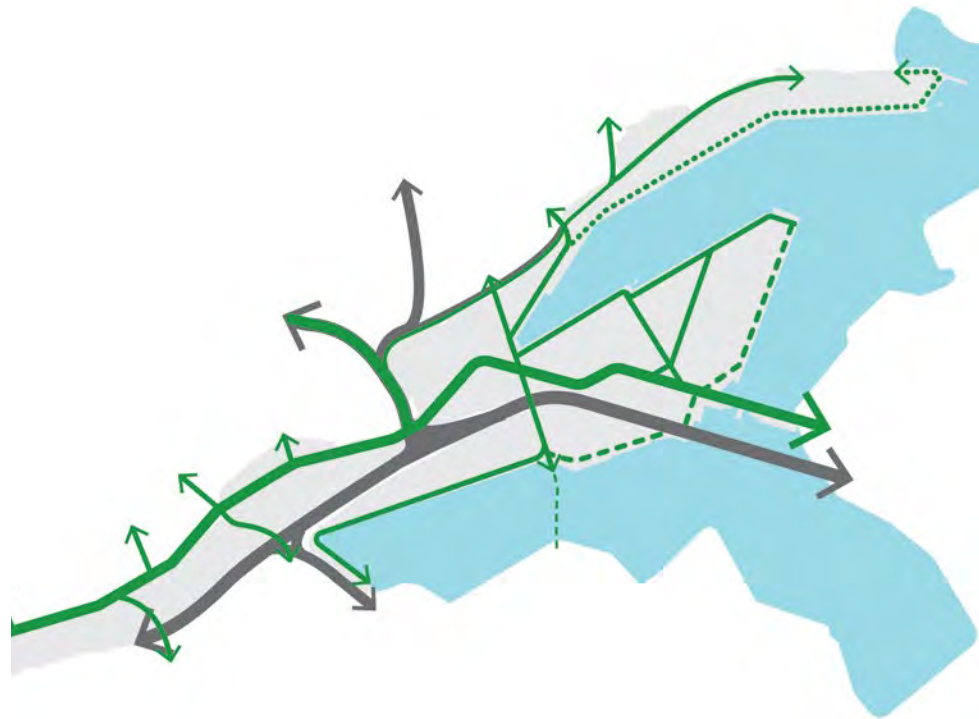
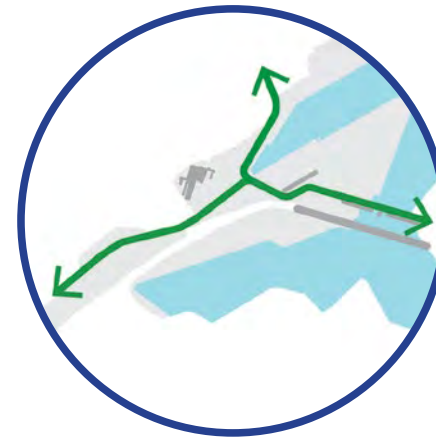


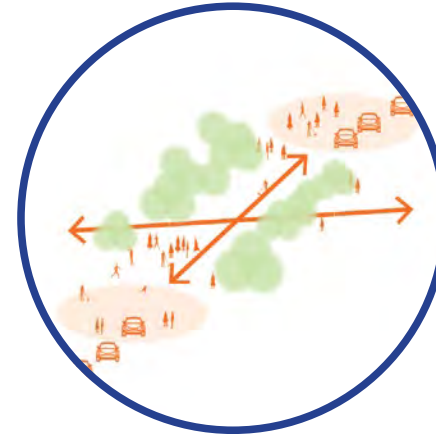
DIAGRAM 2.20 Prioritised and Appealing Networks

*Bays West will set a new benchmark for Sydney – a highly walkable and ‘cyclable’ place focused on human movement, not vehicle movement. Bays West will become a new fully accessible neighbourhood centre that enables the high mode share target for walking and cycling.*



#### Bicycle Highway

Connect to the regional cycle network with a ‘bicycle highway’ that links the Inner West with Pyrmont, through the precinct, and along its main heritage and cultural spine to experience many of the place features.



#### Direct Connections

Provide direct connections within Bays West, linking origin and destination in a cohesive manner. This network should also provide seamless access to public transport.



#### Attractive

Walking and cycling will be the most desirable option at Bays West. Appealing and attractive infrastructure that limits interaction with vehicle traffic, delays and extra exertion due to height and length changes, must be created.



#### Using Place to Navigate

Design the movement system to integrate landmarks and landscape features (water, landform) to aid with wayfinding. Celebrate the sequence of various intermittent views available along travel paths.



#### Prioritising Health & Wellbeing

The safety of pedestrians and cyclists at Bays West must be prioritised within the network, promoting not only efficient transport solutions, but the positive health and wellbeing benefits of using this mode.



# Transport & Movement

## 2.2 PRINCIPLES

### Leveraging & Supporting Metro

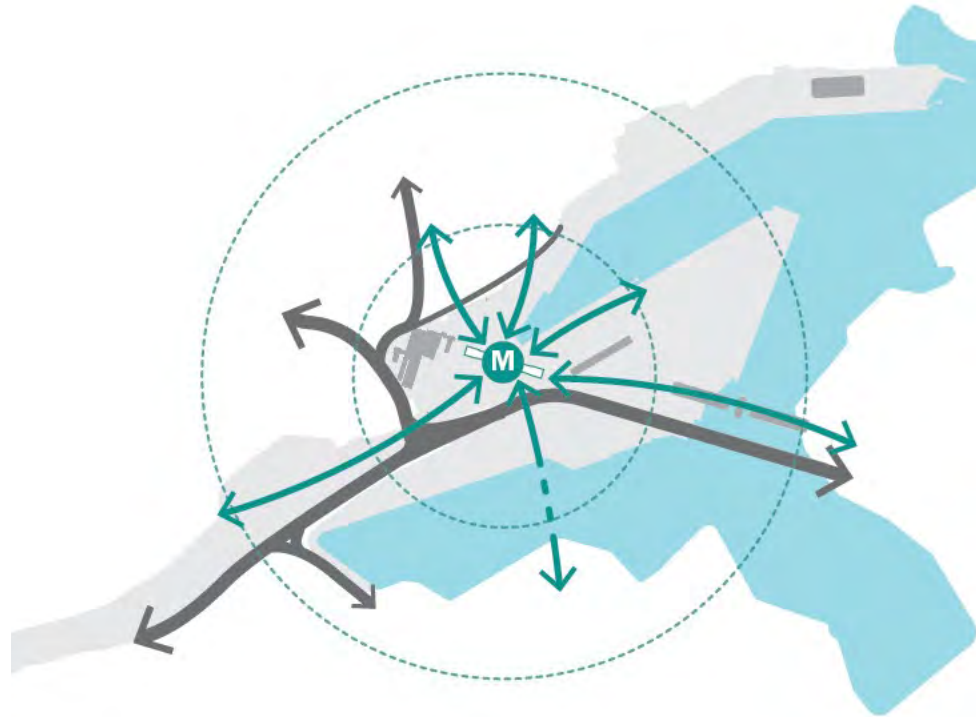
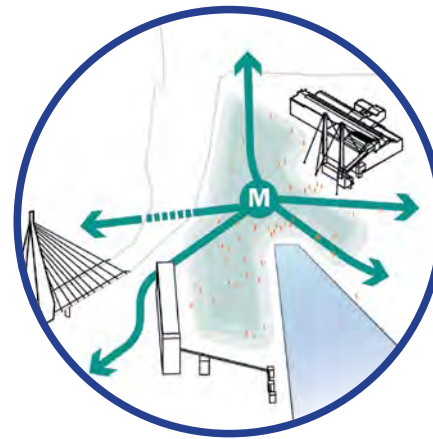


DIAGRAM 2.21 Leveraging and Supporting Metro

*The introduction of Metro to the Bays West site has the potential to not only unlock the immediate site boundary and create links to surrounding suburbs, but create connections to economic centres such as Sydney CBD and Parramatta.*



#### Optimise Access

Create legible and direct connections to expand the catchment across and beyond the Precinct into surrounding suburbs to further widen the catchment area.



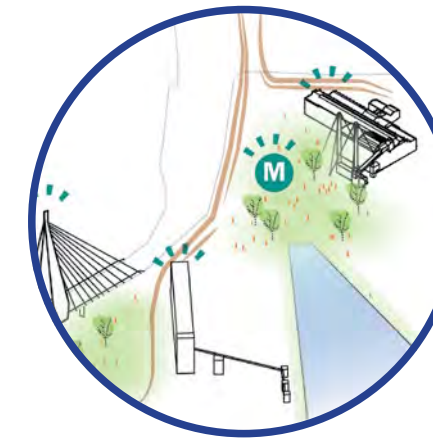
#### Smooth Interchange

Establish an efficient and legible interchange experience between transport modes. Direct and logical connections, supported by clear wayfinding, will contribute to integration of multiple transport networks. Locate bus interchange near to key destinations, but in a location that does not disrupt key desire lines, activation opportunities.



#### Connection

Create connections via the supporting public transport networks (buses and potentially ferries) to cater for movement along other routes. The success of an ambitious mode share to public transport will rely upon an integrated and efficient network of connections.



#### In Keeping With Place

Ensure the design of the metro station, including service buildings and BOH facilities, are well integrated and positively contribute to the amenity and character of the precinct.



#### An Activator

Design the metro station and associated public domain consistent with its key nodal role and generation of significant foot traffic. Ensure the design contributes to the activation of the precinct and creates anchor a hub of activity for the community.



# Transport & Movement

## 2.2 PRINCIPLES

### Low Car Dependency

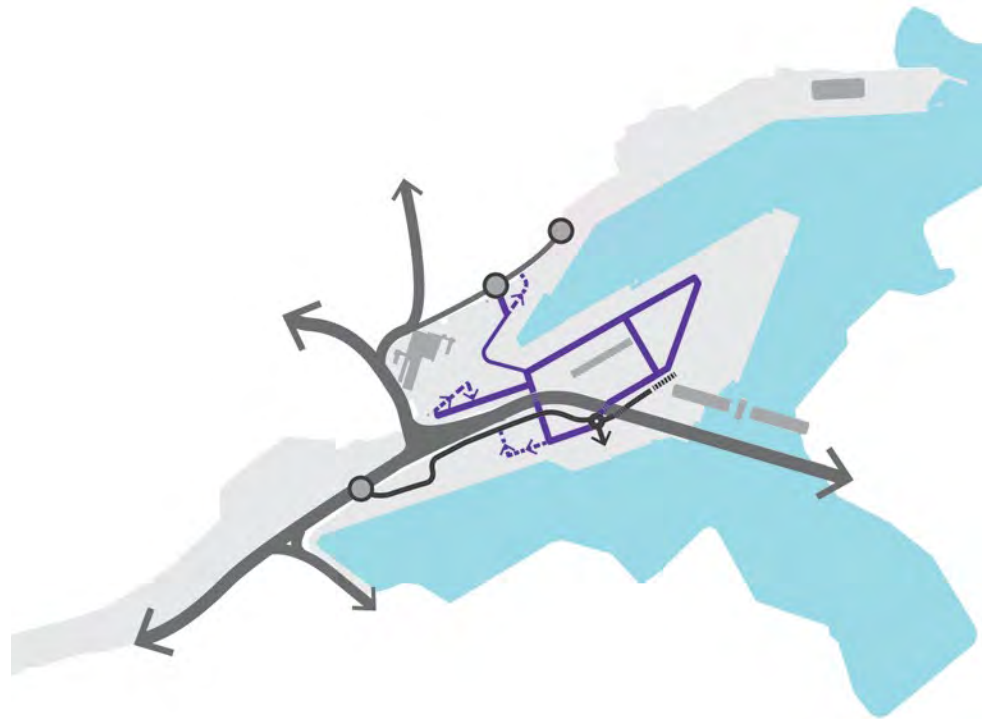


DIAGRAM 2.22 Low Car Dependency

*Bays West is to unlock its local and regional connectivity through the use of active and public transport without reliance on an already-constrained road network. Bays West has ambitious targets to reduce private vehicle use and holistic precinct approaches are needed to deliver these.*



#### Grades of Access

Prioritise vehicular access for those needing mobility assistance along with ports and working harbour and industrial industries as required. Ensure vehicle access is optimised and in locations that minimise the impact on the public domain space and movement of pedestrians and cyclists.



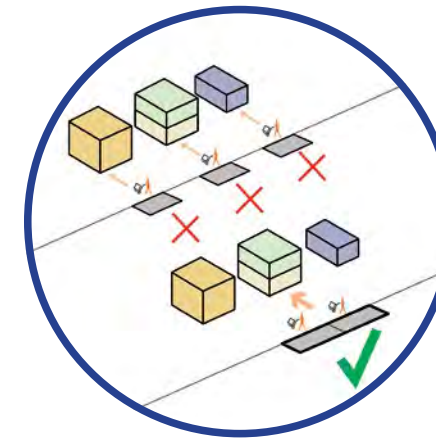
#### Limited

Support reduced car reliance and discourage the use of private vehicle where not necessary via appropriate limitations and management controls. Incentivising alternative travel choice options through network design and promotion of behaviour change.



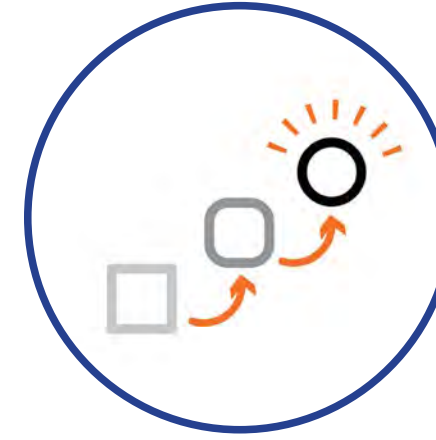
#### Protecting Place

Enable Bays West to become a people-centric place, focused on place quality by limiting vehicle impacts within higher value place areas such as Precinct centre and open space areas.



#### Minimising Impact

Ensure vehicle movements necessary for commercial, retail, delivery or other servicing and ancillary uses have limited impacts on high value places.



#### Transition

Promote low vehicle use culture from the initial phases of the project and ensure it will normalise over a period of time while the precinct evolves, and while the user and worker population adapt to the shift in transport options.



# Transport & Movement

## 2.2 PRINCIPLES

### Forward Looking & Sustainable

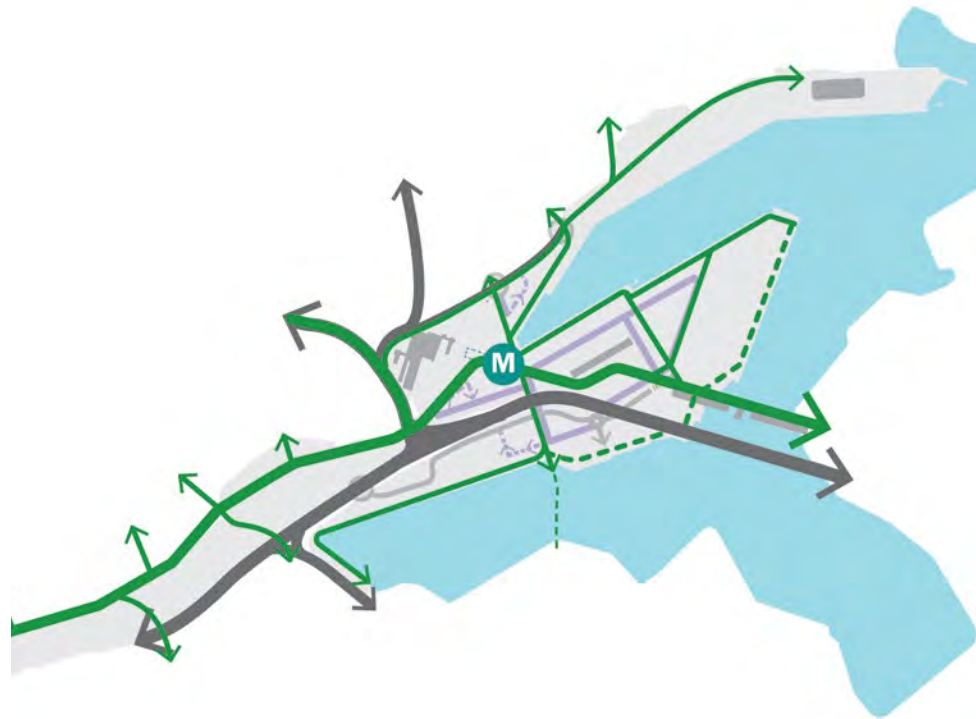


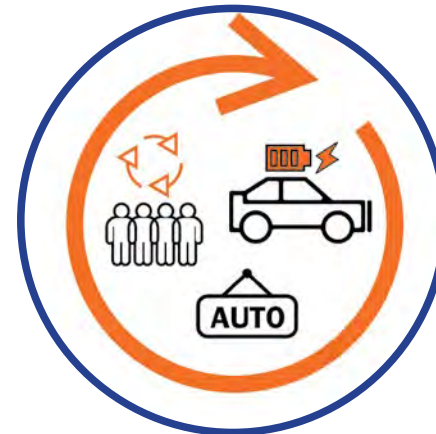
DIAGRAM 2.23 Forward Looking and Sustainable

*Plan for the future of Bays West mobility with flexibility in infrastructure and allowance for a shift in technology. Bays West will continue to evolve and the precinct will continue to strive for optimal sustainability and mobility outcomes in an ongoing manner.*



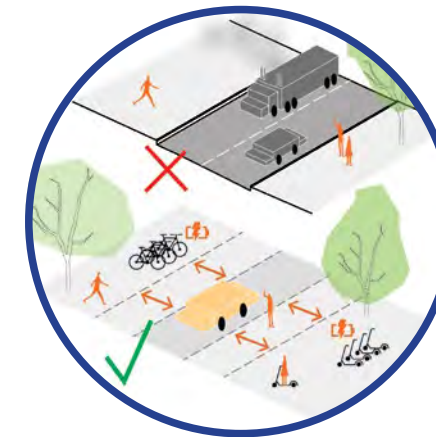
#### Mobility shift

Embrace alternative and shared forms of mobility, enabling vehicle sharing, ride-sharing and connected and autonomous vehicle interfaces via infrastructure provisions. The transition to shared services will be important in supporting a large resident and worker population within the precinct.



#### Technology Trends

Offer the electrification of road mobility options in the precinct design at Bays West. Include charging capacity, optimal data connectivity, the electrification of logistics services, and the ability to adapt to new and emerging technologies over time.



#### Avoid Redundant Infrastructure

Keep up with the constantly evolving data infrastructure and technology associated with vehicle use by considering flexibility in street and utilities infrastructure to allow for new and emerging technologies in the future.



# Heritage & Culture

## 2.2 PRINCIPLES

### Reinforced Heritage & Cultural Spine

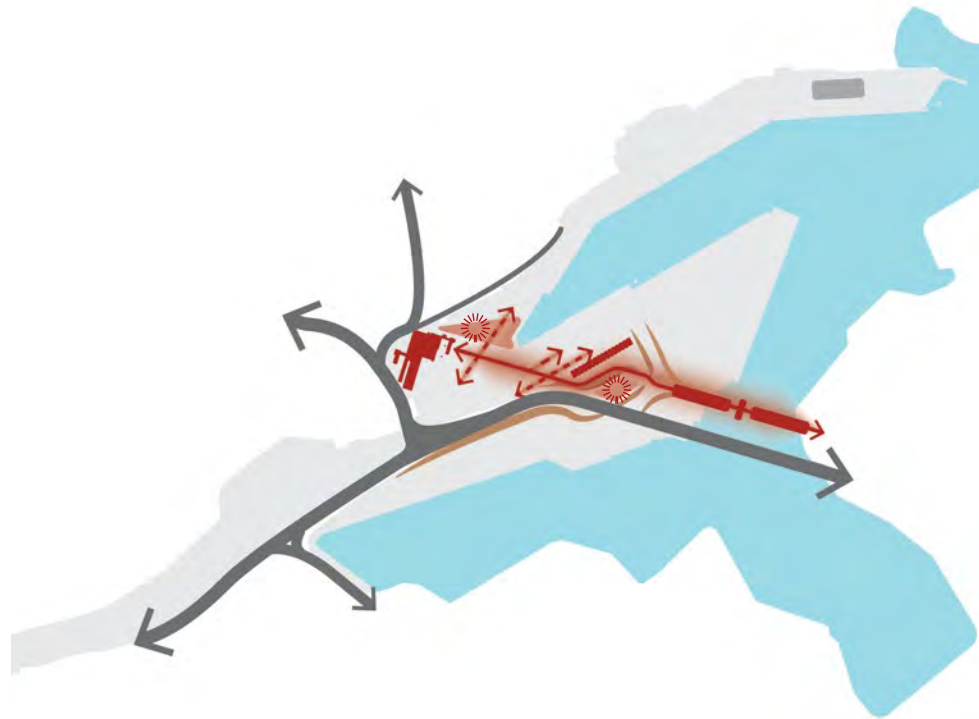
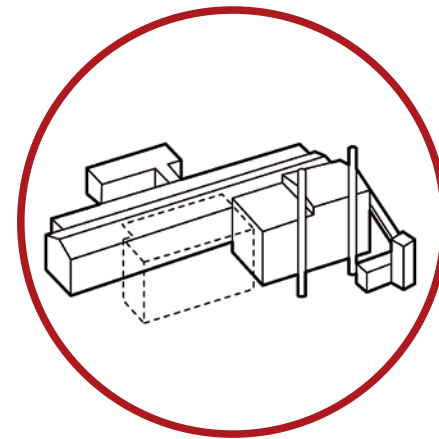


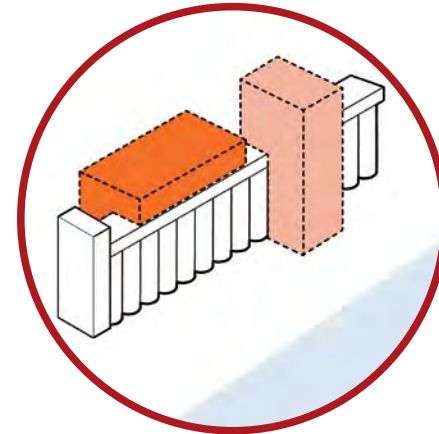
DIAGRAM 2.24 Reinforced Heritage and Cultural Spine

*The spine will anchor the unique heritage and culture at Bays West and provides access to many layers of Country and the history, knowledge and culture of this place. It will integrate with other elements beyond the precinct and within it provides connection between heritage assets, the public places that surround them and finer grain networks and links.*



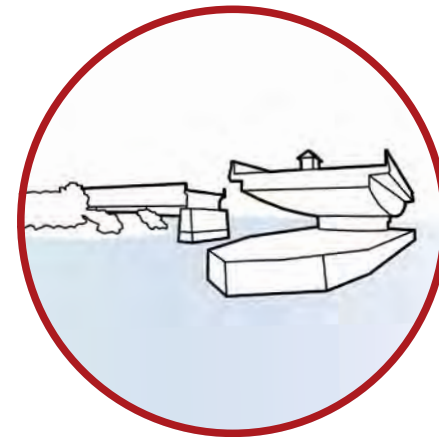
#### Our White Bay Power Station

The White Bay Power Station anchors one end of the heritage and cultural spine and is one of the most unique and celebrated assets at Bays West. It must be adaptively reused and reimagined as a focal point of the Precinct.



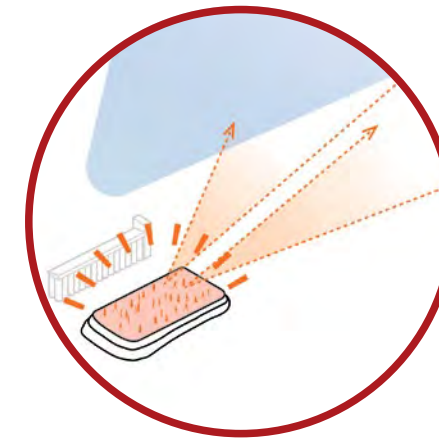
#### Silo Transformation

Consider maximising the long-term opportunities to transform the Silos into an incredible landmark for the Precinct, still linked to the foreshore and integrated into the public domain network. This includes opportunities to support a variety of uses, including ongoing working harbour requirement, and remaining a signifier of the maritime history of the site.



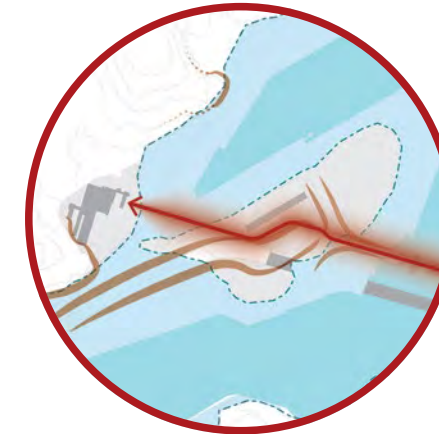
#### Re-Building Bridges

The Glebe Island Bridge is perfectly positioned as a signifier of the maritime and transport history and innovation at this site that future site users can engage with as they arrive or leave the site. As a unit with Anzac Bridge it highlights the changes in transport access that has occurred.



#### A Place of Ceremony

Enable Bays West to become a place of cultural sharing, truth telling and ceremony. Integrate a place for indigenous cultural practices at the top of Glebe Island (potentially referred to as Buhrdi Mana park which means shell collections) that will be recognised, celebrated and linked into the spine and the broader songlines of the site.



#### Natural Elements

Reveal elements including the head of the bay, the foreshore and deep-water harbour, and the sandstone cuttings that express many stories of the site. The natural features of the site should form part of the experience of this spine.



#### Networks & Links

Connected to the major spine and beyond offer a network of places and features that can be integrated with this backbone. These can celebrate Country, art and cultural offerings, other adaptively reused elements, stories, and a variety of public domain spaces.



# Heritage & Culture

## 2.2 PRINCIPLES

### The People's Assets

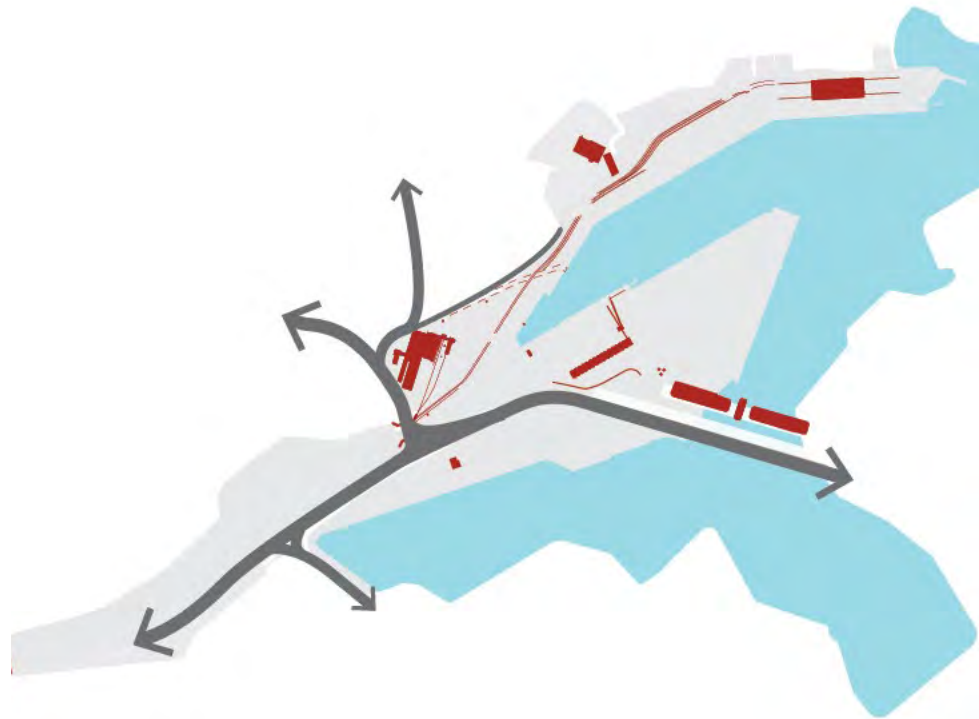
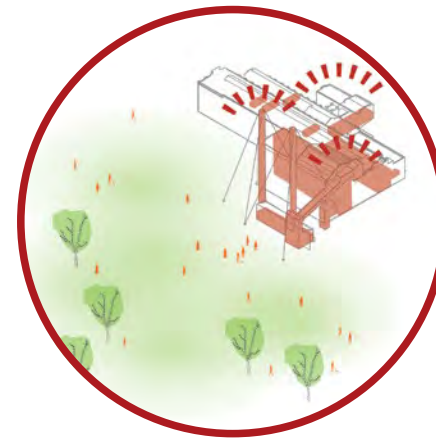


DIAGRAM 2.25 The People's Assets

*The Bays West landmarks form part of the living narrative of the place allowing community attachment and positive association to the place. These are iconic elements that have the potential to unlock the future of the site, engage the community and define the future character of Bays West by opening significant features of these landmarks to the public and integrating them into the land use and activity of Bays West.*



#### Public Access to Significant Features

Provide public access to significant parts of Bays West landmarks that define its character and enable community engagement. Ensure that the public experiences are around the role and significance of these elements and integrate with any interpretation framework.



#### Public Domain Integration

Integrate key public domain spaces in and around significant heritage landmarks and ensure public access. High-quality, welcoming and accessible spaces that enable engagement with and access to the rich cultural elements of the site, will provide great benefit to the public.



#### Public Offering

Create a public offering within the significant parts and around significant heritage assets at Bays West to enable social, cultural and community benefit. Part of the heritage assets should consider public access and support for creative and cultural institutions.



# Heritage & Culture

## 2.2 PRINCIPLES

### A Living Narrative



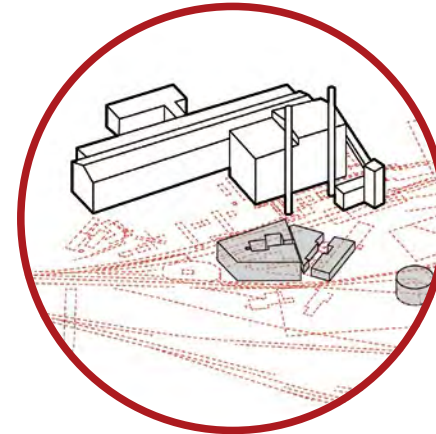
DIAGRAM 2.26 A Living Narrative

*Consideration and care for Country including all of the remnants and layers of heritage and sandstone is integral to the future of the precinct. Bays West will be a place where site (hi)stories are made clear and can be well understood, and that language and cultural expression is shared and valued.*



#### Starting With Country

Country must be considered as a fundamental of the precinct design from the beginning, through consultation, delivery, care, recognition and engagement with Indigenous culture and language, strengthening the connection with place.



#### Revealing Layers

Embed interpretation and public art in the public domain and built form which will reveal the layers of history in a coordinated way. Express existing and former elements, sub-surface elements and other tangible traces of the past within the precinct structures.



#### Telling Stories

Deliver a site that integrates, interprets and conserves the wider site heritage elements into a cohesive story that can be understood by future users. Use digital platforms, integrated signage, artwork and remnant artefacts to illustrate the stories across the site.



#### Cultural Expression

Creative culture creates richness, interest, vibrancy and identity in place. Build upon the Indigenous culture of the place, combined with its industrial and maritime history, and create places to enact culture as part of an evolving identity for the precinct.



#### Beyond the Site

Connect to heritage elements and areas within and beyond the site. Provide benefit from integration and connection, establishing heritage, creative and cultural networks and connecting places of heritage significance.



# Heritage & Culture

## 2.2 PRINCIPLES

### Informing Place Character

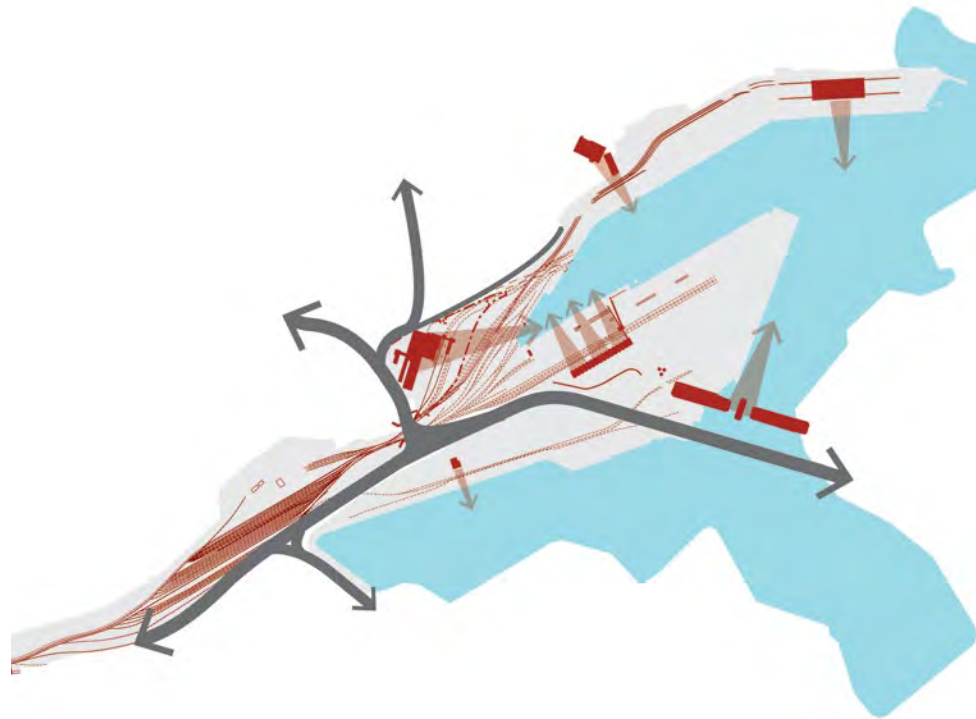
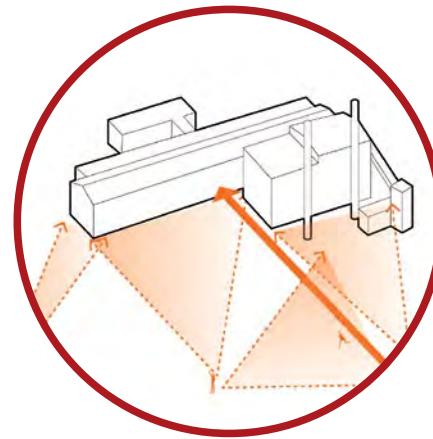


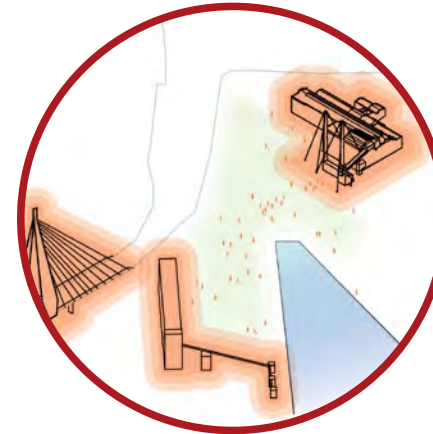
DIAGRAM 2.27 Informing Place Character

*Celebrate this Country and the history and heritage in a way which preserves place character and recognising the uniqueness of this place, will in turn foster its sense of place, its desirability and liveability.*



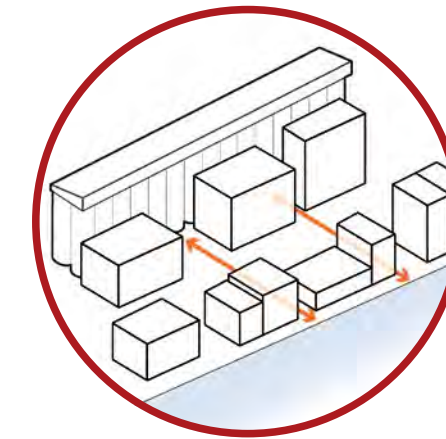
#### Setting & Curtilage

Consider the appropriate setting and curtilages for heritage items and possibilities in relation to access and use. The setting of the power station and other heritage assets, and their integration in the public domain, has significant impact on the place character. Respect the setting and curtilage of heritage landmarks when considering adjacent built form.



#### Character and meaning

Retain the character of the place to strengthen the sense of uniqueness and contribute to the attractiveness of the precinct. New development should acknowledge the historic industrial character of the site, and respond in a sensitive way without mimicry or losing authenticity. Weaving the new with the old will allow users to enjoy the discovery of an intriguing place.



#### Waterfront Connections

Ensure the relationship to the water which each of the heritage elements is retained in a meaningful way. Interpretation of water conduits, channels, heating and cooling systems, dykes and other water systems will contribute to understanding of these layers.



#### Beyond The Heroes

Beyond the landmarks are a series of other unique but smaller, less obvious heritage remnants such as the penstocks, railway tracks, heritage canals, pumping stations and other built form associated with a long maritime and industrial history. Each of these assets can be reused, reinterpreted and revealed to contribute to the overall place character.



# Infrastructure Delivery & Governance

## 2.2 PRINCIPLES

### Integrating Perspectives



DIAGRAM 2.28 Integrating perspectives

*Consider a range of perspectives at Bays West, including those of all communities associated with this Country, to foster the community's long terms engagement with the place by a broad range of users.*



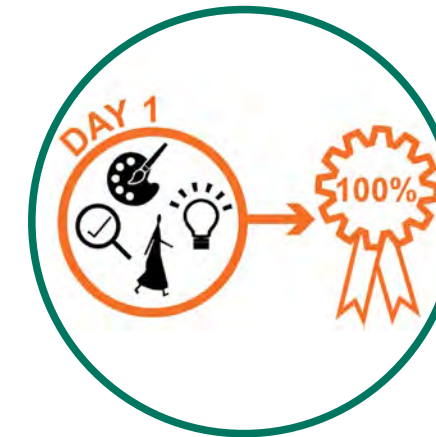
#### Engagement

Engage with existing and future community and stakeholder groups to be informed and improve the overall outcome. An understanding of how people will interact with the site, combined with a sense of belonging and ownership, will encourage engagement with the place long into the future.



#### Participation

The perspective of a range of people including Indigenous groups, existing and future workers, future residents and the surrounding community, will support a deep understanding of the future of the place. Grass roots and community-led initiatives will assist in a range of voices being heard.



#### Early Activation & Temporal Uses

Provide a living lab for ideas in the early phases of site activation at Bays West, where creativity and experimentation will be embraced. Enable public access to be unlocked early to allow users to have greater engagement with the site, long before the commencement or completion of the full redevelopment.



# Infrastructure Delivery & Governance

## 2.2 PRINCIPLES

### Coordinated & Informed Response

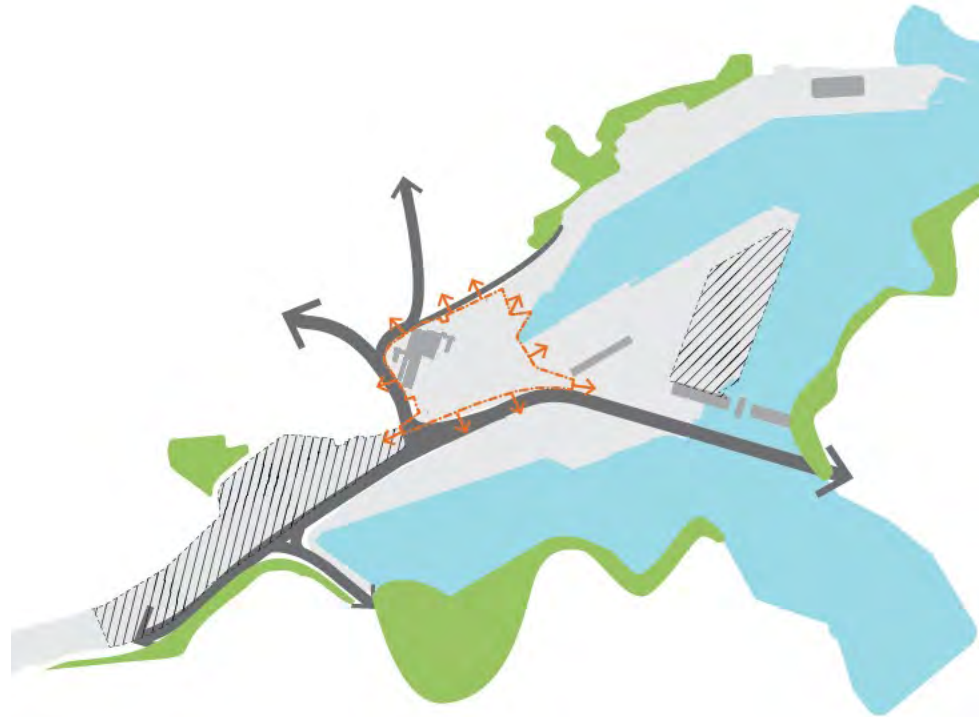
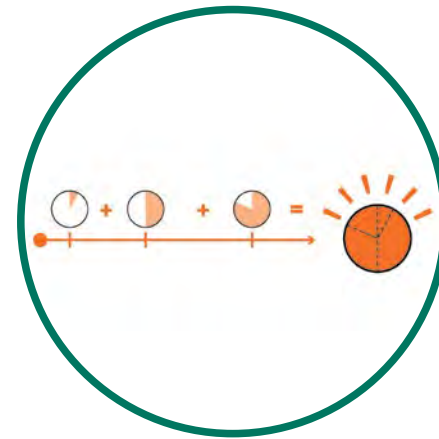


DIAGRAM 2.29 Coordinated and Informed Response

*Ensure that the decision making for the precinct is fully informed including consideration of Country and driven by long-term aspirations and public benefit.*



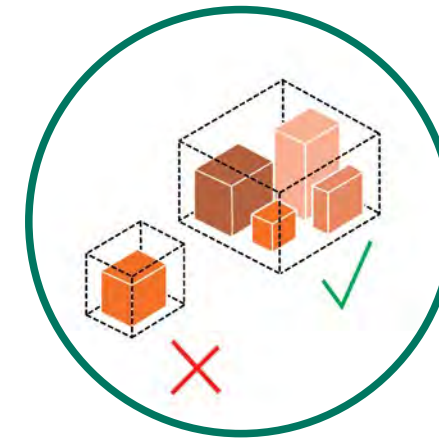
#### Cohesive Goals

Establish and regularly review a cohesive set of agreed goals and targets that are established via studies and strategies and informed by the precinct stakeholders. The end-state desired outcomes for the precinct must be balanced with the short-term and long-term strategic needs of the project.



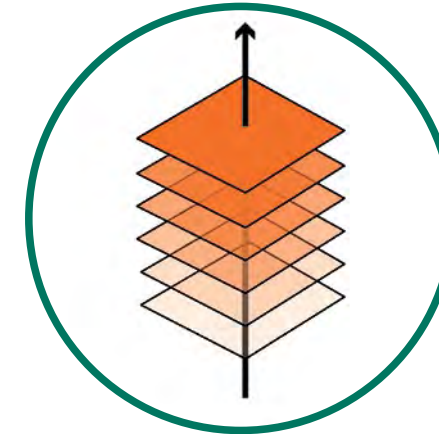
#### Establishing needs

Ensure that the needs assessments across the various forms of infrastructure is undertaken and provisions are built into ongoing development and forward planning.



#### Achieving Efficiencies

Capitalise on opportunities for large-scale infrastructure projects to maximise broader benefits. Seek opportunities to solve multiple issues within larger scale proposals.



#### Planning for Staging

Ensure efficiency in the planning and delivery of future stages, pairing resources and aligning timelines so that resources are optimised. Ensure that decisions relating to precinct coordination are made based on the best outcome, not cost-saving or current restrictions and will ultimately contribute to the long-term aspirations.



# Infrastructure Delivery & Governance

## 2.2 PRINCIPLES

### Establishing & Maintaining Place

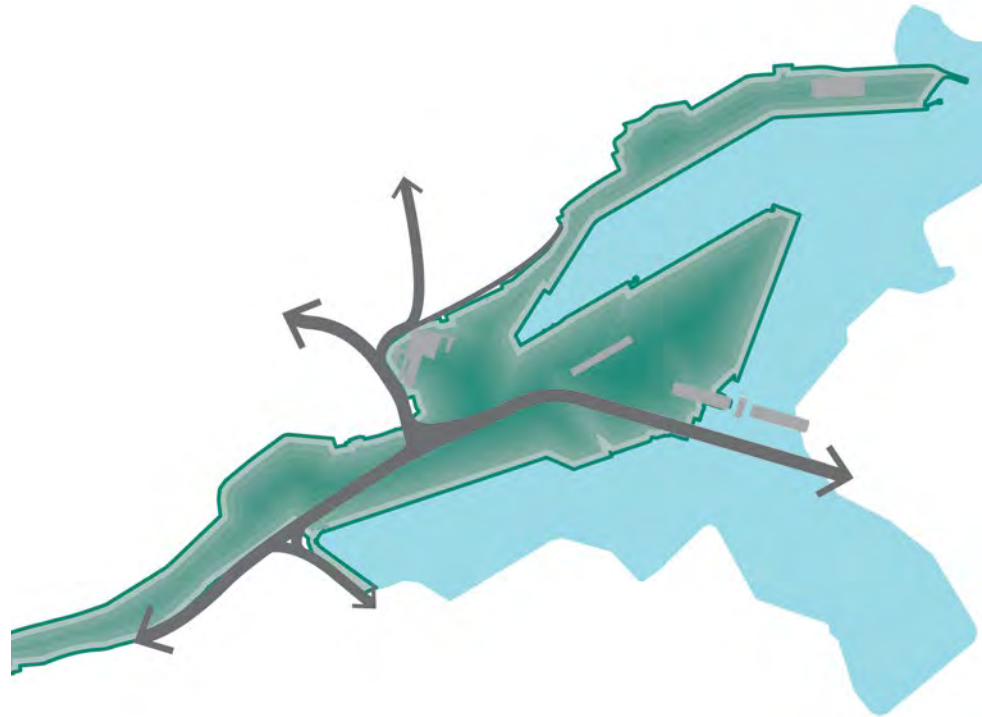


DIAGRAM 2.30 Establishing and Maintaining Place

*Establish appropriate mechanisms and coordination of precinct delivery and management to ensure that Country is restored and protected and public benefits outcomes are maximised.*



#### Competitive and Attractive

Establish a globally attractive brand that is highly magnetic in attracting investment and global talent. Create a place that businesses and investors want to be a part of, a place that fosters talent and will strengthen the NSW and Australian economy.



#### Creating and Retaining Value

Ensure that the innovation ambitions and total investment into Bays West is realised through ongoing management of the place quality to retain and expand on the value created.



#### Precinct Infrastructure and Plans

Develop holistic precinct plans which enact the ambition for a sustainable digital precinct at Bays West and integrate the systems and infrastructure which can catalyse jobs creation and productivity. Utilise these to enhance social and environmental performance and disclosure through emerging digital technology and the internet of things.



#### Assurance

Incorporate assurance at Bays West across the multiple strands to provide stakeholders with confidence of project veracity across all stages of the project life cycle and into the future. Areas for assurance include development and operational governance, digital strategy, and precinct/ built environment certification but others may be identified through the implementation process.



# Infrastructure Delivery & Governance

## 2.2 PRINCIPLES

### Beyond the Boundary

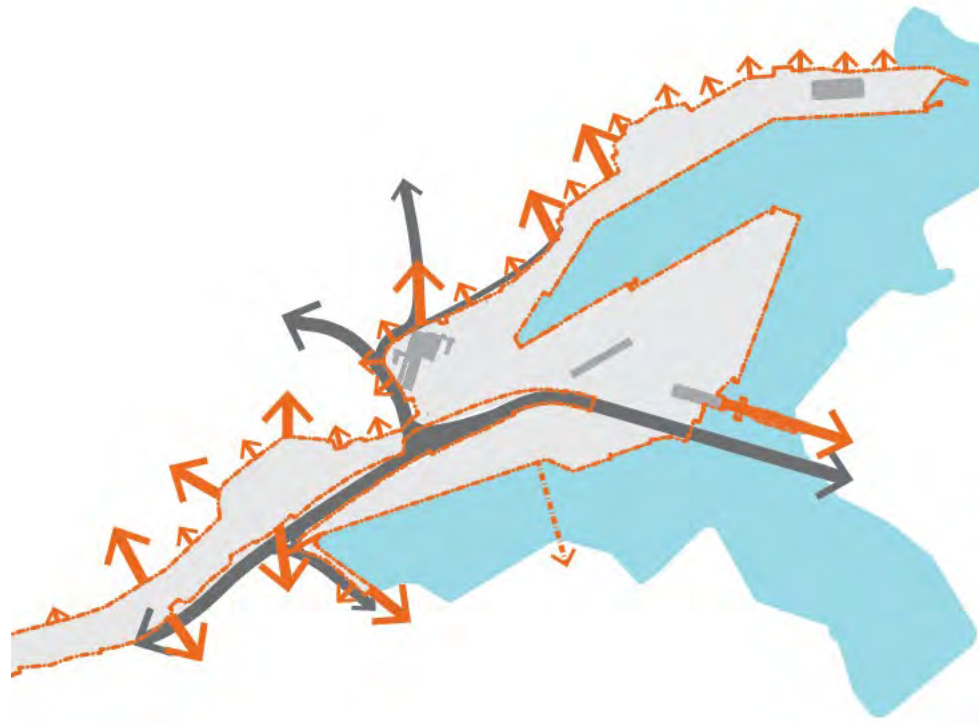


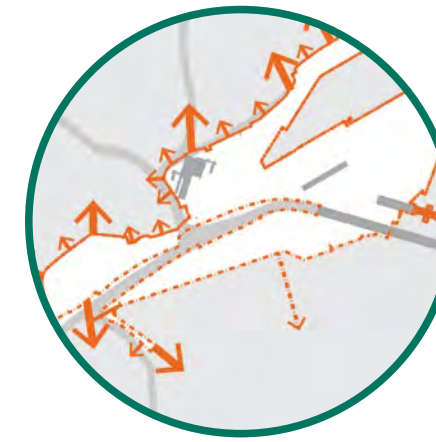
DIAGRAM 2.31 Beyond the Boundary

*Integrate Bays West into its surrounding Country and ensure that the precinct maximises benefits beyond just the physical extents of the precinct via coordination with landowners and other government agencies.*



#### Blended Edges

Coordinate with stakeholders and landowners along the precinct boundary to ensure that opportunities for street upgrades, public domain interfaces/ integration and future plans for key lots/buildings can be considered in a synchronised manner.



#### Realising Benefit Beyond Precinct

Consider where new components of Bays West precinct can also provide substantial benefit to the broader community beyond the precinct and how this may influence alignments and positioning. The benefits that can be achieved through connectivity, open space, recreational and community/social infrastructure should be a particular focus.



DRAFT FOR CONSULTATION

3

## PRECINCT-WIDE SUPPORTING INFORMATION



# 3.1

## INTRODUCTION



# Precinct-wide Overview

## 3.1 INTRODUCTION

### INTRODUCTION

This section will be completed as further detailed investigations into the precinct are undertaken including studies identified in the Bays West Precinct Wide Actions. This may include the following:

#### 3.2 LAND USE

- Employment and Innovation
- Land Use & Capacity
- Land/Water Interface Arrangement

#### 3.3 DESIGN OF PLACES AND SPACES

- Layout & Morphology
- Built Form Parameters
- Viewsheds (key views identified at this early stage are shown at Attachment A)
- Public Domain & Key Spaces

#### 3.4 TRANSPORT AND MOVEMENT

- Movement & Place
- Street Profiles & Character

#### 3.5 HERITAGE & CULTURE

- Response to Country, Interpretation & Heritage
- Cultural facilities

#### 3.6 INFRASTRUCTURE DELIVERY & GOVERNANCE

- Staging and Sequencing

#### 3.7 DESIGN EXCELLENCE

The Bays West Design Excellence Strategy outlines the key design processes, design related activities and policy context that will guide and promote design quality for Bays West. As part of the on-going collaboration between Government Architect NSW and the Department of Planning, Industry and Environment, the Strategy guides NSW Government agencies involved in the precinct’s development and delivery with an overview of the requirements and considerations for attaining design excellence. The Strategy is forward facing, and process-focused, setting expectations for design quality and providing guidance for the on-going development of future detailed design excellence strategies for individual projects and future statutory Design Excellence provisions under the relevant Environmental Planning Instruments.



# 3.2

## LAND USE AND FUNCTION



3.3

## DESIGN OF PLACES AND SPACES



# 3.4

## TRANSPORT & MOVEMENT



3.5

## HERITAGE AND CULTURE



3.6

# INFRASTRUCTURE DELIVERY & GOVERNANCE



3.7

# **BAYS WEST PRECINCT DESIGN EXCELLENCE STRATEGY - GANSW**



# Bays West Precinct - Design Excellence Strategy

*The Government Architect NSW acknowledges the traditional custodians of the land and pays respect to Elders past, present and future. We honour Australian Aboriginal and Torres Strait Islander peoples' primary cultural and spiritual relationships to place and their rich contribution to our society. To that end, all our work seeks to uphold the idea that if we care for Country, it will care for us.*

## 1.0 ABOUT THIS STRATEGY

As part of the *Draft Urban Design Framework*, Government Architect NSW (GANSW) provides the *Bays West Precinct - Design Excellence Strategy* in line with the on-going collaboration between GANSW and the Department of Planning, Industry and Environment.

Design excellence has been identified in the Bays West strategic planning documents – the *Draft Place Strategy*, *Draft Strategic Place Framework* and *Draft Urban Design Framework* - as a strategic Direction to be promoted in the planning, design and development of the precinct.

*“DIRECTION - Promote design excellence and utilise a people focused approach to deliver high quality built-form and amenity outcomes that respond to the precinct's context”.*

## 1.1 SCOPE OF THIS STRATEGY

The role of the Bays West Precinct - Design Excellence Strategy (DE Strategy), is to promote design excellence and guide good design processes for the planning, development and delivery of the precinct. The DE Strategy addresses the design process at all scales of development and across the various stages of a project's life span - from strategic planning to delivery of individual buildings and public open space. The DE Strategy provides a consistent and integrated approach to design quality for a precinct with complex development pathways, long implementation timeframes and multiple sub-precincts. The DE Strategy is future facing and will develop over time in parallel with the finalisation of the Urban Design Framework.

## 1.2 AUDIENCE AND INTENT

*The DE Strategy, outlines for all stakeholders the key design processes, design related activities and policy context to guide and promote design quality for the precinct. The primary audience is NSW Government agencies involved in the precinct's development and delivery. The DE Strategy provides these agencies with a high-level and process-focused overview of the requirements and considerations for attaining design excellence.*

Through a precinct-wide approach, it is the intent of the DE Strategy to:

- promote design quality, by setting the ambition and expectations for all stakeholders at Bays West
- provide confidence that good design processes are embedded in the NSW Government approach

to the planning, development and delivery of Bays West

- guide NSW Government agencies as they prepare for the development and delivery of the precinct
- provide a basis for future Design Excellence strategies for individual projects across the precinct
- guide future Statutory Design Excellence provisions for Environmental Planning Instruments and assessment study requirements
- promote an integrated approach and on-going collaboration as central to realising design quality.

## 1.3 STRATEGY FOCUS AND GUIDING CONSIDERATIONS

The DE Strategy focuses on process, addressing the mechanisms, activities and considerations that establish and maintain design quality for a project's life cycle. In a NSW Government context, this focus on process establishes 5 inter-related areas as drivers of design quality for the DE Strategy:

1. **project alignment with GANSW design policy and guidance**
2. **Design Governance - NSW Government agencies leading in best practice**
3. **following Statutory Design Excellence requirements**
4. **a 'whole of project' approach to collaboration for planning, design and development**
5. **an integrated approach to the design process.**

## GUIDING CONSIDERATIONS

The following considerations inform the DE Strategy and its approach to design quality:

- **design quality at every project stage**
- **continuity at key project transitions**
- **a cumulative approach to design quality**
- **consistent and proportionate Statutory Design Excellence practices**
- **good design as generator of public value.**

## TERMINOLOGY

*“Design excellence as a term usually describes a requirement or expectation that a project will achieve a level of design quality that is above and beyond the usual.”*

GANSW Advisory Note - Good Design and Design Excellence in the Planning System.

The term design excellence is used to cover both the aspiration for design quality as an outcome (the 'what'), and in a process sense, to address the range of design and design related activities that contribute to attaining a high level of design quality (the 'how'). This DE Strategy, uses the term in both senses, the what and the how.

Where design excellence is used to define a statutory planning requirement it is described in this document as 'Statutory Design Excellence'. The term good design is a reference to its definition within *Better Placed* - the GANSW integrated design policy for the built environment of NSW - but can also be understood in a general usage sense.



# Good Design and it's Supporting Policy

## 2.1 BETTER PLACED - GOOD DESIGN IS NSW GOVERNMENT POLICY

The NSW Planning system established by the *Environmental Planning and Assessment Act 1979*, recognises the importance of design and amenity of the built environment. To encourage and support good design, GANSW has prepared *Better Placed* – an integrated design policy for the built environment of NSW. The policy through its inter-related objectives establishes expectations for good design across built environment projects of all scales - from cities, towns, and precincts to individual buildings. This Design Excellence Strategy is based on the principles and objectives of *Better Placed* and its supporting suite of and documents that guide the design process.

## 2.2 WHAT IS GOOD DESIGN?

Good design is a phrase that encapsulates the aspirations of *Better Placed* including its vision for NSW, its definition of good process, and its objectives for the built environment, by defining its characteristics as healthy, responsive, integrated, equitable and resilient.

## CHARACTERISTICS OF A WELL DESIGNED BUILT ENVIRONMENT



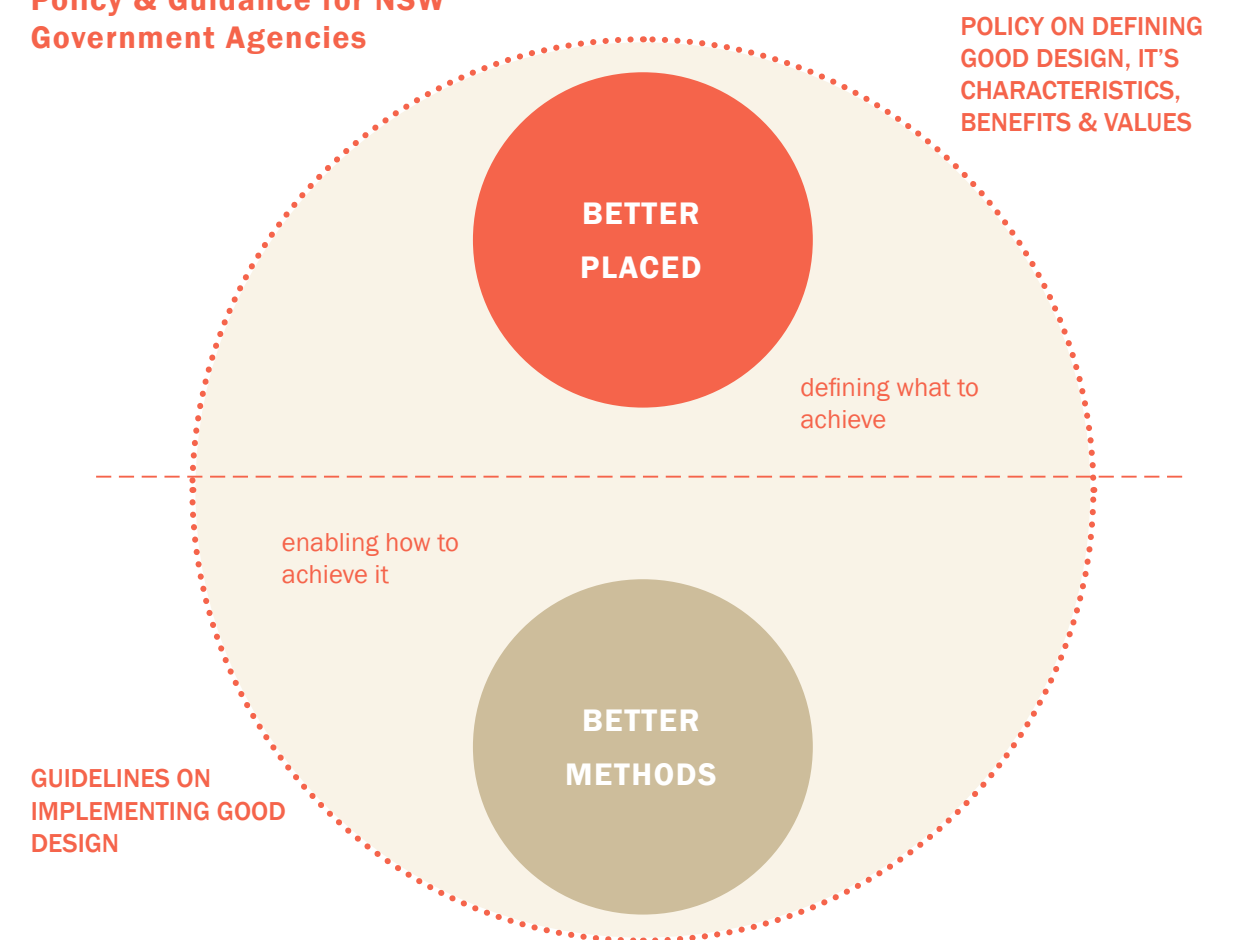
DIAGRAM 3.1 Characteristics of a well design built environment / *Better Placed*, GANSW

## 2.3 THE BETTER PLACED SUITE OF SUPPORTING DOCUMENTS

Along with *Better Placed*, GANSW has also released the following supporting documents specific to the design process:

- The *Better Methods* documents of,**
  - Implementing Good Design*** – this guide focuses on good design process through 3 inter-related activities – *discover, create and deliver*. Applied in a cyclic and iterative manner these activities enable good design outcomes, for example, design concepts becoming better informed and more resolved through application of experience, insight, skills and knowledge from multiple sources.
  - Evaluating Good Design*** - this guide expands on the objectives of *Better Placed*. It enables design evaluation criteria aligned to *Better Placed* to be developed and promotes evaluation as part of a continuous and iterative process.
- Advisory Notes and guidelines** on specific design activities and Statutory Design Excellence processes including:
  - State Design Review Panel - Introduction Brochure*
  - Government Architect's Design Excellence Competition Guidelines* (Draft 2018)
  - How to develop a brief*
  - How to select design consultants*
  - Master planning process*
- Design frameworks and guides** – that outline considerations for integrating *Better Placed* relevant to specific subject matter, including:
  - Greener Places* - an urban green infrastructure design framework
  - Good Urban Design* - an integrated approach that promotes strong governance, shared responsibility, shared vision, and effective communication
  - Design guide for heritage* – guidance for heritage sites, buildings and precincts
  - The practitioners guide to movement and place*

## Good Design - GANSW Policy & Guidance for NSW Government Agencies



DESIGN QUALITY FRAMEWORKS	GANSW GUIDANCE TOOLS
Design Quality Statement	Design Guides
Design Governance	Design Frameworks
Project Lifecycle Structure	Advisory Notes & Case Studies
Protocols for procurement, design review & design integrity	Design Excellence Guidelines

DIAGRAM 3.2 Good Design - GANSW Policy Guidance for NSW Government Agencies

For further information refer to the link below:  
<https://www.governmentarchitect.nsw.gov.au/>



# Considerations and Requirements for NSW Government Agencies

## 3.7 DESIGN EXCELLENCE

### 3.0 About This Section

*This section of the DE Strategy assists NSW Government agencies in establishing and maintaining design quality for Bays West, as they prepare for the design process and design related activities. It outlines considerations for good design practice and the expectations for future Statutory Design Excellence requirements.*

### 3.1 DESIGN GOVERNANCE AS A MEANS TO DESIGN QUALITY

Effective design governance is a key consideration for NSW Government agencies to embed design quality in the decision making process for the development and delivery of places at Bays West. GANSW promotes a Design Quality Framework (DQF) approach for agencies to address the life span of a project. A DQF focusses on design governance at both the corporate and project level, to establish and sustain design quality through its processes and foster the value of design outcomes. The framework is tailored to reflect an agency's operating model, offering a structured process to integrate good design practices at the corporate level and from a project lifecycle perspective. It brings together in a cohesive manner the following 6 elements:

1. **The agency's Design Quality Statement** – put simply the statement outlines 'what design means' to an agency. It aligns design quality with an agency's corporate vision and values.
2. **Design governance protocols** – establishes how design decisions will be made and remain integrated across differing workstreams for the life cycle of a project. – This may include performance measures, evaluation and monitoring of design quality.
3. **Project lifecycle structure** – identifies the points within the project lifecycle where design quality reviews take place.
4. **Procurement protocol** - establishes strategies to ensure procurement is consistent in its objectives for quality and realises long-term strategic benefits.

5. **Design review protocol** – optimises the design review process for a project, identifying when reviews are to take place and what type of review is required.
6. **Design integrity protocol** - establishes processes and mechanisms to ensure design quality from early project stages is upheld as the design develops.

### 3.2 SYDNEY METRO & GANSW COLLABORATION

Under a co-design approach, GANSW will assist Sydney Metro in preparing a Design Quality Framework to positively impact the design quality of Metro West, including the Bays West Metro station. This will be supported by on-going collaboration on a corporate Design Quality Statement, design procurement and design review specific to Bays West.

### 3.3 DESIGN EXCELLENCE STRATEGIES FOR INDIVIDUAL PROJECTS

Individual projects within the precinct will require a project specific Design Excellence Strategy, endorsed by the consent authority. The Bays West Precinct Design Excellence Strategy sets the strategic context for project specific Design Excellence strategies to occur. This approach enables each project specific Design Excellence Strategy to be proportionate and responsive to a project's complexity, significance and capacity to deliver design quality outcomes and public benefit within the precinct.

*Design review is an internationally tried and tested process to improve the quality of design outcomes in the built environment.*

### 3.4 STATE DESIGN REVIEW PANEL

Significant development within Bays West, such as the new Metro station and surrounds, will be required to participate in the State Design Review Panel (SDRP) program as a Statutory Design Excellence requirement. The SDRP program provides a consistent state-wide approach to reviewing the design quality of projects. The SDRP evaluates a proposal's design quality by reviewing it holistically, independently and expertly against the objectives of *Better Placed* and the requirements of relevant

Environmental Planning Instruments or other specific guidelines.

### 3.4 DESIGN EXCELLENCE COMPETITIONS

Design Excellence Competitions for select projects will be a Statutory Design Excellence requirement that proponents will need to address as part of the planning approval process for Bays West. Design Excellence Competitions are an opportunity to deliver design quality and foster innovation by generating a range of solutions to the same design challenge. Selecting a preferred competition entry is one stage in an overall process intended to lift design quality. The pre-competition processes (e.g. preparing the brief and reference design), and post competition Design Integrity are also critical to the achieving design quality.

During the development of the future planning controls for Bays West, the process of establishing Design Excellence Competition requirements for the precinct will consider;

- the range of competition types - to provide a proportionate response to the complexity and opportunities of individual projects
- competition triggers based on clear criteria for identifying sites, buildings, groups of buildings and key areas of public open space appropriate for competitions
- diversity of design through differing design teams.

Competition triggers will be developed as part of the Statutory Design Excellence planning controls, and may include:

- capital expenditure
- site area
- significant public open spaces
- building height
- significant buildings - including community facilities, culturally significant spaces or adaptive re-use of heritage items
- sites of high visual prominence.

### 3.5 DESIGN INTEGRITY

Design Integrity is a critical component of design excellence. It ensures continuity of design quality through the design finalisation and delivery of a project. Design Integrity describes the retention

or enhancement of a project's design quality that has been established through an earlier Design Excellence activity. This earlier activity is often a design competition, but Design Integrity requirements may also apply where Design Excellence has been established through a design review process.

There are two key components to ensuring Design Integrity:

1. retention of the key design team members in a directing role from the awarding of design excellence through to the completion of design and the delivery of the project.
2. undertaking a design review process to ensure the integrity of the design is maintained and/or enhanced through design finalisation and delivery. This design review will occur within a project specific Design Integrity Panel defined in the endorsed, project specific Design Excellence Strategy.

### 3.6 STRATEGIES FOR DIVESTMENT

Land divestment from NSW Government agencies to the next tier of development is an important process in the development and delivery of Bays West and has the capacity to positively influence outcomes for the precinct. The DE Strategy promotes an approach to divestment that champions design quality and its continuity.

Supported by a design governance structure, agencies should consider appropriate mechanisms to ensure design quality is well represented in a Project Development Agreement (PDA). This includes measures and parameters of design quality alongside commercial indicators. Under this approach, commercial offers can be evaluated in an integrated way against multi-point criteria that address economic, social, cultural and environmental outcomes. The DE Strategy promotes agencies to seek independent design advice and expertise to support the development of a PDA.



# Considerations and Requirements for NSW Government Agencies

## 3.7 DESIGN EXCELLENCE

### GUIDE TO KEY DESIGN EXCELLENCE ACTIVITIES AT VARIOUS PROJECT STAGES

The below table provides an indicative guide to key design excellence activities at Bays West - ranging in scale from precinct-wide to discreet buildings and spaces within Bays West sub-precincts. It outlines

for NSW Government agencies the anticipated future Statutory Design Excellence requirements and the collaboration required to support it. In addition to this, it lists the key documents and design activities that contribute to cumulative design quality relative to project scale.

	PLANNING & DEVELOPMENT STAGE	DESIGN REVIEW & STATUTORY DESIGN EXCELLENCE REQUIREMENTS	COLLABORATION WITH GANSW TO SUPPORT STATUTORY DESIGN EXCELLENCE	CUMULATIVE DESIGN QUALITY FOR EACH PLANNING & DEVELOPMENT STAGE (KEY DOCUMENTS AND PROCESSES)
	Strategic planning <b>ENTIRE BAYS WEST PRECINCT</b> (all sub-precincts)	<ul style="list-style-type: none"> <li>Bays West Precinct Design Review Panel (State Design Review Panel - special session)</li> </ul>	<ul style="list-style-type: none"> <li>Development of the Bays West Precinct - Design Excellence Strategy</li> </ul>	<ul style="list-style-type: none"> <li>Bays West Urban Design Framework</li> </ul>
	Rezoning - master plan & establishing planning controls <b>MULTIPLE SUB-PRECINCTS</b>	<ul style="list-style-type: none"> <li>Bays West Precinct Design Review Panel</li> <li>Sydney Metro - Design Advisory Panel and Design Review Panel (for the Metro site)</li> </ul>	<ul style="list-style-type: none"> <li>Establishing Design Excellence provisions for implementation in the planning controls.</li> </ul>	<ul style="list-style-type: none"> <li>Bays West Urban Design Framework</li> <li>Rezoning master plan</li> <li>Development Control</li> </ul>
	Design, planning approval & delivery <b>KEY INFRASTRUCTURE &amp; PUBLIC OPEN SPACE</b>	<ul style="list-style-type: none"> <li>Bays West Precinct Design Review Panel</li> <li>Sydney Metro - Design Review Panel (for the Metro site)</li> </ul> <p><b>STATUTORY REQUIREMENTS:</b></p> <ul style="list-style-type: none"> <li>Design Excellence competitions for key public open spaces</li> <li>State Design Review Panel for State Significant Development Applications</li> </ul>	<ul style="list-style-type: none"> <li>Developing project specific Design Excellence strategies in response to planning controls and study requirements.</li> <li>Establishing the competition process.</li> </ul>	<ul style="list-style-type: none"> <li>Bays West Urban Design Framework</li> <li>Rezoning master plan</li> <li>Development Control</li> <li>Design guidelines for key elements of Bays West Structure Plan (e.g. public open space)</li> </ul>
	SSDA master plan – Design & staged planning approval <b>INDIVIDUAL SUB-PRECINCT</b>	<ul style="list-style-type: none"> <li><b>STATUTORY REQUIREMENTS:</b></li> <li>State Design Review Panel for State Significant Development Applications</li> </ul>	<ul style="list-style-type: none"> <li>Developing a sub-precinct specific Design Excellence strategy in response to planning controls and study requirements</li> </ul>	<ul style="list-style-type: none"> <li>Bays West Urban Design Framework</li> <li>Rezoning master plan</li> <li>Development Control</li> <li>Design guidelines for key elements of Bays West Structure Plan</li> <li>Finalised master plan</li> </ul>
	Divestment strategies <b>SUB-PRECINCT LAND PARCELLING</b>	<ul style="list-style-type: none"> <li>Design review to support design quality for Project Development Agreements and divestment land parcelling.</li> </ul>	<ul style="list-style-type: none"> <li>Iterative development of the sub-precinct Design Excellence strategy in response to divestment land parcelling.</li> </ul>	<ul style="list-style-type: none"> <li>Bays West Urban Design Framework</li> <li>Rezoning master plan</li> <li>Development Control</li> <li>Design guidelines for key elements of Bays West</li> <li>Master plan finalised for implementation</li> <li>Project Development Agreements</li> </ul>
	Design, planning approval & delivery <b>INDIVIDUAL BUILDINGS &amp; PUBLIC DOMAIN WITHIN LAND PARCELS</b>	<ul style="list-style-type: none"> <li><b>STATUTORY REQUIREMENTS:</b></li> <li>Design Excellence competitions for individual buildings or groupings of buildings &amp; public domain at select sites</li> <li>State Design Review Panel for State Significant Development Applications.</li> </ul>	<ul style="list-style-type: none"> <li>Establishing the competition process. E.g. <i>Government Architect's Design Excellence Competition Guidelines</i> (draft 2018)</li> <li>Developing project specific Design Excellence strategies in response to planning controls and study requirements.</li> </ul>	<ul style="list-style-type: none"> <li>Bays West Urban Design Framework</li> <li>Rezoning master plan</li> <li>Development Control</li> <li>Design guidelines for key elements of Bays West</li> <li>Master plan finalised for implementation</li> <li>Project Development Agreements</li> <li>Competition brief and guidelines</li> <li>Design Integrity strategy for delivery.</li> </ul>

TABLE 3.1 Overview of the key design excellence activities at various project stages



# Collaboration to Support Good Design

## 4.0 About This Section

Collaboration in an ‘*all of project*’ sense is essential to an effective design process. The DE strategy promotes collaboration across NSW Government agencies for projects of all scales at Bays West, enabling the design process to:

- *create consistent objectives that balance stakeholder requirements and aspirations, alongside benefits for the public good*
- *reframe single issues to address wider goals - enabling systematic rather than ad hoc and uncoordinated problem solving*
- *empathise with other points of view - understanding the role of differing areas of expertise*
- *understand how specialist knowledge can be integrated to generate value and create efficiencies*
- *embrace challenges by forming partnerships to achieve better solutions*
- *promote a culture of shared responsibility*
- *enable effective communication.*

## 4.1 COLLABORATION AT PROJECT TRANSITIONS

GANSW has identified 3 key transitions in the project life span of Bays West as requiring on-going collaboration between the relevant government agencies, GANSW and the appropriate design expertise. These transitions and the role of design expertise is outlined below:

1. **The transition from strategic to statutory planning** - in establishing the future planning controls for Bays West, GANSW and DPIE collaboration will develop; parameters for design excellence strategies of individual projects and the Statutory Design Excellence requirements for inclusion in Environmental Planning Instruments and assessment study requirements. To support this transition, GANSW provides strategic design advice and independent and expert design review via the Bays West Precinct – Design Review Panel.
2. **The transition of land ownership through divestment** - obtaining independent design advice to integrate design quality into Project Development Agreements (refer sub-section 3.6 of Part 3.7 for expanded commentary)
3. **The transition to long-term governance structures** – adopting a comprehensive range of expertise including, design and placemaking advice to establish place-focused governance structures that support design quality.



# An Integrated Approach for Future Master Plans & Beyond

## 3.7 DESIGN EXCELLENCE

### 5.0 About This Section

Embedding design excellence in the outcomes and processes of the precinct's future rezoning and master planning activities requires an integrated approach. GANSW has identified 2 key documents to support integration for these project stages - the **Bays West Urban Design Framework** and the future **Design and Place State Environmental Planning Policy**.

*An integrated, place-led and people focused approach for Bays West responds to;*

- *project issues of all scales and degrees of complexity in a spatial and contextual manner*
- *the full spectrum of requirements and aspirations of the precinct's stakeholders in a wholistic way*
- *economic, social, environmental and cultural considerations on an equal basis.*

### 5.1 THE URBAN DESIGN FRAMEWORK

A finalised *Bays West Urban Design Framework* (UDF) sets the platform for an integrated master planning process. Through its place focus and wholistic 'all of precinct' approach, it establishes parameters for good design outcomes, founded in rigorous spatial analysis, shared principles and performance measures for each sub-precinct. Through an iterative process of updating the document at select milestones, the UDF will guide an integrated approach to future planning and design activities for each sub-precinct.

### 5.2 DESIGN & PLACE SEPP

The Department has released the draft *Design and Place State Environmental Planning Policy* (SEPP). NSW Government agencies engaged in future planning applications at Bays West, will need to consider the requirements of the SEPP as they prepare for and undertake design and development activities.

As part of simplifying and improving planning policy frameworks for NSW, it is the intent of the SEPP to simplify, consolidate and support design and place considerations in the planning system. The SEPP will promote integration at Bays West, with the policy taking a principle-based approach to establish parameters for the design and assessment of places across NSW. The place-led approach of the SEPP starts with Country and integrates themes of sustainability, resilience and design quality. The policy will develop considerations and criteria for projects of all scales within a flexible framework.

For Bays West this entails addressing the SEPP's ambition for;

- integrated outcomes – through its objectives for place-based design and planning, for example prioritising public space, green infrastructure and mobility relative to themes of Country, sustainability and resilience
- the design process – enabling design quality through a consistent set of design principles for NSW.

Under this approach, the SEPP will include considering provisions for:

- standardising methods to evaluate good design
- standardising skills and expertise in the design and review of planning and development proposals
- consolidating the approach to design review in a NSW Government context
- revising and incorporating SEPP 65 and BASIX into the SEPP.

For further information refer to the link below:

<https://www.planning.nsw.gov.au/News/2021/Design-at-the-heart-of-new-planning-policy>

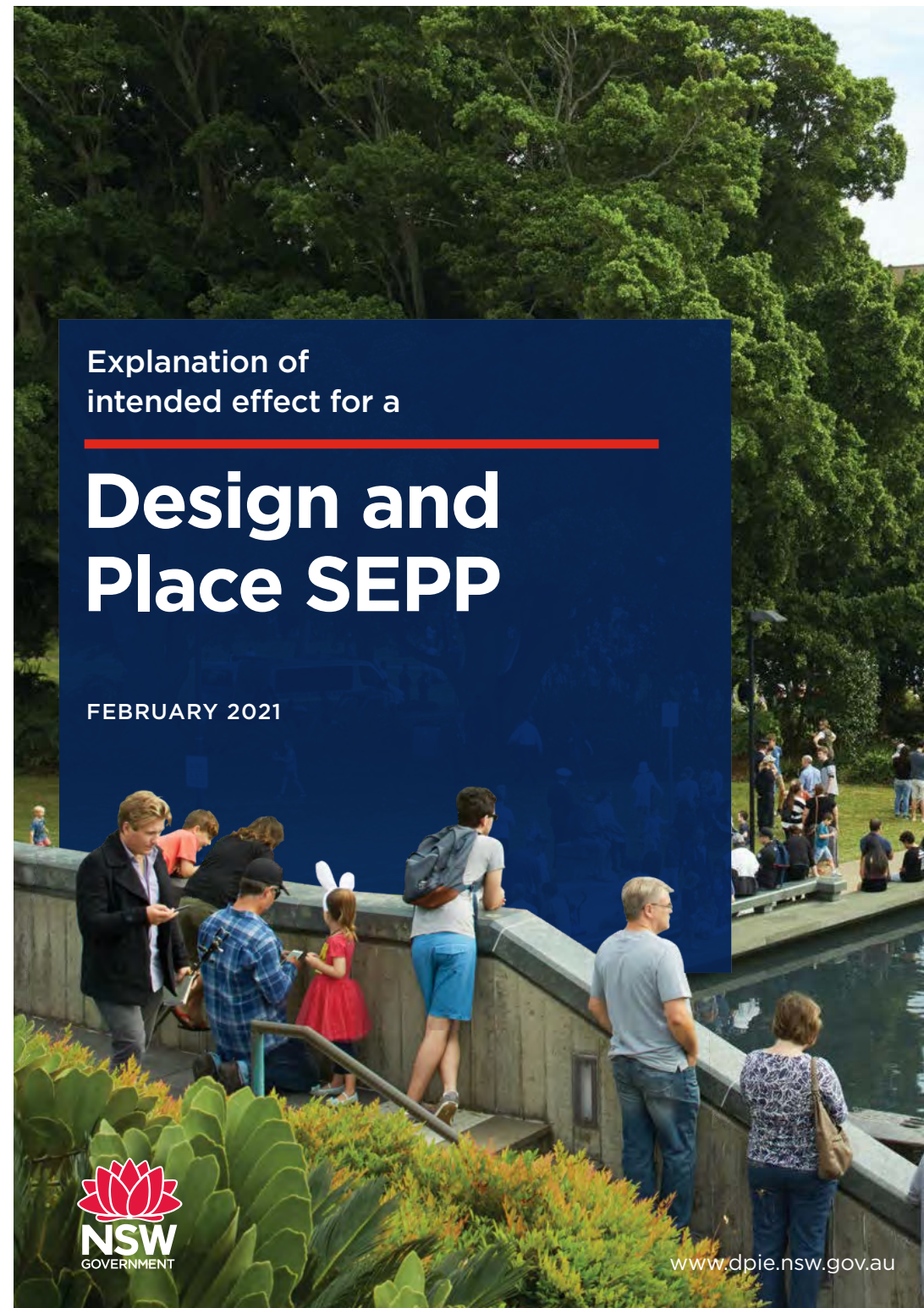


IMAGE 3.5 Explanation of intended effect for a Design and Place SEPP / DPIE



# 4

## SUB-PRECINCT FRAMEWORK



# 4.1

## SUB-PRECINCTS OVERVIEW



# Bays West Sub-Precincts

## 4.1 SUB-PRECINCTS OVERVIEW

This section provides a greater level of detail for each of the sub-precincts of Bay West. Each of the sub-precincts have been explored in terms of their existing condition and the aspirational for their character in the future, with corresponding precedent images.

### INITIAL STAGE

The White Bay Power Station (and Metro) is the initial stage of the Bays West which will be progressing to master planning. As such it has been explored in the greatest detail in the *Draft Urban Design Framework*. For the purpose of this first iteration of this document, this initial sub-precinct is a working example of the detail that will be developed for each of the sub-precincts for the future.

In addition to the overview of the sub-precinct in line with those provided for other sub-precincts there is more detail provided in response to each theme. The further detail against each of the five themes of Land Use & Function, Design of Places & Spaces, Transport and Movement, Heritage and Culture, and Infrastructure Delivery & Governance includes a series of performance criteria upon which the response to the Principles can be considered. To support these more detailed plans of the White Bay Power Station (and Metro) which isolate aspects key plan against each of the theme are provided.

### ONGOING DEVELOPMENT

The Draft Urban Design Framework will evolve in response to further detailed precinct wide studies and strategies being developed. As inputs are received, further detail and options are explored across the site, more certainty on the requirements and structure will be developed for each sub-precincts. This will include the expansion of each subsequent sub-precinct section to include a key plan.

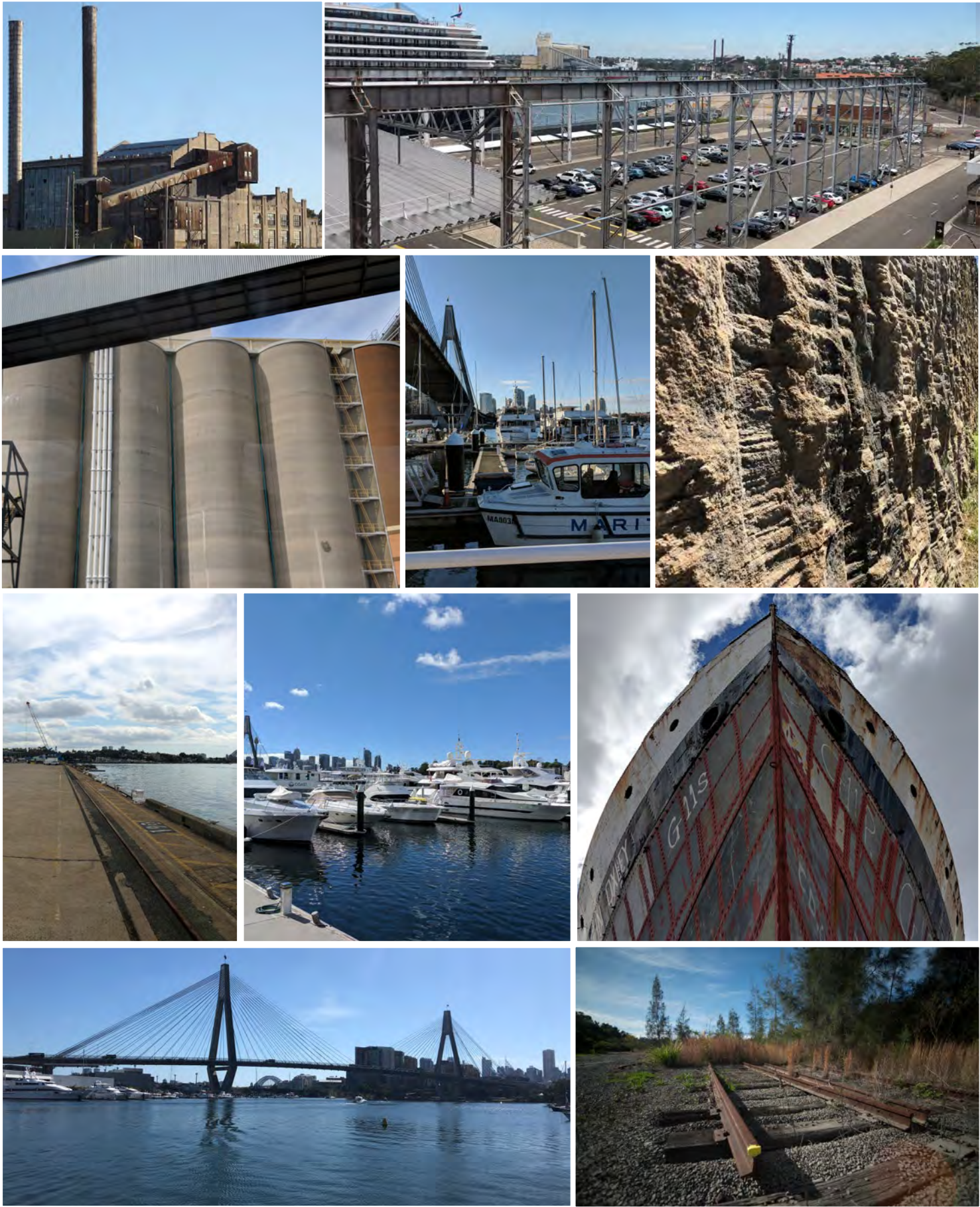


IMAGE 4.2 Bays West existing sub-precinct features



# Bays West Sub-Precincts

## 4.1 SUB-PRECINCTS OVERVIEW

- 1. White Bay Power Station (and Metro)
- 2. Robert Street
- 3. Glebe Island Silos
- 4. Glebe Island Central
- 5. Glebe Island East
- 6. Rozelle Bay East
- 7. Rozelle Bay Central
- 8. Rozelle Bay West
- 9. White Bay
- 10. Rozelle Rail Yards

**Note:** The numbers for each sub-precinct are solely used to identify their location on the following map, they do not represent order of development or any other interpretation.

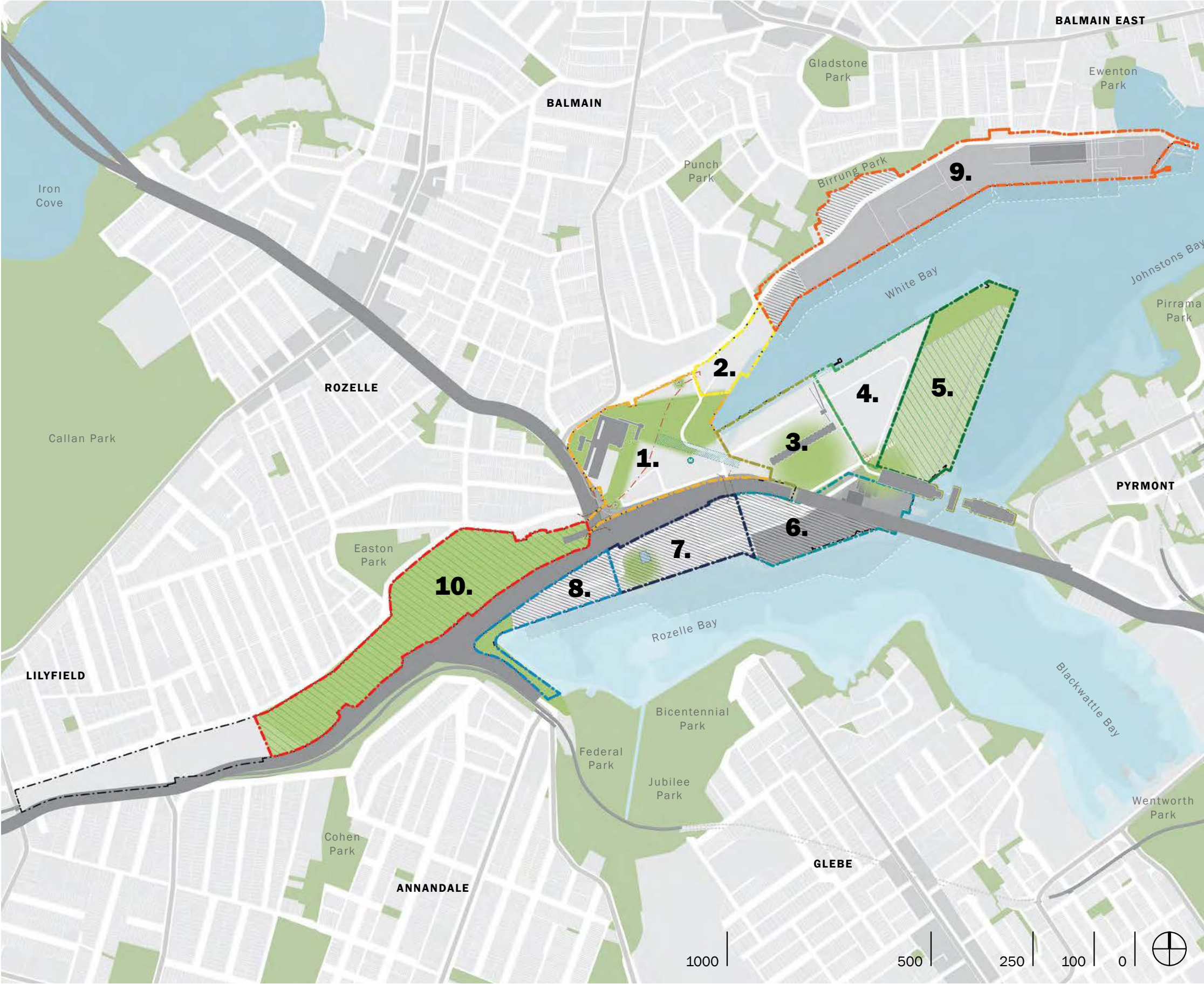


DIAGRAM 4.1 Bays West sub-precincts

Scale 1:10,000 @ A3  
73





# 4.2

## WHITE BAY POWER STATION (THE BAYS STATION PRECINCT)



# Existing Place Character

## 4.2 WHITE BAY POWER STATION (AND METRO)

White Bay Power Station (WBPS) is a Sydney landmark with significant heritage value. The site is not accessible to the public, and has steep level change from Victoria Road down to the water. There is a sandstone escarpment on which the former White Bay Hotel sat which should be celebrated. A ‘mousehole’ connection underneath Victoria Road links the White Bay Power Station to Rozelle Rail Yards and still has the former rail tracks in place.

**Land Use & Function:**

- Single dwelling housing located adjacent to WBPS sub-precinct on Robert St and Victoria Rd
- Light industrial buildings located on Robert St
- No current land use within WBPS sub-precinct

**Design of Places & Spaces - Built Form:**

- White Bay Power Station as major building in sub-precinct
- Other built form elements have been removed by WCX project

**Design of Places & Spaces - Public Domain:**

- Few zones of existing low level vegetation left
- No public domain or public access to WBPS precinct at present
- Sandstone escarpment features and topography changes between site and surrounding arterial roads
- Existing flooding issues from Mullens / Robert St intersection

**Transport & Movement:**

- Existing bus stops located on Victoria Rd and at Robert St
- Ports Access Road currently runs through WBPS sub-precinct
- Constrained access to WBPS sub-precinct due to major arterial roads on boundary
- Key connection between Rozelle Rail Yards and WBPS sub-precinct through ‘mousehole’ under Victoria Rd

**Heritage & Culture:**

- State Heritage Listed White Bay Power Station is a key heritage landmark for the precinct
- Other key heritage items include the sewage pumping station on Roberts St, the Inlet Canal at the head of White Bay, the penstocks.

**LEGEND**

- WBPS Sub-Precinct Boundary
- Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- Sloped Rubble Edge
- Timber Wharf Edge
- No Foreshore Access
- Key Landform
- Green Space

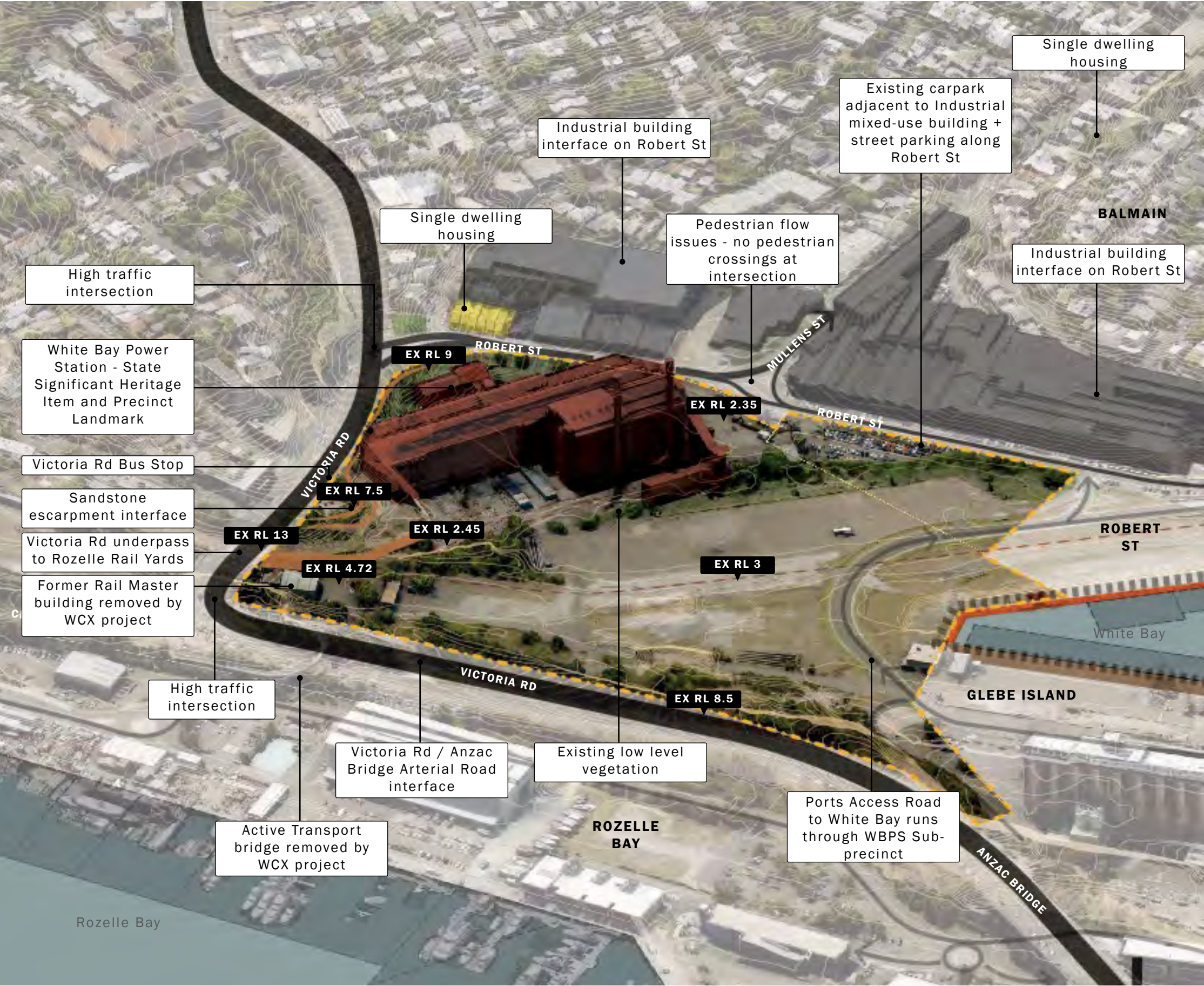


DIAGRAM 4.2 White Bay Power Station Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.2 WHITE BAY POWER STATION (AND METRO)

### WHITE BAY POWER STATION (THE BAYS STATION PRECINCT)

*a focused centre at the heart of the Precinct,  
and a nexus of connectivity to adjacent  
communities and Greater Sydney...*

The White Bay Power Station (and Metro) sub-precinct will be a new destination that serves as a focal arrival point to the Bays West. Focused around the White Bay Power Station and new The Bays metro station, connecting the Precinct to both the Parramatta and Sydney CBDs, new day/night mixed-use development will support a lively and activated public waterfront park opening up to the head of the bay with views across the harbour to the Sydney Harbour Bridge and city skyline.

This sub-precinct will celebrate the White Bay Power Station, sensitively restored and adapted to allow public access and engagement with the stories and artefacts of its former use. The building will continue to serve as one of the landmark structures of the precinct. Its significant preserved through its integration into waterfront parkland and future surrounding development.

A series of further public domain zones surrounding the Power Station embed heritage tracings and markers of the site's rich and evolving history. The former rail track alignment defines the key active transport movement spine through the site, linking the Rozelle Rail Yards Parklands to metro and the head of White Bay. This corridor divides the sub-precinct into two built form character zones both of which will provide a cohesive response between the old and the new elements. New structures north of this alignment are low scale and nestle into the landscape under the shadow of the Power Station. Whereas, south of the rail corridor and around the Metro station, built form intensifies in scale and height, offering visual and acoustic buffering along Victoria Road and the Anzac Bridge approach, but with taller forms still responding to the heritage and gateway context and maintaining key viewsheds towards the Power Station.

As the future heart of the Precinct, this sub-precinct will maximise connection opportunities to other sub-precincts and the surrounding suburbs. Permeability along the Robert Street interface allows for future links to Balmain, while street structures and the active transport network promote walkability and offer links to Rozelle Bay, Glebe, Lilyfield, and Pyrmont. and the active transport network promote walkability and offer links to Rozelle Bay, Glebe, Lilyfield, and Pyrmont.



IMAGE 4.4 Kensington Street / Sydney Fringe



IMAGE 4.5 Landschaftspark Duisburg-Nord



IMAGE 4.6 Coal Drop Yard / World Architecture News



IMAGE 4.7 Kings Cross Square / Will Scott Photography



IMAGE 4.8 White Bay Power Station / DPIE



IMAGE 4.9 Brewery Yard Markets / Broadsheet



IMAGE 4.10 Landschaftspark Open-Air-Cinema / Thomas Burns



IMAGE 4.11 Zollverein / Jochen Tack



IMAGE 4.12 Jack Evans Boat Harbour / Simon Wood



# Key Plan

This key plan is a representation of future development of the precinct and will be subject to further testing and refinement as part of subsequent master planning work. Maximum heights have been defined by the White Bay Power Station CMP and viewsheds.

## LEGEND

- WBPS Sub-Precinct Boundary
- HERITAGE:**
  - Key Heritage Landmark
  - WBPS CMP Boundary
  - WBPS Water Cooling Channel
  - WBPS Viewsheds - Mandatory View
  - Skyline Zone of Influence
  - WBPS Key Axis
  - Key Heritage Interpretation Rail Tracks
- PUBLIC DOMAIN:**
  - Foreshore Promenade
  - Proposed Key Public Domain
  - WCX Land Take
  - Proposed Public Domain Hardscape
  - Overland Flow
- MOBILITY & ACCESS:**
  - Proposed Active Transport Network - Key Link
  - Proposed Active Transport Network - Secondary Link
  - Potential Future Active Transport Links
  - Bus Network
  - Bus Stop
  - Future 'The Bays' Metro Station Box
  - Future 'The Bays' Metro Station
  - Access to Future 'The Bays' Metro Station
  - Intersection Treatment Required
- BUILT FORM:**
  - Arterial Road Acoustic Impact
  - Permeability through ground plane required (additional streets or links)
  - Proposed Zone of Development
  - Additional Proposed Zone of Development
  - Tunnel Zone
  - Key Ground Plane Link
  - Primary surrounding roads
  - Secondary surrounding roads

CMP - Conservation Management Plan

The Zone of Potential Development do not represent building footprint.

## 4.2 WHITE BAY POWER STATION (AND METRO)

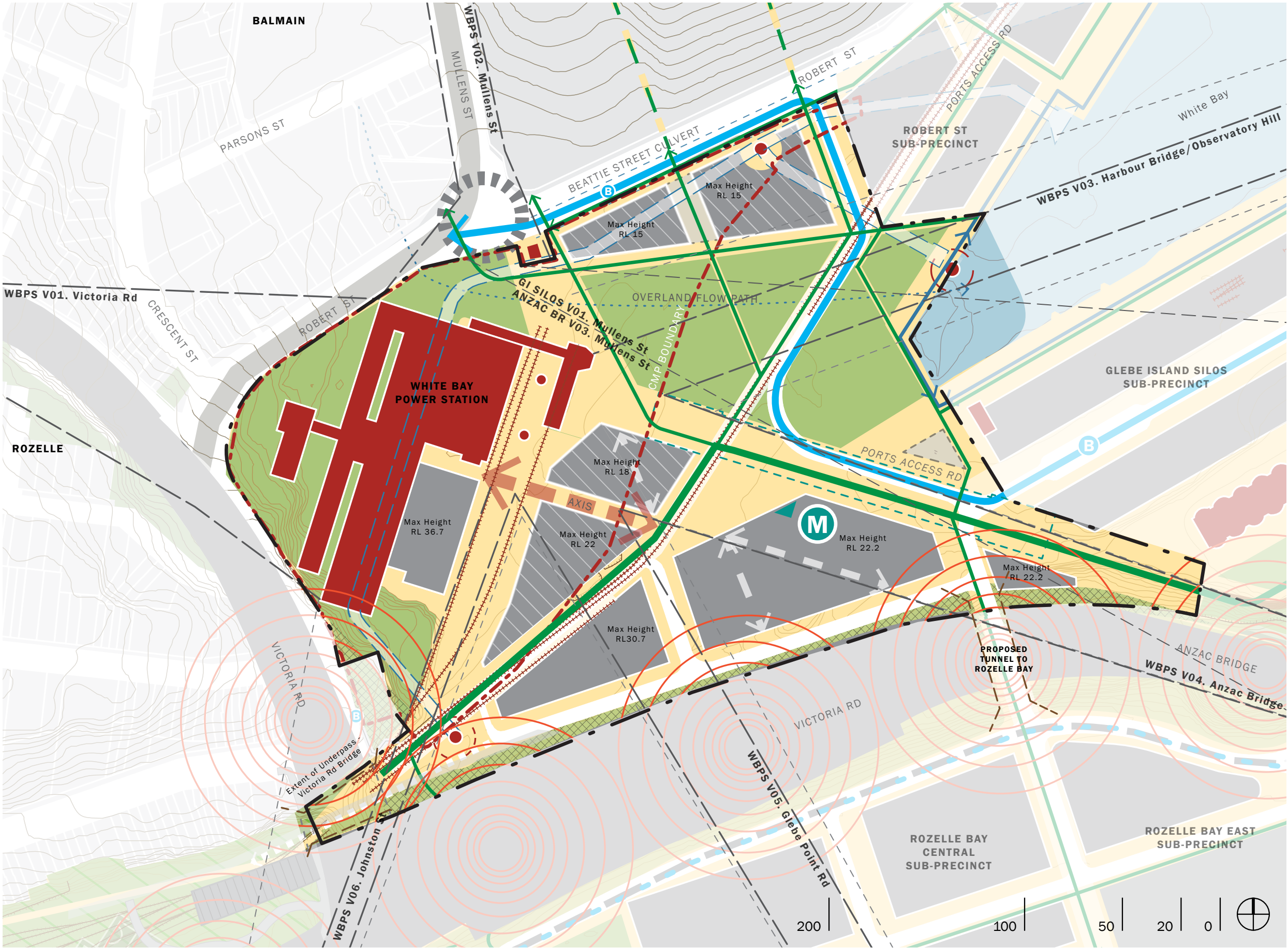


DIAGRAM 4.3 White Bay Power Station Sub-Precinct Key Plan



# Land Use & Function

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### RESPONSE TO PLACE

CONSIDERATIONS	
<b>Celebrate Key Features</b> <i>Underpin the structure of the site with the unique features at Bays West, including the heads of bays and peninsulas, heritage assets and landform. Locate people-focused uses at these key features to optimise community enjoyment whilst balancing ports and working harbour needs.</i>	<ul style="list-style-type: none"><li>• Deliver uses adjacent to and around White Bay Power Station, and associated heritage features, which maximise the enjoyment and appreciation of these items.</li><li>• Deliver a broad range of uses in proximity to head of White Bay and the foreshore promenade to create an active and vibrant place 24/7</li><li>• Deliver appropriately considered uses within White Bay Power Station which respond to the building’s uniqueness through character, scale and volume of spaces – placing limitations on uses such as residential.</li></ul>
<b>Boundary Edges</b> <i>Position uses to create good transitions and integration with surrounding areas. Support existing and new communities by delivering community infrastructure that is well-connected to surrounding areas and situated in clear logical locations which assist in unlocking the Precinct boundary.</i>	<ul style="list-style-type: none"><li>• Ensure uses and community facilities are located to support connectivity between broader Bays West and the surrounds. This will require careful consideration of how this sub-precinct interfaces with surrounding communities. Particular attention is drawn to Mullens Street access zone, Robert Street, Southern Penstock zone (access point from Rozelle Rail Yards and Victoria Rd bus stop)</li><li>• Carefully consider uses located along access to Rozelle Bay that will be required to encourage and support future connections.</li></ul>
<b>Restricted Zones</b> <i>Ensure restricted areas (under bridges, level changes, overshadowed and noisy areas) are well integrated, and accommodate non people-focused uses. Locate high-use and people-focused places are in the areas of highest amenity.</i>	<ul style="list-style-type: none"><li>• Carefully consider interface conflicts associated with arterial roads, ports and working harbour uses, and allocation of adjacent uses. Note future evolution of ports and working harbour interfaces around this sub-precinct.</li></ul>
<b>A Focused Centre</b> <i>Deliver a considered volume and intensity of residential, employment and other uses in proximity to the metro and head of White Bay (and associated open space) to create a centre with 24/7 activity and vibrancy from the initial stage. This creates strong opportunities in proximity to heritage landmarks, the head of the bay and significant open space.</i>	<ul style="list-style-type: none"><li>• Carefully consider land uses within the sub-precinct as a cohesive and complementary proposal</li><li>• Deliver a genuine mix of uses to support sub-precinct’s role as precinct centre from initial stage of delivery through to the future delivery of the entire Bays West precinct</li></ul>

#### A CURATED MIX

CONSIDERATIONS	
<b>Balance</b> <i>Deliver a wide variety of uses that balance the retention of ports and working harbour activity with residential, commercial, education, community and other uses, to support a highly desirable and thriving place that fosters community.</i>	<ul style="list-style-type: none"><li>• Deliver appropriate uses to foster a community-minded mixed-use precinct centre that supports the knowledge intensive precinct ambitions in balance with surrounding local centres, supporting the Innovation Corridor</li></ul>
<b>Evolving Anchors</b> <i>Retain and evolve the role of the existing ports and working harbour uses as anchors of the Precinct. Foster innovation in the ports and maritime related economy, contributing to the overall desired place and strategic outcomes. This creates a unique setting for the precinct and one which has the potential to be a significant draw card, if manage appropriately. .</i>	<ul style="list-style-type: none"><li>• Consider ways of integrating with existing and evolving ports and working harbour elements</li></ul>
<b>Vibrant and activated</b> <i>Locate publicly accessible uses to interface with the ground plane and lower levels of all buildings to create a precinct with magnetic place quality and a 24/7 economy. Enable increased trading in the high activity precinct centre.</i>	<ul style="list-style-type: none"><li>• Provide supporting uses which will be attractive to all members of the community – workers, residents and visitors</li><li>• Integrate a diverse range of ‘sticky spaces’ both inside and outside of buildings that support the precinct centre day and night and activate both commercial and retail uses delivered on site</li></ul>
<b>Considered Infrastructure Distribution</b> <i>Equitably distribute social and cultural infrastructure to maximise public benefit. Position in areas of high amenity and adjacent to high-quality public domain to ensure engagement with and activation of these space.</i>	<ul style="list-style-type: none"><li>• Deliver allocations and locations for supporting social, community, active recreation and cultural uses appropriate to heart of Bays West Precinct</li><li>• Adaptively reuse the Coal Loader as an extraordinary building for public benefit, an entry point to the power station, an events space, or other uses that celebrate its key features.</li></ul>
<b>Diverse and Affordable</b> <i>Ensure diversity in the land use mix to create a place that will attract a broad range of users, residents and workers. Allow for affordable housing and employment spaces to add to the vitality and energy of the place.</i>	<ul style="list-style-type: none"><li>• Ensure appropriate diversity within use mix proposed in line with first stage of precinct centre and to foster sense of community</li><li>• Ensure a diverse range of housing options</li><li>• Ensure a diverse range of commercial spaces in the proposal that will support economies of different scales and types</li><li>• Deliver adequate capacity for supportive uses including community/social infrastructure</li><li>• Address affordability challenges for housing and employment</li></ul>



# Land Use & Function

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### INNOVATIVE USE OF SPACE

CONSIDERATIONS	
<b>Hybrid Mix</b> <i>Facilitate the delivery of diverse strategic needs at Bays West by accommodating non-typical and more diverse mixes within development proposals. Utilise the opportunity to create innovative and distinct building typologies that focus on public use and water access at the lower levels.</i>	<ul style="list-style-type: none"><li>Consider innovation in the mix of use integrated into proposals to accommodate greater allocations of strategic uses</li><li>Recognise that the uniqueness of Bays West comes not only from its location but also history of diverse uses which are still ongoing. The ports and maritime uses within the site are an opportunity.</li></ul>
<b>Flexible &amp; Adaptable</b> <i>Establish a flexible, adaptable and multi-functional approaches to land use, delivering space and building typologies to accommodate the variety of economic activities and the evolving needs of the precinct over time.</i>	<ul style="list-style-type: none"><li>Consider how the demands and needs within the precinct evolve over time as it is realised</li></ul>
<b>Integrated</b> <i>Integrate a range of land uses in developments to deliver a truly mixed-use precinct. This includes the optimisation of ports and working harbour and other uses in a balanced relationship and integration into unified proposals where appropriate.</i>	<ul style="list-style-type: none"><li>Recognise that uses within the precinct will need to be diverse; not only residential, retail, commercial and conventional ‘renewal’ uses.</li></ul>
<b>Clustering</b> <i>Consider place features to determine position for clusters of strategically important ports and working harbour in locations which allow ongoing growth and evolution of the port and working harbour uses in harmony with the new. Clustering will assist in managing amenity impacts on other sensitive land uses and provide proximity and agglomeration benefits.</i>	<ul style="list-style-type: none"><li>Ensure that locations of clusters of ports and working harbour uses are considered.</li></ul>
<b>Considered Interfaces</b> <i>Where development and ports and working harbour uses exist in integrated or separate clustered zones, it is essential to design buffers to mitigate the impacts of industry and protect their ongoing operation. Consider innovative solutions which address impacts such as noise, emissions and traffic and for public foreshore access can be provided.</i>	<ul style="list-style-type: none"><li>Respond to amenity challenges needs to consider how people will use spaces being delivered in buildings, including times of use and times of amenity loss.</li><li>Provide quality resolutions in zones adjacent to ports and working harbour uses with allowance for longer term transition as precinct evolves towards overall ambition</li></ul>

#### SHARING OUR HARBOUR

CONSIDERATIONS	
<b>Public Access To &amp; Use of Water</b> <i>Strive for balancing public access to the foreshore and use of the harbour with the ports and working harbour activities. Deliver an exemplar integrated public and working harbour for Sydney.</i>	<ul style="list-style-type: none"><li>Balance competing priorities of ports and maritime uses with public access and place outcomes.</li><li>Ensure that the opportunity to increase public access in the long term as the precinct evolves is preserved</li></ul>
<b>Grading of Scales</b> <i>Locate and protect larger scale strategic port and working harbour uses at deep-water berthing zones. Where possible consolidate smaller / lower impact vessels (e.g. recreational vessels; charter boats) along public sections of foreshore and in proximity to public domain areas.</i>	<ul style="list-style-type: none"><li>Address short and long-term relationship with vessel berthing on north east face of Glebe Island</li></ul>
<b>Space for Water Ecosystem</b> <i>Demonstrate caring for Country in harmony with ports and working harbour uses. Establish ecology and biodiversity zones at the heads of bays connected to new blue-green corridors and including parameters for establishing sea horse protections.</i>	<ul style="list-style-type: none"><li>Integrate design solutions to create space for local flora and fauna within water zones</li><li>Establish greater protections for sea horse habitat in short and long term</li></ul>
<b>Engagement with Maritime Uses</b> <i>Foster a sense of engagement with port and working harbour activity. Recognising these uses, create a distinctive place and places for people to interact with the port and working harbour.</i>	<ul style="list-style-type: none"><li>Provide appropriate ways to engage with ports and working harbour uses in proximity to initial stage of development</li></ul>



# Land Use & Function

## 4.2 WHITE BAY POWER STATION (AND METRO)

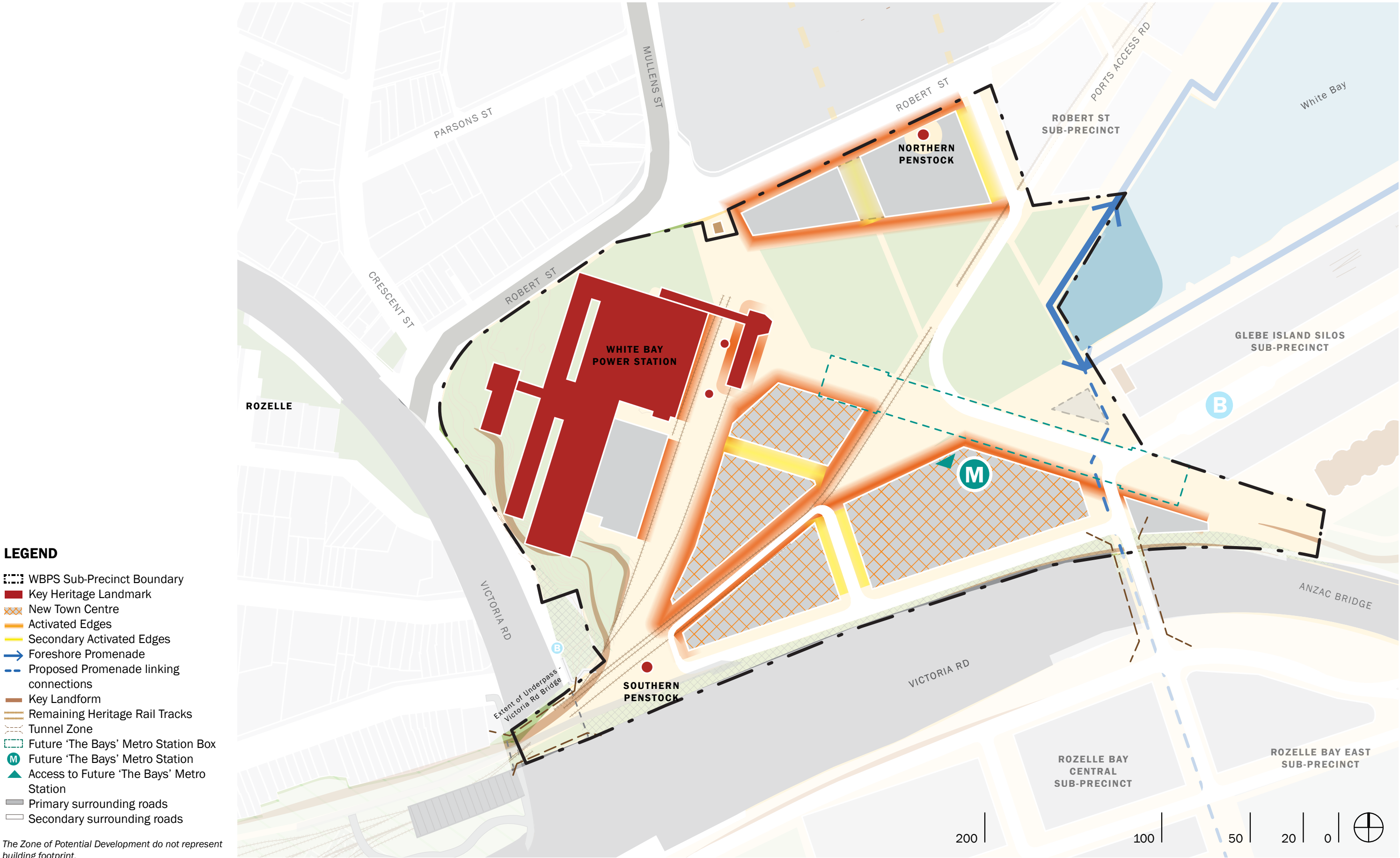


DIAGRAM 4.4 White Bay Power Station Sub-Precinct Land Use & Function Plan



# Design of Places & Spaces - Public Domain

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### A PUBLIC GROUND PLANE

CONSIDERATIONS		CONSIDERATIONS	
<b>On Arrival</b> <i>Unlock the Precinct thresholds and ensure entering the Precinct is a wonderful experience, discovering a place that shares the stories of Country, and the Harbour's maritime identity, is welcoming to all people and has views to significant features of Bays West.</i>	<ul style="list-style-type: none"><li>• Ensure that unique character and features of the precinct are on display at all access nodes.</li></ul>	<b>Permeable</b> <i>Prioritise public access and accessibility for pedestrians and cyclists. Create high levels of permeability between and within buildings and direct and intuitive movement paths to the precinct's major features and services.</i>	<ul style="list-style-type: none"><li>• Recognise that not all movements through the precinct are equal – some meander, some have focus and others are exploring.</li><li>• Ensure appropriate permeability provided through ground plane with particularly focus on:</li><li>• New development zones around WBPS</li><li>• New development zones located between Robert Street and Power Station Park</li><li>• Provision of connections between major features within sub-precinct and to future access nodes and features of other sub-precincts</li><li>• Deliver compliant grading to public domain and streetscapes across site</li><li>• Design a high-quality accessible solution to level change at WBPS coal loader plaza and northern facade</li></ul>
<b>Public Access</b> <i>Open the ground plane up to people, whilst considering the operational and safety factors constraints of ports and working harbour areas. In all other areas be open and public with free access in, around and through the built form and generous public domain, creating places to gather and foster a sense of community.</i>	<ul style="list-style-type: none"><li>• Ensure public access provided across site connected to ground level – this will vary depending on the location, accessibility and character of uses delivered.</li><li>• Deliver meeting and gathering spaces which promote social contact and foster community</li></ul>		
<b>Quality Interface &amp; Active Edges</b> <i>Ensure the design of the ground and lower levels of buildings contribute to the amenity, character, passive surveillance and activation of adjacent streets and public spaces.</i>	<ul style="list-style-type: none"><li>• Prioritise extents of building façade which are visually and physically permeable for public uses</li><li>• Allow for distribution of active edges to major connections and public domain zones</li><li>• Create human scale and engaging buildings at the street level.</li></ul>		
<b>Ensuring Amenity</b> <i>Ensure built forms contribute to comfortable streets and public spaces. This include consideration for solar, wind, acoustic and other micro-climatic conditions and protection of solar access to existing open space areas.</i>	<ul style="list-style-type: none"><li>• Ensure significant proportions of external public spaces have solar access for more than 2 hours on the winter solstice – this will need to be examined and tested for all sites. All open spaces will need to be optimised and responded to by buildings.</li><li>• Deliver wind conditions suitable for pedestrians, cyclists and other occupants of streets and open space</li><li>• Deliver acoustic conditions suitable for pedestrians, cyclists and other occupants of streets and open space.</li><li>• Consider other micro-climatic impacts which would affect amenity of ground plane and how buildings are affecting / creating these.</li></ul>		



# Design of Places & Spaces - Public Domain

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### QUALITY FORESHORE & HARBOUR INTERFACE

CONSIDERATIONS		CONSIDERATIONS	
<b>Transition</b> <i>Create a quality relationship between the landform, built form, public domain and foreshore. Transition the height of built form down to the water and minimise adverse effects on the foreshore conditions.</i>	<ul style="list-style-type: none"><li>• Ensure appropriate scale of building along foreshore edge</li></ul>	<b>Connecting with water</b> <i>Enable direct physical engagement and access to water with prioritised permeability to foreshore. Ensure you can dip your toes in the water and marvel at the natural beauty of the precinct and its place in Sydney Harbour.</i>	<ul style="list-style-type: none"><li>• Ensure direct and meaningful engagement with water around the head of White Bay</li></ul>
<b>Setback and Edge Diversity</b> <i>Provide substantive and varied setback from the water and a diverse set of water’s edge conditions to set up a variety of waterfront character zones with opportunities for outlook to both water and ports and working harbour activities. A combination of boardwalks, steps to water, places to touch the water and heritage canals and inlets, will create a unique experience.</i>	<ul style="list-style-type: none"><li>• Ensure appropriate scale of foreshore depth to accommodate movements and any uses spilling from proposed built form and public domain. Wide foreshore depth required at head of White Bay.</li><li>• Provide quality edge conditions appropriate to head of White Bay</li></ul>	<b>Cohesive</b> <i>Ensure the foreshore does not feel like it is delivered in many different parts. There must be an integrated language that works across the whole precinct that blends into neighbouring zones. The cohesion and legibility of the foreshore design will be important to the delivery and success of the entire Precinct.</i>	<ul style="list-style-type: none"><li>• Establish appropriate design language for foreshore area which can be extended to other sections</li><li>• Provision for future connections of foreshore to be seamlessly integrated without appearance of joints</li></ul>
<b>Solar Access</b> <i>Maximise solar access to foreshore promenade must be considered at Bays West, prioritising high quality public domain areas, the heads of the bays, the foreshore promenade and the water to foster marine life. Overshadowing impacts are to be minimised but it is recognised that these may be unavoidable in limited circumstances.</i>	<ul style="list-style-type: none"><li>• Deliver minimum solar access provisions for new Power Station Park. This will be tested through subsequent master planning but could be expected to deliver 3hrs direct solar access on June 21 between 10-2pm to 75% of public domain area.</li><li>• Allow for future compliant built form along Robert Street</li><li>• Allow for minimum solar access provisions for Heritage Canal Park given challenging solar orientation of Rozelle Bay area. This will be tested through subsequent master planning but could be expected to deliver 3hrs direct solar access on June 21 between 10-2pm to 75% of public domain area with zone within 30m of foreshore promenade to have no overshadowing from this sub-precinct throughout day</li></ul>		
<b>Continuous and Connected Lengths</b> <i>Establish long, uninterrupted sections of public foreshore which connect to existing. Locations of larger public access lengths to be determined with consideration of the needs of the working harbour. Avoid small fragmented foreshore sections and where the ports and working harbour uses restrict public access at the waterfront, it is essential for high amenity connections joining these spaces together.</i>	<ul style="list-style-type: none"><li>• Establish initial section of foreshore to future anticipated connections to other Bays West sub-precincts</li></ul>		



# Design of Places & Spaces - Public Domain

PERFORMANCE CONSIDERATIONS

CELEBRATING LEVELS AND LANDFORM

CONSIDERATIONS	
<p><b>Revealing the escarpment</b></p> <p><i>Retain the escarpment and sandstone cuttings as features of Bays West that contribute to its drama and uniqueness. The escarpments at White Bay, Rozelle Rail Yards and Glebe Island are signifiers of the natural land formation and speak to changes over time that have reshaped this place.</i></p>	<ul style="list-style-type: none"><li>• Ensure that sandstone cuttings to Mid South Yard, Upper South Yard and around Administration entrance areas of WBPS are expressed.</li></ul>
<p><b>Legibility of Landform</b></p> <p><i>Reveal the story of the site evolution through the landform and level changes which make each part of Bays West distinct. Maintain the legibility of the current landform and ensure that public places, and buildings, are sited and designed appropriately according to this unique landform.</i></p>	
<p><b>Level transition and integration</b></p> <p><i>Provide design solutions to enable level change including built form integration, visibility between zones and highly visible access arrangements. Ensure convenient and legible public access provisions provided to all public use areas.</i></p>	<ul style="list-style-type: none"><li>• Sensitively integrate any built form to areas where cuttings/escarpments occur around WBPS in line with provisions of CMP.</li></ul>
<p><b>Overcoming Barriers</b></p> <p><i>Use infrastructure creatively (tunnel links, bridges, stairs or lifts) to increase connectivity and access to and across the precinct and enhance the human experience of the precinct's landforms /topography / nature</i></p>	<ul style="list-style-type: none"><li>• Provide publicly accessible stair and lift access from Victoria Road and existing bus stop location down to sub-precinct level</li><li>• Coordinate arrangements for potential access point under Anzac Bridge off ramp to access Rozelle Bay</li><li>• Recognise that buildings can overcome access challenges by delivering new connections in appropriate locations</li></ul>
<p><b>Experiencing Scale</b></p> <p><i>Respond to the unique characteristics of the waterfront setting. Retain areas where the unusual experience of the being up close to both the grand scale of heritage buildings, large ships and landscape features and the finer grain features occur.</i></p>	<ul style="list-style-type: none"><li>• Retain the experience of dramatic scales of WBPS elements including more intimate moments through sensitive placement of new built form and public domain elements</li><li>• Retain the experience of harbour scale from public domain space</li></ul>

PLACE-BASED OPEN SPACE NETWORK

CONSIDERATIONS	
<p><b>Blue Green Corridors</b></p> <p><i>Reinstate and protect original creek corridors that provide logical links between the site and the surrounding areas with smooth level transitions. Extend the blue/green corridors through the site as the backbone of public domain linking existing open spaces to the harbour.</i></p>	<ul style="list-style-type: none"><li>• Deliver high-quality district/destination open space early in delivery as Power Station Park.</li><li>• Deliver extent of space, the major emphasis and views and setting anticipated for space (refer page 86)</li><li>• Recognise that these extend beyond the precinct and that Bays West can link these together.</li><li>• Work with key stakeholders to ensure connections extend beyond the site.</li></ul>
<p><b>Heritage and Topographical Elements</b></p> <p><i>Locate extended public domain spaces in response to heritage elements and dramatic landform locations, ensuring these elements are the heroes of these spaces. Theses should provide meaningful place anchors as the character focus of different spaces.</i></p>	<ul style="list-style-type: none"><li>• Provide high-quality heritage related spaces. Deliver major emphasis and views and setting identified for:<ul style="list-style-type: none"><li>• Transformer and South Yards (refer page 86)</li><li>• Coal Loader Plaza (refer page 87)</li><li>• Northern Penstock Zone (refer page 87)</li><li>• Southern Penstock Zone (refer page 87)</li></ul></li></ul>
<p><b>Infrastructure Integration Benefits</b></p> <p><i>Optimise opportunities within larger scale infrastructure to create innovative new public domain spaces. Ensure they are well integrated and well-connected, and do not feel like 'left over' space.</i></p>	<ul style="list-style-type: none"><li>• Integrate with Rozelle Rail Yard parklands design and delivery, including public domain area along south-eastern boundary of sub-precinct</li></ul>
<p><b>Distributed and Diverse</b></p> <p><i>Deliver a wide variety of landside and waterfront activities within the open space network, ranging from restorative, to passive and active, and integrate them into high-quality public domain spaces. Determine a hierarchy of parks, squares, streets and lanes and ensure they are well connected and distributed across the precinct.</i></p>	<ul style="list-style-type: none"><li>• Deliver distinct zones across public domain with a clear hierarchy, and provide a diversity of landside and waterfront activities and both recreational and restorative spaces</li><li>• Consider the needs of various user groups</li><li>• Consider events and programming opportunities</li><li>• Consider the broader planned network of Bays West public domain spaces and their roles, and how the community will move between them. Avoid duplication or overloading role/provisions within initial public domain.</li></ul>
<p><b>View and Outlook</b></p> <p><i>Provide uninterrupted outlook to sky, water and views of landmark features within and beyond the precinct at key locations. Key views must be visible and prioritised from the public domain via siting and orientation.</i></p>	<ul style="list-style-type: none"><li>• Protect quality views to water from public domain spaces</li><li>• Maintain access to signature views from within public domain spaces including views to harbour bridge, city skyline and local ridgelines</li><li>• Maintain access to WBPS from majority of public domain space</li><li>• Consider the broad range of views across the precinct and balance how these are responded to within the sub-precinct</li></ul>



# Design of Places & Spaces - Public Domain

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### SUPER GREEN STREETS

##### CONSIDERATIONS

<b>Canopy Cover</b> <i>Promote green ecology and biodiversity throughout the Bays West Precinct via connected green street networks. Minimise the impacts of heat island effects through well shaded streets that will also offer amenity and comfort to users.</i>	<ul style="list-style-type: none"> <li>• Deliver strong and connected canopy cover through the streets of the sub-precinct noting the importance of street trees on delivering overall canopy cover targets</li> </ul>
<b>Street level experience</b> <i>Create a great experience from the street level and understand the implications of scale, height, shadow and views. Optimise the ground plane experience by ensuring views to sky, place features and a balance of light and shade</i>	<ul style="list-style-type: none"> <li>• Ensure views to place features and surroundings from within street network including to water, Sydney Harbour Bridge and heritage features</li> <li>• Ensure street wall heights and length support a human scale</li> <li>• Consider experience of overall building bulk and scale along street corridors including proportion of sky view available</li> </ul>
<b>Comfort and Safety</b> <i>Prioritise the comfort and safety of users in the Precinct providing shade, protection and shelter year-round, ensuring a positive experience for pedestrians, cyclists and those using the footpath in other ways.</i>	<ul style="list-style-type: none"> <li>• Ensure that protection such as awnings are provided along street edges for users at appropriate building interfaces</li> <li>• Ensure that streets are appropriately shaded by trees during summer months</li> </ul>
<b>Sustainable Drainage</b> <i>Deliver integrated Water Sensitive Urban Design and integrate with urban water cycles throughout the precinct.</i>	<ul style="list-style-type: none"> <li>• Deliver best practice permeable surfaces within street corridors.</li> <li>• Support the integrated watering of street trees from stormwater solutions</li> <li>• Deliver best practice WSUD features in line with the significance of the precinct and recognise that it will need to connect beyond the boundaries of the sub-precinct.</li> </ul>
<b>Space allocation</b> <i>Provide adequate space for each form of movement and activity needed along the street. Streets are crucial parts of the public domain and create opportunities for activation and inhabitation of the street. i.e. outdoor dining, parklets etc</i>	<ul style="list-style-type: none"> <li>• Deliver appropriate width of street corridors to accommodate movements, green space and other uses of footpath zones as appropriate to building edge interfaces</li> </ul>

#### MAINTAIN LANDMARK VIEWSHEDS INTERFACE

##### CONSIDERATION

<b>Gateway Sequence</b> <i>Retain the role of the three major landmarks as visual markers of the gateway sequence between the Inner West and Pyrmont/CBD by enabling these to be experienced together. Protect and retain important views along major movement corridors and consider new buildings impacts within this experience.</i>	<ul style="list-style-type: none"> <li>• Consider composition of new built form along arterial road corridors and integration with and experience of existing landmarks (framing and foregrounding / not overwhelming)</li> </ul>
<b>Prioritise Public Views</b> <i>Prioritise the retention of public views to the precinct landmarks as well known and iconic features of the Bays West landscape. Public views to them from inside and outside the precinct must be maintained.</i>	<ul style="list-style-type: none"> <li>• Ensure that views to landmarks are appropriately considered and balanced to support the delivery of the precinct.</li> </ul>
<b>District Views</b> <i>Protect the district views of landmark features which shift and change along public movement and open space areas. Retain their contribution to the place character and display symbolic wayfinding of the Sydney skyline. Protect the district views in line with the detailed viewshed studies and recommendations.</i>	<ul style="list-style-type: none"> <li>• Compliance with WBPS district view protections: V01 - Victoria Road V03 – Harbour Bridge / Observatory Hill other surroundings to consider</li> <li>• Compliance with Anzac Bridge district view recommendation: V02 - Victoria Road</li> </ul>
<b>Local Views</b> <i>Utilise views from surrounding suburbs to support the preservation of the character and grain of the place. Protect the local views in line with the detailed viewshed studies and recommendations.</i>	<ul style="list-style-type: none"> <li>• Compliance with WBPS local view protections: V01 - Victoria Road V02 - Mullens Street</li> <li>• Compliance with Anzac Bridge local view recommendations: V02 - Victoria Road V03 - Mullens Street</li> <li>• Compliance with Anzac Bridge local view recommendations: V01 - Mullens Street V11 – Victoria Rd ‘Mousehole’</li> <li>• Consideration of other district and local views retained to landmarks</li> </ul>
<b>Precinct Views</b> <i>Preserve internal views to landmarks to maintain the clarity of these elements within the Precinct. Provide understanding of the size, scale, texture and materiality of these elements when viewed from within the precinct.</i>	<ul style="list-style-type: none"> <li>• Protection of views to WBPS from head of White Bay contained within WBPS viewshed V03 – Harbour Bridge / Observatory Hill (limiting height of amenities and supporting structures within open space to under AHD 10m)</li> <li>• Protection of views to WBPS along axis as identified in CMP</li> <li>• Consideration of other precinct views retained to landmarks</li> </ul>



# Design of Places & Spaces - Public Domain

## 4.2 WHITE BAY POWER STATION (AND METRO)

PUBLIC DOMAIN FOCUSED

LEGEND

- WBPS Sub-Precinct Boundary
- Key Heritage Landmark
- Proposed Key Public Domain
- Proposed Public Domain Hardscape
- Overland Flow
- Key Landform
- Key View to Harbour
- Previous Heritage Tracings
- Remaining Heritage Rail Tracks
- Future 'The Bays' Metro Station Box
- Future 'The Bays' Metro Station
- Access to Future 'The Bays' Metro
- WBPS Water Cooling Channel
- Tunnel Zone
- Primary surrounding roads
- Secondary surrounding roads

The Zone of Potential Development do not represent building footprint.



DIAGRAM 4.5 White Bay Power Station Sub-Precinct Public Domain Plan



# Key Public Domain Spaces

## 4.2 WHITE BAY POWER STATION (AND METRO)



IMAGE 4.13 Jack Evans Boat Harbour / Simon Wood



IMAGE 4.14 Seagrass / ABC News



IMAGE 4.15 Yangpu Riverside / Yong Zhang

(A)

### POWERSTATION PARK

The Powerstation Park is a destination. It will be a place where you can touch the water, watch water flow through channels, play, gather, rest and take in the view. Its unique setting between the harbour and the White Bay Power Station will be celebrated, and it will offer a range of public domain spaces that are intriguing, restorative and exciting.

**TYPE:** Destination Park / District Park

**EMPHASIS:**

- Immersion in nature and heritage
- Revealing past heritage tracings, such as rail tracks, former and current buildings through public domain structure and interpretation
- Rehabilitation of marine ecologies and water quality through water capture and treatment
- Access to the water and water level change and interpretation
- Address significant overland flow and flooding issues in this zone
- Restorative and climate responsive waterfront parkland to deal with storm surge and sea level rise
- Northern Forecourt to White Bay Power Station entry area
- Transport interchange interface

**POTENTIAL USES & FEATURES:**

- An iconic public place in a unique setting with access to water
- Events spaces
- Children’s play and waterplay
- Greens for informal play and gathering
- Extensions of food and beverage offerings from surrounding development zones

**VIEWS + SETTING:**

- Views across harbour to Sydney Harbour Bridge and city skyline
- Ensuring visibility of White Bay Power Station through public domain
- Key public domain forecourt integrating with the plaza area for ‘The Bays’ metro station



IMAGE 4.16 Landschaftspark Duisburg-Nord



IMAGE 4.17 Goods Line / Florian Groehn



IMAGE 4.18 Landschaftspark Duisburg-Nord

(B)

### WHITE BAY POWER STATION TRANSFORMER AND SOUTH YARDS

The Transformer Yard public domain space integrates heritage remnants into the public domain, whilst providing a meeting place adjacent to the power station, and a buffer zone at a key node between Victoria Road and Robert Street. It will navigate significant level changes to create a space that contributes to the green open space network within the precinct.

**TYPE:** Pocket Park

**EMPHASIS:**

- Revealing past heritage tracings through public domain interpretation
- Negotiate existing level changes
- Entrance experiences from Robert Street and Victoria Road

**POTENTIAL USES & FEATURES:**

- Local recreation space
- Meeting and gathering zone directly associated with areas of White Bay Power Station
- Habitat rehabilitation and high volume of tree canopy cover
- Access point from Victoria Rd bus stop into White Bay Power Station sub-precinct

**VIEWS + SETTING:**

- Key Gateway and entrance node into precinct from Victoria Rd
- More open interface along Robert Street



# Key Public Domain Spaces

## 4.2 WHITE BAY POWER STATION (AND METRO)



IMAGE 4.19 Brewery Yard / Broadsheet



IMAGE 4.20 Kensington Street / Sydney Fringe



IMAGE 4.21 Landschaftspark Duisburg-Nord

(C)  
COAL LOADER PLAZA

The forecourt of the White Bay Power Station will be known as Coal Loader Plaza, a potentially paved public place that will retain and integrate the legibility of the rail corridor as part of the public domain. It could be used for large-scale public events, small gatherings or outdoor community recreation.

**TYPE:** Linear Park / Destination Park

**EMPHASIS:**

- Revealing past heritage tracings and rail lines, enabling public access and legibility throughout the public domain
- Capture unique nature of the previous use of Coal Loader Plaza to create a new public domain experience
- Meeting and gathering space + community events space

**POTENTIAL USES & FEATURES:**

- Local event space and programmable associated with White Bay Power Station
- Installations
- Ensure compliant ramping requirements around Coal Loader Plaza, level changes and flood mitigation
- Vehicle access for servicing of White Bay Power Station including emergency access

**VIEWS + SETTING:**

- Views of White Bay Power Station, particularly Boiler House, Chimney, Ash Handling, Conveyor and Coal Loader Shed
- An extension of the new built form edges with spill out zones for retail, outdoor dining etc



IMAGE 4.22 Mill Ruins Park / Minneapolis Parks



IMAGE 4.23 Kings Cross Square / Will Scott



IMAGE 4.24 Das Ruhr Museum

(D) (E)  
PENSTOCK ZONES

The retention and integration of the heritage Penstocks provide opportunities for rich, diverse public places that recognise the history of the site and enable engagement with it. The Penstock Zones could be hardstand plazas that include green canopy cover, encourage meeting, gathering and engaging with heritage.

**TYPE:** Local Park

**EMPHASIS:**

- Retention, interpretation and adaptive reuse of Penstocks and integration of interpretation of water channels connecting these to White Bay Power Station
- Reveal past tracings surrounding Penstocks
- Southern Penstock is a key gateway moment into White Bay Power Station sub-precinct from Rozelle Rail Yards
- Northern Penstock can become a feature of Robert Street interface

**POTENTIAL USES & FEATURES:**

- Meeting and gathering zones
- Extensions of food and beverage offerings from surrounding development zones
- Hardstand Plaza zones

**VIEWS + SETTING:**

- Legibility as part of White Bay Power Station power production processes



# Design of Places & Spaces - Built Form

PERFORMANCE CONSIDERATIONS

CARE WITH BUILT FORM

CONSIDERATIONS	
<p><b>Density Done Well</b></p> <p><i>Capitalise on the Precinct’s strategic location and access to alternative modes of transport to deliver compact development with exceptional levels of urban amenity.</i></p>	<ul style="list-style-type: none"><li>• Ensure proposed density is relative to context (heritage and suburb scale) and location as new precinct centre with high volume transport provision</li><li>• Future development is to ensure that the density needs to be supported by appropriate social and community infrastructure, including open space</li></ul>
<p><b>Responsive</b></p> <p><i>Integrate new built form within the current and evolving context to ensure the suitability of proposals to its setting. Create an appropriate relationship between the new built form with the surrounding low scale suburbs and sensitively treat the transition to these.</i></p>	<ul style="list-style-type: none"><li>• Ensure that the retention and adaptive reuse of WBPS as the hero/dominant feature of the site is seen from public domain spaces through careful consideration of the buildings in immediate vicinity (particularly to north of railway track alignment)</li><li>• Ensure the alignment of building envelope and scale and bulk are supported by the WBPS CMP</li><li>• Provide high quality interfaces to adjacent suburbs</li><li>• Integration of new development adjacent to Robert Street with scale and character of existing built form and overall scale of Balmain peninsula</li><li>• Ensure future development responds to the potential of the non-standard geometries between the large-scale elements of the site (WBPS and Anzac Bridge) and harbour settings.</li></ul>
<p><b>Grain</b></p> <p><i>Ensure that building bulk, mass, height and scale considers both the location within the precinct and relationship with the surroundings. Provide an appropriate human-scaled experience and contribute to a walkable Precinct.</i></p>	<ul style="list-style-type: none"><li>• Ensure the future development responds to scale of landmark elements and dense urban character, scale, form and topography of the neighbouring precincts.</li><li>• Deliver a grain of built form appropriate for human-scale experience</li></ul>
<p><b>Materiality</b></p> <p><i>Consider built form materiality relative to the place context which combines the post-industrial attributes with stunning harbourfront. This uniqueness must be captured in the materiality which tells the story of the site’s past and future but does not compete with these landmarks.</i></p>	<ul style="list-style-type: none"><li>• Ensure a selection of high-quality building materials appropriate to site's industrial and waterfront character. Selections to be sympathetic and supportive of WBPS. This must avoid confusing the legibility of this feature</li><li>• Consider selection of materials in line with sustainability and building performance. This should include low-carbon materials and construction processes, reduced embodied emissions, cool materials and circular economy initiatives</li></ul>

APPROPRIATE DISTRIBUTION OF HEIGHT

CONSIDERATIONS	
<p><b>Amenity Impacts</b></p> <p><i>Ensure built form is shaped to protect the amenity of the adjacent low-rise suburbs, the water along with the existing and new foreshore and public domain.</i></p>	<ul style="list-style-type: none"><li>• Consider the mitigation of increased amenity impacts from taller building elements</li></ul>
<p><b>Locating Height</b></p> <p><i>Locate height in the most appropriate zones and transition it across the site in a logical manner. Utilise elements of built form to assist as a buffer from noise and emissions created by arterial roads and ports and working harbour operations.</i></p>	<ul style="list-style-type: none"><li>• Ensure appropriate positioning of height (south of railway tracks and away from WBPS) and utilisation to buffer impacts of arterial road</li></ul>
<p><b>Separation</b></p> <p><i>Provide appropriate separation between taller buildings to elements and avoid negative impacts on the pedestrian experience of streets and public spaces (induced winds, overshadowing). Also avoid the visual impacts of large walls of taller buildings.</i></p>	<ul style="list-style-type: none"><li>• Ensure appropriate separation provided between taller buildings to maintain visual appearance from external view point and internal building conditions</li><li>• Ensure appropriate amenity is delivered to future occupants and people in the public domain</li></ul>
<p><b>Shaping The Skyline</b></p> <p><i>Create a new skyline for Sydney that works as a unified whole and is sympathetic to its surroundings. Ensure the precinct landmarks to have space to breathe and remain features as part of its composition.</i></p>	<ul style="list-style-type: none"><li>• Ensure transition of height of tower elements to create interest in skyline and responsive to contextual and amenity impact factors.</li><li>• Consider scale and massing of future buildings not only at a sub-precinct level but also how this will read as the broader precinct is delivered over time.</li></ul>
<p><b>Controls</b></p> <p><i>Ensure that known controls are considered and complied with as part of the holistic and place driven built form response. Known controls include the White Bay Power Station Conservation Management Plan and airspace protection regulations. Future controls will be defined through the rezoning process.</i></p>	<ul style="list-style-type: none"><li>• Ensure compliance with WBPS CMP controls on proposed heights</li><li>• Ensure compliance with aeronautical controls</li></ul>



# Design of Places & Spaces - Built Form

PERFORMANCE CONSIDERATIONS

REBALANCING NATURAL SYSTEMS

CONSIDERATIONS	
<b>Water Systems</b> <i>Prioritise the naturalisation of water systems at Bays West. Reinstating natural water systems must be considered alongside capturing, treating and improving water health across the precinct. Meet ambitious targets in relation to water management and the preservation of non-renewable water sources.</i>	<ul style="list-style-type: none"><li>• Ensure that future development delivers and considers:<ul style="list-style-type: none"><li>• Extent of permeable surface provisions within open space areas</li><li>• Allow for deep soil provision which ensures that significant tree planting and high infiltration rates of water can be achieved</li><li>• Deliver natural water management systems to address overland flow (associated with flooding) and water quality improvement (via slowing, retention and filtration strategies) for corridor connecting Beattie Street Channel to head of White Bay</li></ul></li><li>• Integrate comprehensive WSUD which minimises stormwater runoff from the site</li></ul>
<b>Habitat Creation &amp; Corridors</b> <i>Provide habitats for animals, insects and sea life to thrive, and promote green ecology and biodiversity throughout the precinct. Connect new habitats to surrounding areas, creating corridors that do not stop at the site boundary.</i>	<ul style="list-style-type: none"><li>• Design for protection of remnant areas of indigenous flora</li><li>• Allow for terrestrial new habitat created for native fauna</li><li>• Create water-based new habitats created for native fauna</li><li>• Allow for protection and habitat creation for the seahorse colony in White Bay</li></ul>
<b>Biophilic Environment</b> <i>Establish a biophilic environment at Bays West that creates a strong connection between the thriving natural systems and the people who engage with it. The natural environment that is fostered at Bays West will contribute to its unique place character.</i>	<ul style="list-style-type: none"><li>• Deliver canopy cover across the site, noting that the site should aim to exceed 25% urban allocation recommendation in GANSW Draft Greener Places Design Guide.</li><li>• Extent of proposed green façade and roof provisions</li><li>• Integrate indoor biophilic elements with buildings</li></ul>
<b>Restorative and Regenerative</b> <i>Demonstrate an understanding and care for Country by restoring and regenerating complete local indigenous ecologies within the precinct.</i>	<ul style="list-style-type: none"><li>• Ensure that landscape areas are dominated by native endemic planting</li><li>• Enable complete ecology establishment via low, mid and upper canopy elements</li></ul>
<b>Repair Damage</b> <i>Repair the damage that has been caused throughout its long industrial history and integrate this as a learning experience. Reparation to this place will see quick and significant value in the improvement of the place, starting with the treatment and containment of contaminants and supported green initiatives</i>	<ul style="list-style-type: none"><li>• Respond to and consider known and found contamination issues, recognising that this will need to be addressed at rezoning</li><li>• Integrate remediation stories within interpretation plan, noting that this connects communities to the site’s history</li></ul>

CLIMATE RESPONSIVE & SUSTAINABILITY PERFORMANCE

CONSIDERATIONS	
<b>Climate Risk and Resilience</b> <i>Design to mitigate chronic stresses and acute climate risks particularly those that are critical due to the relationship with water. Plan for expected and evolving scenarios and design place-responsive solutions, including the intersecting flooding, storm surges and sea level rise.</i>	<ul style="list-style-type: none"><li>• Respond to future climates through managing heat, bushfire (and smoke), flood and storm impacts through extreme events</li><li>• Deliver an integrated solution for flooding, storm surge and sea level rise which avoids reliance on total engineered solution and allows meaningful connection to water within a slowly changing landscape</li><li>• Deliver an integrated solution for WBPS and surroundings to provide flooding protections without overly isolating WBPS</li><li>• Ensure community facilities support social resilience during major shock events</li><li>• Ensure mitigation of climate risk</li><li>• Utilisation of flexible, adaptive and regenerative systems with the capacity to be changed subject to uncertain future pressures</li></ul>
<b>Urban heat mitigation</b> <i>Provide urban cooling through systems-based approaches that utilise both natural systems and materials-based approaches. Include canopy cover, water and air flow, cool building materials and approaches which increase the permeability and minimise amounts of hard ground finishes.</i>	<ul style="list-style-type: none"><li>• Ensure delivery of natural systems, building and public domain design and materials for best practice to mitigate urban heat island effects</li></ul>
<b>GHG Emissions &amp; Energy</b> <i>Target net zero emissions across energy, greenhouse gas and other port related emissions as an early ambition of the Bays West precinct in both construction and operation.</i>	<ul style="list-style-type: none"><li>• Allow for electrification operating systems</li><li>• Ensure energy efficiency and demand control within building design</li><li>• Ensure procurement of 100% renewable energy (on-site and off-site) and offsetting of all residual emissions from construction and operation with nature-based solutions.</li></ul>



# Design of Places & Spaces - Built Form

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### REBALANCING NATURAL SYSTEMS (CONT.)

CONSIDERATIONS	
<div><b>Circular Economy, Supply Chain &amp; Materials</b></div> <div><i>Establish processes and systems which are restorative and regenerative to eradicate the end-of-life concept. In doing so, work to design out waste, keep produce and materials in use in a closed loop with minimal finite resource inputs. Work in harmony with natural systems.</i></div>	<ul style="list-style-type: none"><li>• Promote longevity by allowing easy adaptive reuse to accommodate alternative occupancies</li><li>• Embed end-of-life considerations for building disassembly or long-term re-use</li><li>• Maximise of re-used, recycled, or renewably sourced materials in construction</li><li>• Provide spaces that facilitate sharing economy programs</li><li>• Divert the majority of construction waste from landfill to beneficial re-use</li><li>• Consider measures to be implemented which supports diversion operational waste from landfill</li><li>• Provide stewardship in procurement through procuring services rather than products</li></ul>
<div><b>Indoor Environmental Quality</b></div> <div><i>Provide high quality environments within buildings which address light, acoustics, thermal comfort and other best practice wellness approaches to support the experience of the users and contribute to liveability and productivity</i></div>	<ul style="list-style-type: none"><li>• Deliver quality indoor spaces in respect to access to daylight, acoustic performance, arterial road and ports and working harbour interfaces and thermal comfort</li><li>• Ensure other best practice wellness approaches</li></ul>
<div><b>Outdoor air quality</b></div> <div><i>Improve the outdoor air quality at Bays West through large-scale urban greening, the introduction of new public domain spaces and green streets. Innovative buffer zones can protect or filter air may be required at thresholds adjacent to arterial roads.</i></div>	<ul style="list-style-type: none"><li>• Achieve optimum local air quality</li><li>• Eliminate on-site combustion</li><li>• Allow for focused treatments on arterial road interfaces</li></ul>



# Design of Places & Spaces - Built Form

## 4.2 WHITE BAY POWER STATION (AND METRO)

BUILT FORM FOCUSED

Maximum heights have been defined by the White Bay Power Station CMP and viewsheds.

LEGEND

- WBPS Sub-Precinct Boundary
- Key Heritage Landmark
- WBPS CMP Boundary
- Previous Heritage Tracings
- WBPS Viewsheds - Mandatory View
- Skyline Zone of Influence
- Remaining Heritage Rail Tracks
- Key View to Harbour
- Future 'The Bays' Metro Station Box
- Future 'The Bays' Metro Station
- Access to Future 'The Bays' Metro
- Acoustic Impact Zone
- Permeability through ground plane required (additional streets or links)
- Proposed Zone of Development
- Additional Proposed Zone of Development
- Tunnel Zone
- Key Ground Plane Link
- Height Transition (arrow points to lower height of transition)
- Zone of higher development
- Primary surrounding roads
- Secondary surrounding roads

The Zone of Potential Development do not represent building footprint.

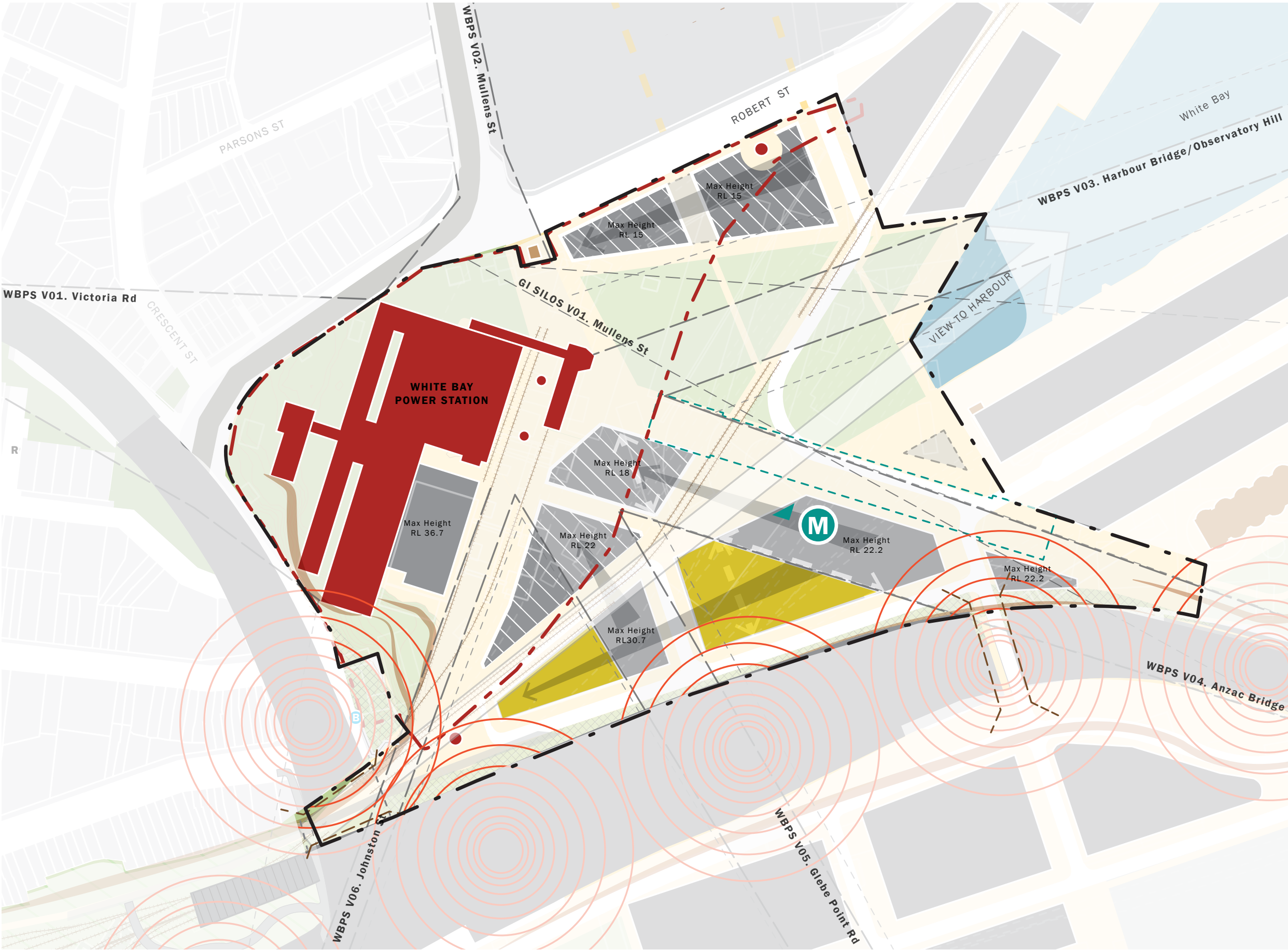


DIAGRAM 4.6 White Bay Power Station Sub-Precinct Built Form Plan



# Transport & Movement

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### PRIORITISED & APPEALING NETWORKS

CONSIDERATIONS		CONSIDERATIONS	
<b>Bicycle Highway</b> <i>Connect to the regional cycle network with a 'bicycle highway' that links the Inner West with Pyrmont, through the precinct, and along its main heritage and cultural spine to experience many of the place features.</i>	<ul style="list-style-type: none"><li>• Deliver a movement network which caters to high targets of walking and cycling throughout the delivery of Bays West – WBPS sub-precinct is crucial to this as it includes The Bays Station and regional east-west and north-south links.</li><li>• Provide logical and direct regional dedicated bicycle highway connecting from Rozelle Rail Yards to east to Pyrmont (to integrate beyond) with branch to connect to Balmain from both locations</li><li>• The bicycle highway must make cycling easy and enjoyable to maximise patronage. This should include allowing for experience of place through views of WBPS, views to harbour and Sydney Harbour Bridge, Power Station Park, views to Glebe Island Silos and onwards connection along Heritage and Cultural spine</li></ul>	<b>Using Place to Navigate</b> <i>Design the movement system to integrate landmarks and landscape features (water, landform) to aid with wayfinding. Celebrate the sequence of various intermittent views available along travel paths.</i>	<ul style="list-style-type: none"><li>• Provide access to views of water and Harbour Bridge along cycling and pedestrian</li><li>• Provide intuitive wayfinding within network design</li></ul>
<b>Direct Connections</b> <i>Provide direct connections within Bays West, linking origin and destination in a cohesive manner. This network should also provide seamless access to public transport.</i>	<ul style="list-style-type: none"><li>• Provide direct &amp; legible network connections for pedestrians and cyclists between access nodes, waterfront, heritage features, public transport access points, social/community infrastructure and public domain</li></ul>	<b>Prioritising Health &amp; Wellbeing</b> <i>The safety of pedestrians and cyclists at Bays West must be prioritised within the network, promoting not only efficient transport solutions, but the positive health and wellbeing benefits of using this mode.</i>	<ul style="list-style-type: none"><li>• Ensure cycling and walking links access known and anticipated active recreation facilities</li><li>• Support casual active transport which has been proven to deliver significant health and wellbeing benefits.</li></ul>
<b>Attractive</b> <i>Walking and cycling will be the most desirable option at Bays West. Appealing and attractive infrastructure that limits interaction with vehicle traffic, delays and extra exertion due to height and length changes, must be created.</i>	<ul style="list-style-type: none"><li>• Avoid conflicts between pedestrians, cyclists and vehicles</li><li>• Ensure safe and delay-limiting crossing points where cycling and walking links cross vehicle routes</li><li>• Deliver routes for walking and cycling that engage with an experience of place features</li><li>• Deliver dedicated cycling infrastructure provided (shared pathways avoided along commuter routes)</li><li>• Deliver bicycle parking provisions</li><li>• Provide end of trip facilities for cyclists both publicly facilities and privately within buildings which is in line with the high demands resulting from large numbers of cyclists</li></ul>		



# Transport & Movement

## PERFORMANCE CONSIDERATIONS

### 4.2 WHITE BAY POWER STATION (AND METRO)

#### LEVERAGING & SUPPORTING METRO

##### CONSIDERATIONS

<b>Optimising Access</b> <i>Create legible and direct connections to expand the catchment across and beyond the Precinct into surrounding suburbs to further widen the catchment area.</i>	<ul style="list-style-type: none"><li>• Create access nodes and connections for pedestrians and cyclists which provide direct access from along sub-precinct boundary to Rozelle and Balmain.</li><li>• Ensure future anticipated connections from other Bays West sub-precincts to metro station</li></ul>
<b>Smooth Interchange</b> <i>Establish an efficient and legible interchange experience between transport modes. Direct and logical connections, supported by clear wayfinding, will contribute to integration of multiple transport networks. Locate bus interchange near to key destinations, but in a location that does not disrupt key desire lines, activation opportunities.</i>	<ul style="list-style-type: none"><li>• Locate bus interchange and cycling facilities in close proximity to metro station without disrupting key desire lines and activation opportunities</li><li>• Enable transition of bus interchange location into preferred longer-term location as identified in structure plan</li><li>• Consider potential ferry stop location within White Bay with interchange to metro and buses</li><li>• Integrate shuttle provision to White Bay Cruise Terminal to cater for passenger access on cruise days within interchange arrangements</li><li>• Ensure kiss and ride plus taxi/ride share drop off in a position which minimises vehicle impacts with long term ability for vehicle routes to avoid crossing Power Station Park</li><li>• Provide weather protection for people interchanging between transport modes</li></ul>
<b>Connection</b> <i>Creating connections via the supporting public transport networks to cater for movement along other routes. The success of an ambitious mode share to public transport will rely upon an integrated and efficient network of connections.</i>	<ul style="list-style-type: none"><li>• Limit bus service routes which impact on adjacent areas and Bays West key place features</li><li>• Deliver a network which can operate for initial stage and long-term adjustments including potential terminating and through site bus services</li></ul>
<b>In Keeping With Place</b> <i>Ensure the design of the metro station, including service buildings and BOH facilities, are well integrated and positively contribute to the amenity and character of the precinct.</i>	<ul style="list-style-type: none"><li>• Integrate metro station entry and plaza into overall precinct design proposal and heritage response</li><li>• Minimise impact of station services on key place features (heritage, open space, key streets and activation opportunities)</li></ul>
<b>An Activator</b> <i>Design the metro station and associated public domain consistent with its key nodal role and generation of significant foot traffic. Ensure the design contributes to the activation of the precinct and creates anchor a hub of activity for the community.</i>	<ul style="list-style-type: none"><li>• Activate edges of buildings in zones along key access routes to metro station and key public domain areas adjacent</li></ul>

#### LOW CAR DEPENDENCY

##### CONSIDERATIONS

<b>Grades of Access</b> <i>Discourage the use of private vehicles within the Bays West precinct via various physical limitations and coordinated approaches to limit car parking provisions.</i>	<ul style="list-style-type: none"><li>• Prioritise access for people requiring mobility assistance and service vehicles</li><li>• Limit on-street parking provision to car-share schemes</li><li>• Provide ongoing small vehicle access for passengers to Cruise Terminal via Port Access Road connection</li><li>• Route all other White Bay traffic via Robert Street in line with current precinct transport approvals</li></ul>
<b>Limited</b> <i>Support reduced car reliance and discourage the use of private vehicle where not necessary via appropriate limitations and management controls. Incentivising alternative travel choice options through network design and promotion of behaviour change</i>	<ul style="list-style-type: none"><li>• Allow for new vehicle access points which balance needs of public transport efficiency with discouragement of use of private vehicles</li><li>• Deliver road structures designed to minimise conflicts with high pedestrian zones and key public domain areas</li><li>• Deter private vehicle use through limitations on access to parts of network, speed limits and limitation on both on street parking and parking in developments</li><li>• Evidence of parking provision and ability to shift parking controls (including pricing) to provide for longer term precinct mode share ambitions</li></ul>
<b>Protecting Place</b> <i>Enable Bays West to become a people-centric place, focused on place quality by limiting vehicle impacts within higher value place areas such as Precinct centre and open space areas.</i>	<ul style="list-style-type: none"><li>• Design a vehicle network which limits impact on areas identified as precinct centre and public domain zones</li></ul>
<b>Minimising Impact</b> <i>Ensure vehicle movements necessary for commercial, retail, delivery or other servicing and ancillary uses have limited impacts on high value places.</i>	<ul style="list-style-type: none"><li>• Create a strategy for deliveries and loading to minimise impact on major connections and public domain spaces</li><li>• Locate commercial, retail and any other proposed complementary and ancillary uses to ensure minimum impact on streetscape amenity</li><li>• Consider consolidated below grade loading zones shared between buildings to reduce volume of access points and limit impacts of traffic movements</li></ul>
<b>Transition</b> <i>Promote low vehicle use culture from the initial phases of the project and ensure it will normalise over a period of time while the precinct evolves, and while the user and worker population adapt to the shift in transport options.</i>	<ul style="list-style-type: none"><li>• Integrate long term goal to limit vehicle movements through public domain to bus services and after-hours maintenance and service vehicles into network design</li><li>• Deliver road structures to facilitate transition from initial access requirements to end-state solution when additional sub-precincts are delivered and other access opportunities are available (Rozelle Bay potential tunnel connection)</li></ul>



# Transport & Movement

## PERFORMANCE CONSIDERATIONS

### 4.2 WHITE BAY POWER STATION (AND METRO)

#### FORWARD LOOKING AND SUSTAINABLE

##### CONSIDERATIONS

<b>Mobility Shift</b> <i>Embrace alternative and shared forms of mobility, enabling vehicle sharing, ride-sharing and connected and autonomous vehicle interfaces via infrastructure provisions. The transition to shared services will be important in supporting a large resident and worker population within the precinct.</i>	<ul style="list-style-type: none"><li>• Ensure mobility transitions, including vehicle sharing, ride-sharing and other on demand transport forms, micro-mobility, and connected and autonomous vehicles have been provided for in initial stage and ability to transition to in long term</li></ul>
<b>Technology Trends</b> <i>Offer the electrification of road mobility options in the precinct design at Bays West, charging capacity, optimal data connectivity, the electrification of logistics services, and the ability to adapt to new and emerging technologies over time.</i>	<ul style="list-style-type: none"><li>• Deliver electrification of road mobility options</li><li>• Provide for emerging transitions in the freight network, including the electrification of logistics systems.</li><li>• Consider alternative off road building servicing possibilities</li></ul>
<b>Avoid redundant infrastructure</b> <i>Keep up with the constantly evolving data infrastructure and technology associated with vehicle use by considering flexibility in street and utilities infrastructure to allow for new and emerging technologies in the future.</i>	<ul style="list-style-type: none"><li>• Design streets and data infrastructure with flexibility to support future transport solutions</li></ul>



# Transport & Movement

## 4.2 WHITE BAY POWER STATION (AND METRO)

### BUS & ROAD NETWORK STAGING

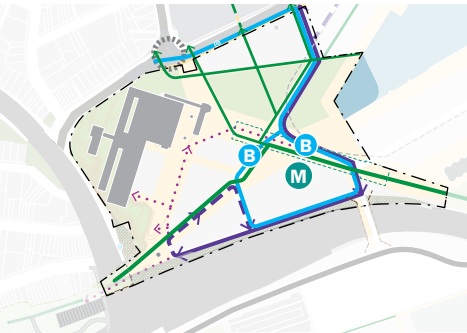


DIAGRAM 4.7 Initial Stage

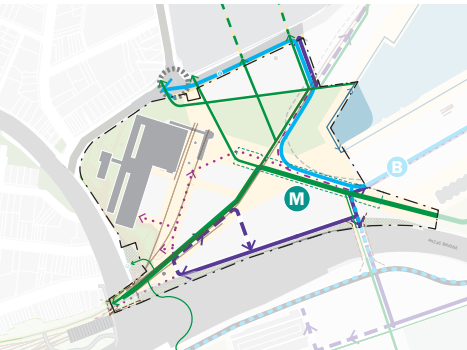


DIAGRAM 4.8 Long Term

- LEGEND**
- WBPS Sub-Precinct Boundary
  - Key Heritage Landmark
  - Proposed Active Transport Network - Key Link
  - Proposed Active Transport Network - Secondary Link
  - Potential Proposed Active Transport Links
  - Proposed Pedestrian Network Links
  - Foreshore Promenade
  - Bus Network
  - Bus Stops for interchange with metro
  - Future 'The Bays' Metro Station Box
  - Future 'The Bays' Metro Station
  - Access to Future 'The Bays' Metro
  - Intersection Treatment Required
  - Existing Ports Access Road
  - Limited Access - Emergency Vehicles and Maintenance Priority
  - Long term Limited Access - Emergency Vehicles and Maintenance Priority (End State only)
  - One-Way Local Street
  - Two-Way Local Street
  - Proposed Tunnel
  - Primary surrounding roads
  - Secondary surrounding roads

The Zone of Potential Development do not represent building footprint.  
BAYS WEST URBAN DESIGN FRAMEWORK  
MARCH 18, 2021

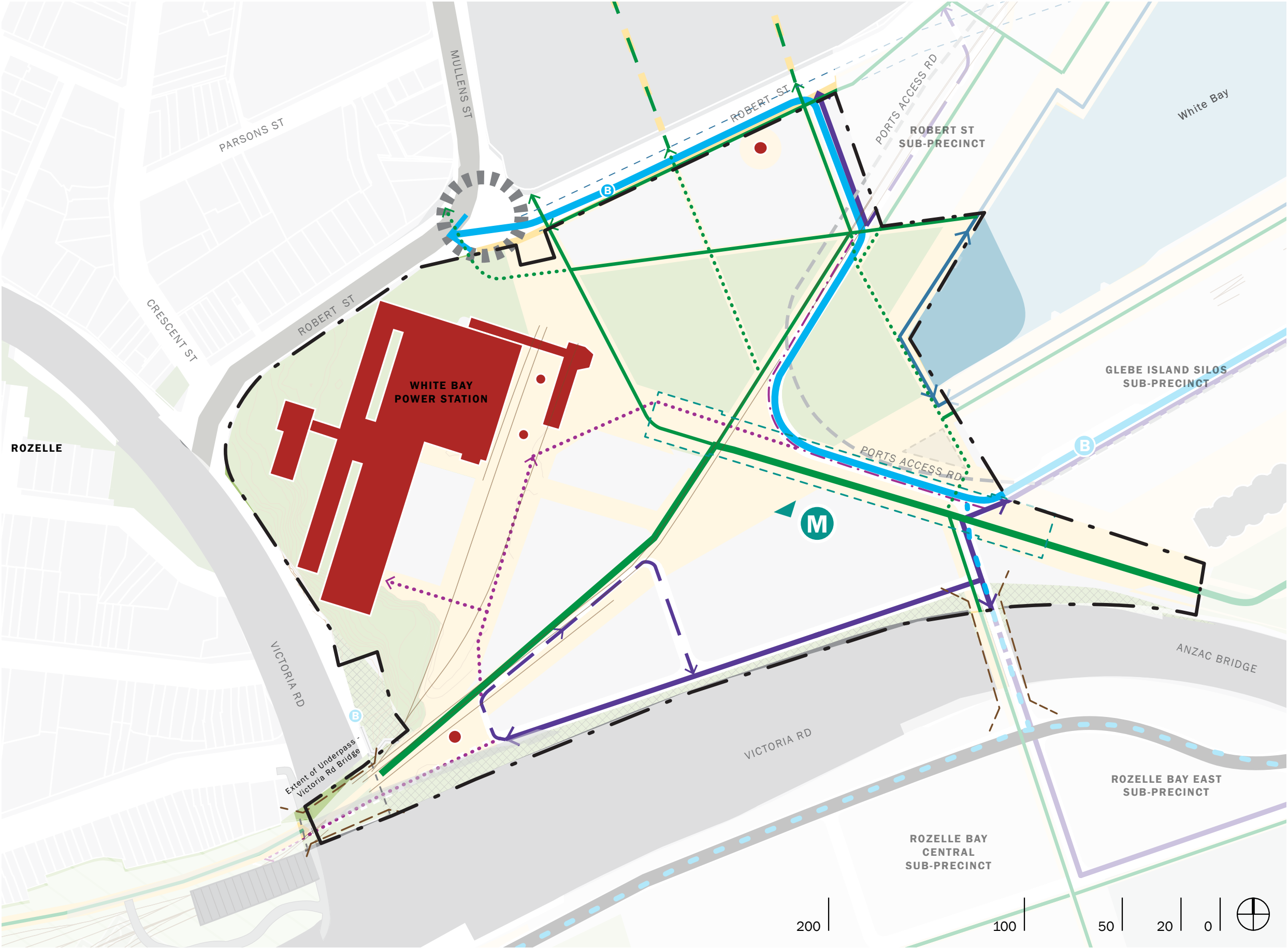


DIAGRAM 4.9 White Bay Power Station Sub-Precinct Transport & Movement Plan



# Heritage & Culture

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### REINFORCED HERITAGE AND CULTURAL SPINE

CONSIDERATIONS	
<b>Our White Bay Power Station</b> <i>The White Bay Power Station anchors one end of the heritage and cultural spine and is one of the most unique and celebrated assets at Bays West. It must be adaptively reused and reimagined as a focal point of the Precinct.</i>	<ul style="list-style-type: none"><li>• Conserve, adaptively reuse, and interpret the WBPS to a standard that is appropriate for a piece of nationally significant industrial heritage and in response to its identified cultural significance</li><li>• Create new insertions within WBPS that are clear, modern overlays on the historic structures, so as not to confuse their evolution or significance</li><li>• New works should do as much as is necessary for the care of the place and make it usable, but change as little as possible so that its cultural significance is retained</li><li>• Adapt, enrich and strengthen the integrity and significance of the place and its components rather than downgrading it</li><li>• Develop an appropriate framework of reuse for each of the parts of the WBPS to guide any proposals sought for future use. Framework to be in line with guidance contained within Conservation Management Plan</li></ul>
<b>Silo Transformation</b> <b>Re-building Bridges</b> <b>A Place of Ceremony</b> <b>Natural Elements</b>	<ul style="list-style-type: none"><li>• Protect and retain the future possibilities and characteristics of the key elements along the spine</li></ul>
<b>Networks and Links</b> <i>Connected to the major spine and beyond offer a network of places and features that can be integrated with this backbone. These can celebrate Country, art and cultural offerings, other adaptively reused elements, stories, and a variety of public domain spaces.</i>	<ul style="list-style-type: none"><li>• Retain connecting links and design features associated with spine via heritage features (current and former)</li><li>• Deliver new cultural and creative offerings along or linked to spine</li></ul>

#### THE PEOPLE’S ASSETS

CONSIDERATIONS	
<b>Public Access to Significant Features</b> <i>Provide public access to significant parts of Bays West landmarks that define its character and enable community engagement. Ensure that the public experiences are around the role and significance of these elements and integrate with any interpretation framework.</i>	<ul style="list-style-type: none"><li>• Enable public access to the WBPS to significant features deeply connected to the original operation of WBPS</li><li>• Integrate public access areas into overall interpretation framework which is linked into the master planning and public domain design across the site</li></ul>
<b>Public Domain Integration</b> <i>Integrate key public domain spaces in and around significant heritage landmarks and ensure public access. High-quality, welcoming and accessible spaces that enable engagement with and access to the rich cultural elements of the site, will provide great benefit to the public.</i>	<ul style="list-style-type: none"><li>• Deliver high quality logical and legible public access points to publicly accessible areas and integrate into public domain layout</li><li>• Activate public domain zones associated with WBPS uses and design proposals</li></ul>
<b>Public Offering</b> <i>Create a public offering within the significant parts and around significant heritage assets at Bays West to enable social, cultural and community benefit. Part of the heritage assets should consider public access and support for creative and cultural institutions.</i>	<ul style="list-style-type: none"><li>• Ensure publicly accessible uses within WBPS to designated public access areas</li><li>• Provide public access provisions (expansion of access to significant features and integration of community and cultural facilities prioritized)</li><li>• Allow for publicly accessible tours to all major zones of WBPS – particularly to enable access to all significant spaces where machinery is still located</li></ul>



# Heritage & Culture

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### A LIVING NARRATIVE

##### CONSIDERATIONS

###### Starting With Country

*Country must be considered as a fundamental of the precinct design from the beginning, through consultation, delivery, care, recognition and engagement with Indigenous culture and language, strengthening the connection with place.*

- Recognise that Country can drive decision making and that identified opportunities have been integrated and expanded upon in manner which recognized the value of these inputs
- Utilise indigenous language for naming of significant site elements including streets and public domain spaces in accordance with recommended protocols
- Integrate possibilities for honest acknowledgment of the impacts that the ways in which the site has been and continues to be used

###### Revealing Layers

*Embed interpretation and public art in the public domain and built form which will reveal the layers of history in a coordinated way. Express existing and former elements, sub-surface elements and other tangible traces of the past within the precinct structures.*

- Incorporate and interpret sub-surface components (including WBPS coolant water channels and power reticulation), former buildings/transport corridors and former shorelines have been integrated into overall structures

###### Telling Stories

*Deliver a site that integrates, interprets and conserves the wider site heritage elements into a cohesive story that can be understood by future users. Use digital platforms, integrated signage, artwork and remnant artefacts to illustrate the stories across the site.*

- Ensure the quality of an interpretation plan in response to Country and Post-colonial Era elements
- Ensure clarity of interpretation proposal in delivering a cohesive story
- Deliver interpretation of remnant elements, new public domain and new art proposals and ensure they have been integrated within interpretation proposal

###### Cultural Expression

*Creative culture creates richness, interest, vibrancy and identity in place. Build upon the Indigenous culture of the place, combined with its industrial and maritime history, and create places to enact culture as part of an evolving identity for the precinct.*

- Integrate permanent art elements temporary art installations
- Allow for spaces that enable forms of cultural expression to be practiced and performed
- Create spaces which enable teaching and sharing of Indigenous cultural practices – particularly in regards to holistic restorative sustainability

###### Beyond the Site

*Connect to heritage elements and areas within and beyond the site. Provide benefit from integration and connection, establishing heritage, creative and cultural networks and connecting places of heritage significance.*

- Provide links and connections which have the opportunity to extend beyond the site, supporting links to surrounding heritage features and trails, creative and cultural networks

#### INFORMING PLACE CHARACTER

##### CONSIDERATIONS

###### Setting & Curtilage

*Consider the appropriate setting and curtilages for heritage listed items and possibilities in relation to access and use. The setting of the power station, and indeed all other heritage assets, and their integration in the public domain has a significant impact on the place character. Respect the setting and curtilage of the heritage landmarks when considering adjacent built form.*

- Enable appropriate responses to WBPS. Refer to Conservation Management Plan (CMP) for detailed policies on immediately surrounding WBPS and consider extensions of appropriate responses in zones beyond boundaries of CMP)
- Respond to other remnant heritage attributes within the boundaries of site

###### Character and meaning

*Retain the character of the place to strengthen the sense of uniqueness and contribute to the attractiveness of the precinct. New development should acknowledge the historic industrial character of the site, and respond in a sensitive way without mimicry or losing authenticity. Weaving the new with the old will allow users to enjoy the discovery of an intriguing place.*

- Respond to identified Country stories and former industrial uses, former/current maritime uses of site, and surrounding areas

###### Waterfront connections

*Ensure the relationship to the water which each of the heritage elements is retained in a meaningful way. Interpretation of water conduits, channels, heating and cooling systems, dykes and other water systems will contribute to understanding of these layers.*

- Retain WBPS relationship to head of White Bay

###### Beyond The Heroes

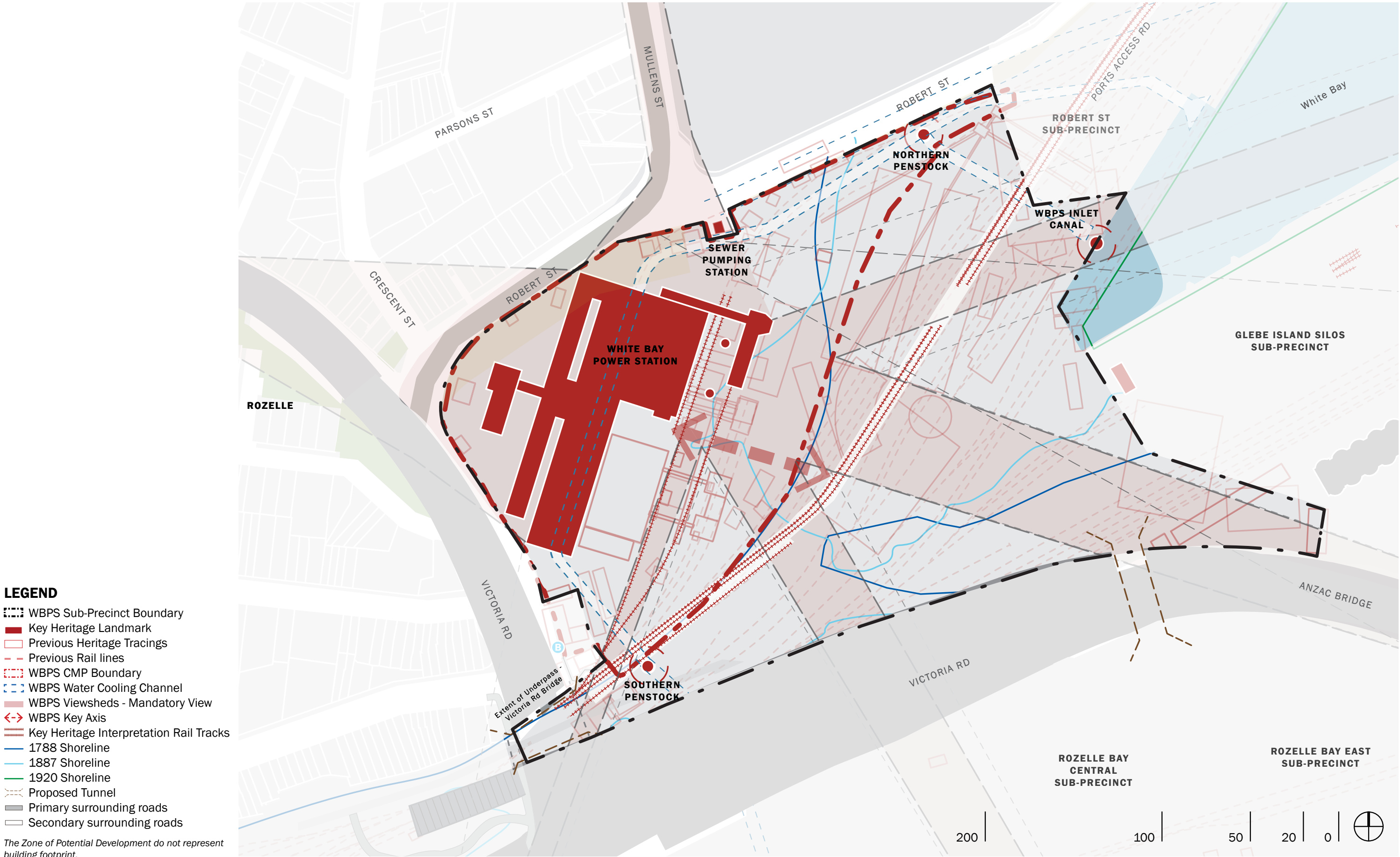
*Beyond the landmarks are a series of other unique but smaller, less obvious heritage remnants such as the penstocks, railway tracks, heritage canals, pumping stations and other built form associated with a long maritime and industrial history. Each of these assets can be reused, reinterpreted and revealed to contribute to the overall place character.*

- Ensure the finer grain heritage listed and identified attributes (penstocks, sewer pumping station, rail tracks) have inform the design response in their surroundings



# Heritage & Culture

## 4.2 WHITE BAY POWER STATION (AND METRO)





# Infrastructure Delivery & Governance

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### INTEGRATING PERSPECTIVES

##### CONSIDERATIONS

<b>Engagement</b> <i>Engage with existing and future community and stakeholder groups to be informed and improve the overall outcome. An understanding of how people will interact with the site, combined with a sense of belonging and ownership, will encourage engagement with the place long into the future.</i>	<ul style="list-style-type: none"> <li>• Ensure engagement with indigenous communities with association to Bays West and degree to which high priority issues have been addressed- Ensure engagement undertaken with broader community with interest in Bays West</li> <li>• Ensure engagement undertaken with Bays West stakeholders</li> </ul>
<b>Participation</b> <i>The perspective of a range of people including Indigenous groups, existing and future workers, future residents and the surrounding community, will support a deep understanding of the future of the place. Grass roots and community-led initiatives will assist in a range of voices being heard.</i>	<ul style="list-style-type: none"> <li>• Enable participation with On Country indigenous people within project team and ensure they are directly contributing to design and development proposal</li> <li>• Evidence of participation and ability to integrate community-led initiatives within proposal</li> </ul>
<b>Early Activation &amp; Temporal Uses</b> <i>Provide a living lab for ideas in the early phases of site activation at Bays West, where creativity and experimentation will be embraced. Enable public access to be unlocked early to allow users to have greater engagement with the site, long before the commencement or completion of the full redevelopment.</i>	<ul style="list-style-type: none"> <li>• Consideration of arrangements to integrate early activation into delivery including living lab initiatives and temporary or pop-up uses such as early testing of proposed techniques with public access arrangements and ongoing research project embedded into buildings and public domain</li> </ul>

#### COORDINATED AND INFORMED RESPONSE

##### CONSIDERATIONS

<b>Cohesive Goals</b> <i>Establish and regularly review a cohesive set of agreed goals and targets that are established via studies and strategies and informed by the precinct stakeholders. The end-state desired outcomes for the precinct must be balanced with the short-term and long-term strategic needs of the project.</i>	<ul style="list-style-type: none"> <li>• Align targets established at precinct-wide level including transport mode share, sustainability and climate change, noting other forms of targets may be developed during further investigation)</li> <li>• Align with precinct-wide plans for public domain, heritage and interpretation, connecting with Country</li> </ul>
<b>Establishing needs</b> <i>Ensure that the needs assessments across the various forms of infrastructure is undertaken and provisions are built into ongoing development and forward planning.</i>	<ul style="list-style-type: none"> <li>• Provide supporting infrastructure as identified within precinct-wide plan</li> <li>• Evidence that future supporting infrastructure provisions have been retained</li> </ul>
<b>Achieving Efficiencies</b> <i>Capitalise on opportunities for large-scale infrastructure projects to maximise broader benefits. Seek opportunities to solve multiple issues within larger scale proposals.</i>	<ul style="list-style-type: none"> <li>• Consider opportunities from other major infrastructure or government projects to integrate additional benefits for Bays West and surrounding areas</li> </ul>
<b>Planning for Staging</b> <i>Ensure efficiency in the planning and delivery of future stages, pairing resources and aligning timelines so that resources are optimised. Ensure that decisions relating to precinct coordination are made based on the best outcome, not cost-saving or current restrictions and will ultimately contribute to the long-term aspirations.</i>	<ul style="list-style-type: none"> <li>• Enable long term outcomes and avoidance of later redundancy by integrating staging considerations into design proposals</li> <li>• Avoid serious issues or poor transitions that need to be addressed in later phases of work</li> <li>• Coordinate across the boundaries of immediate metro station delivery to that of the broader sub-precinct</li> </ul>



# Infrastructure Delivery & Governance

## 4.2 WHITE BAY POWER STATION (AND METRO)

### PERFORMANCE CONSIDERATIONS

#### ESTABLISHING AND MAINTAINING PLACE

CONSIDERATIONS	
<b>Competitive and Attractive</b> <i>Establish a globally attractive brand that is highly magnetic in attracting investment and global talent. Create a place that businesses and investors want to be a part of, a place that fosters talent and will strengthen the NSW and Australian economy.</i>	<ul style="list-style-type: none"><li>• Provide an appropriate initial stage 1 quality to establish benchmark for Bays West long-term renewal</li></ul>
<b>Creating and Retaining Value</b> <i>Ensure that the innovation ambitions and total investment into Bays West is realised through ongoing management of the place quality to retain and expand on the value created</i>	<ul style="list-style-type: none"><li>• Consider long-term place management requirements including operating models, appropriate ongoing maintenance, and programming capability integrated into design</li></ul>
<b>Precinct Infrastructure and Plans</b> <i>Develop holistic precinct plans which enact the ambition for a sustainable digital precinct at Bays West and integrate the systems and infrastructure which can catalyse jobs creation and productivity. Utilise these to enhance social and environmental performance and disclosure through emerging digital technology and the internet of things.</i>	<ul style="list-style-type: none"><li>• Provide and plan for digital infrastructure and ongoing stewardship</li></ul>
<b>Assurance</b> <i>Incorporate assurance at Bays West across the multiple strands to provide stakeholders with confidence of project veracity across all stages of the project life cycle and into the future. Areas for assurance include development and operational governance, digital strategy, and precinct/built environment certification but others may be identified through the implementation process.</i>	<ul style="list-style-type: none"><li>• Plan external accreditation tools or mechanism(s) and identify targets in line with precinct wide aspirations</li><li>• Ensure clear appointment of responsibilities for each stage of the development and operations life cycle</li></ul>

#### BEYOND THE BOUNDARY

CONSIDERATIONS	
<b>Blended Edges</b> <i>Coordinate with stakeholders and landowners along the precinct boundary to ensure that opportunities for street upgrades, public domain interfaces/ integration and future plans for key lots/buildings can be considered in a synchronised manner.</i>	<ul style="list-style-type: none"><li>• Coordinate as necessary on Robert St carriageway and intersection upgrades (including addressing of flooding issue) with relevant stakeholders to achieve quality outcome for Bays West and surrounding area</li><li>• Coordinate as necessary with surrounding landowners and stakeholders on future possibilities for evolution of public domain and private lots relative to Bays West precinct changes</li><li>• Coordinate with Sydney Water for integration of sewer pumping station into design of northern forecourt of WBPS</li></ul>
<b>Realising Benefit Beyond Precinct</b> <i>Consider where new components of Bays West precinct can also provide substantial benefit to the broader community beyond the precinct and how this may influence alignments and positioning. The benefits that can be achieved through connectivity, open space, recreational and community/social infrastructure should be a particular focus.</i>	<ul style="list-style-type: none"><li>• Design for broader stakeholder needs within elements that can provide public benefit to areas outside of precinct including:</li><li>• Bicycle highway integration into broader network</li><li>• Early delivery of significant and high-quality harbourfront open space which integrates with place features</li><li>• Attractive infrastructure in prime and easily accessible locations for both precinct and broader community to access</li></ul>



# Infrastructure Delivery & Governance

## 4.2 WHITE BAY POWER STATION (AND METRO)

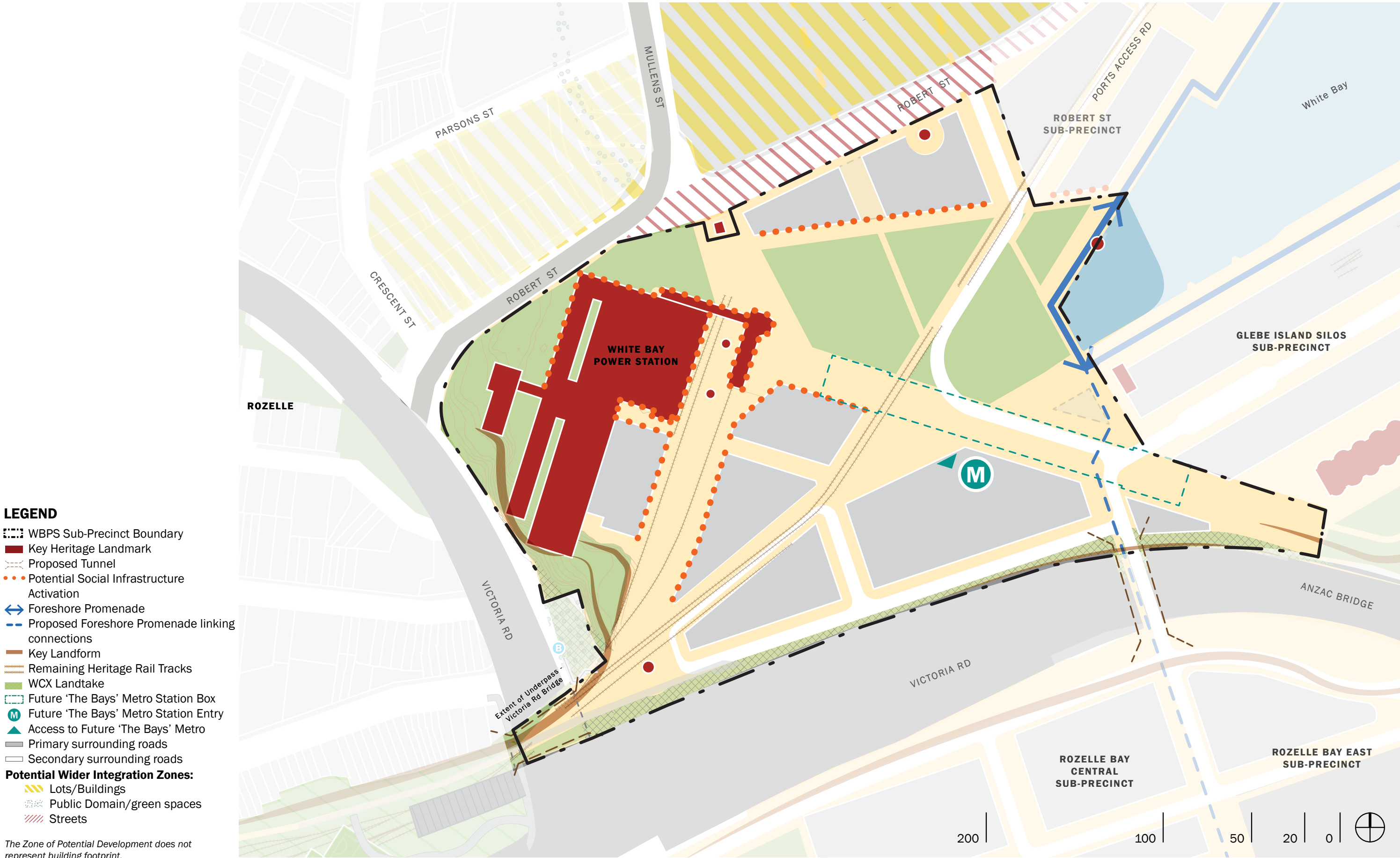


DIAGRAM 4.11 White Bay Power Station Sub-Precinct Infrastructure Delivery & Governance Plan





# 4.3

## ROBERT STREET



# Existing Place Character

## 4.3 ROBERT STREET

Robert Street is characterised by working harbour uses, its interface with White Bay Power Station and its suburban interface. There are currently marine contractors occupying a portion of the land towards White Bay.

### Land Use & Function:

- Marine contractors occupy the land and water berths on the White Bay interface
- The remaining land is currently unused for permanent uses, with temporary land takes fore the surrounding infrastructure projects
- There are light industrial and commercial uses along Robert St in the old warehouse buildings, as well as low density housing along the Robert St interface

### Design of Places & Spaces - Built Form:

- There is no significant built form within the sub-precinct
- The adjacent warehouse interface on the northern edge of Robert St defines the street character along Robert Street

### Design of Places & Spaces - Public Domain:

- There is no public access to Robert Street, both to the land or to the foreshore
- There is some existing low level vegetation and know flooding issues through this sub-precinct.
- On the interface with Robert Street is a sandstone escarpment and green space above, as well as a smaller green zone in front of the residential apartments.

### Transport & Movement:

- Access to Robert Street is restricted to Ports operators only, with the Ports access road running through to White Bay.
- Existing active transport network runs along Robert Street

### Heritage & Culture:

- Robert Street sub-precinct is adjacent to the White Bay Power Station and looks towards the Glebe Island Silos, two key heritage landmarks in the Bays West precinct
- The inlet canal and penstock are key heritage items with Robert Street precinct

### LEGEND

- ▬ Robert Street Sub-Precinct Boundary
- ▬ Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- ▬ Sloped Rubble Edge
- ▬ Timber Wharf Edge
- ▬ Concrete Hardstand Edge
- ▬ No Foreshore Access
- ▬ Key Landform
- ▬ Green Space

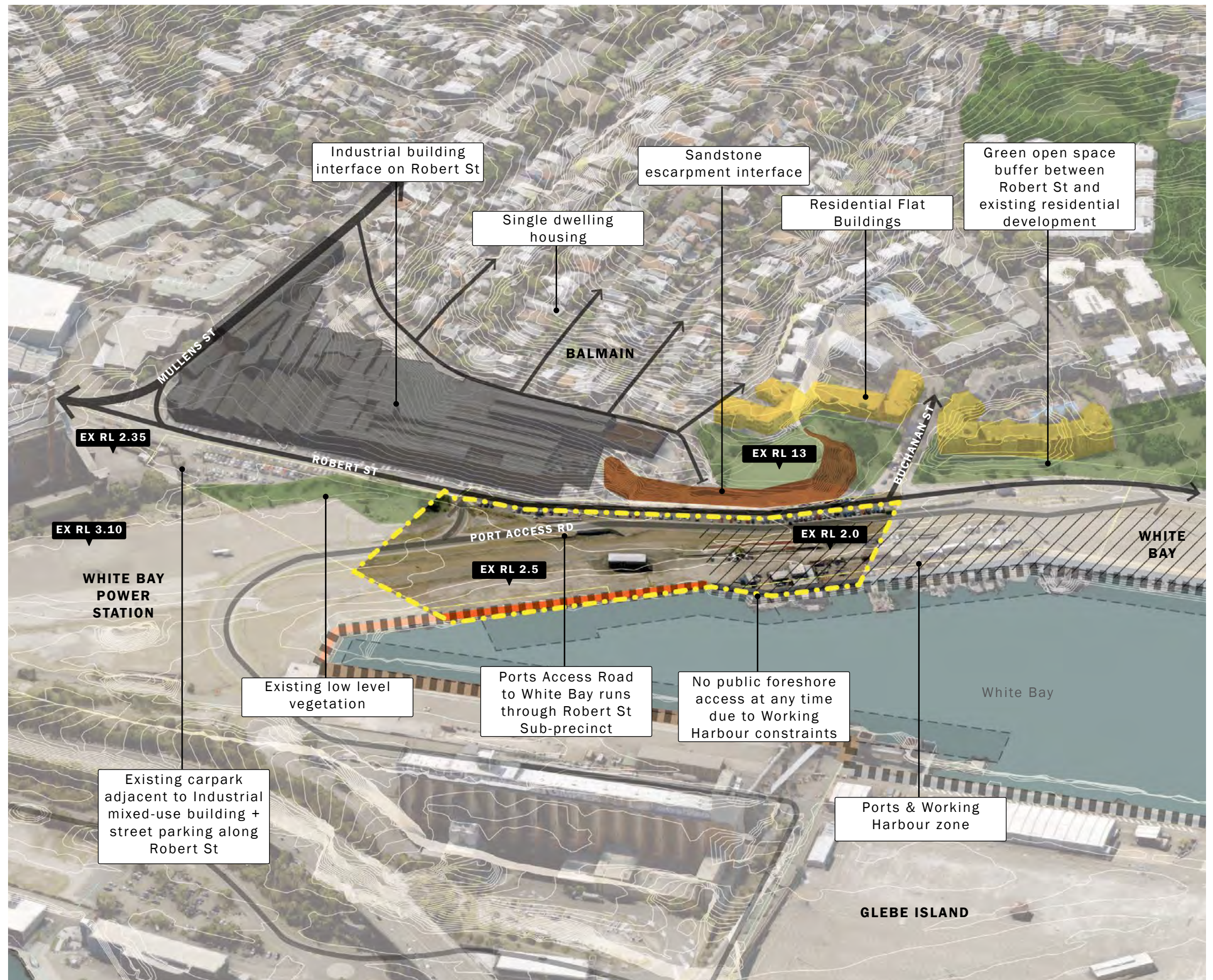


DIAGRAM 4.12 Robert Street Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.3 ROBERT STREET

### ROBERT STREET

*A key transition point, providing new access opportunities to the Precinct from Balmain and interface moments to White Bay...*

The Robert Street sub-precinct will see an expansion of the Precinct's new public offering and extension of its renewal around the head of White Bay from the Power Station and metro station. This sub-precinct will play a critical gateway role for the broader Precinct, establishing new connection opportunities between Bays West and the existing resident/worker population of the Balmain Peninsula, while also filtering access to the Ports operational zones in the adjacent White Bay sub-precinct.

The site will provide a highly porous ground plane and inviting pedestrian experience, with numerous opportunities to see and cut-through to the harbour foreshore from Robert Street. Low and medium scale mixed-use development, responsive to the existing neighbourhood scale of Balmain above, will provide a highly activated network of laneways and protect the recreational amenity of the foreshore promenade.

The existing sandstone escarpment to the north side of Robert Street will funnel pedestrians and cyclists down into the Precinct along Buchanan Street. An active transport link will connect this access node through the site towards the White Bay Power Station and metro station, allowing commuters to interchange with mass transit or continue through to the Rail Yards Parkland or over the Glebe Island Bridge to Prymont and the CBD.

The eastern boundary of this sub-precinct provides a unique interface moment with the Ports and maritime uses to be retained at White Bay. Landscape buffering along this edge will minimise amenity impacts on new development from port and maritime activities, while also restoring overland flow-paths and improving water quality within the bay.

The future evolution and consolidation of some uses within White Bay, would facilitate public access to continue through from the Robert Street sub-precinct along the White Bay foreshore (on non-cruise/special operations days). Other meanwhile or temporary public uses could also be easily integrated into these adjacent hardstand areas, such as special event infrastructure or linemarkings and removable nets/goal posts to provide the Precinct with additional outdoor playing courts.



IMAGE 4.26 White Bay Power Station Inlet Canal



IMAGE 4.27 Bar Noord / Fagrebi



IMAGE 4.28 Barrack Place / Architectus



IMAGE 4.29 Angel Place / Simon Wood



IMAGE 4.30 Kensington Street / Turf Design Studio



IMAGE 4.31 MoVida / Concrete Playground



IMAGE 4.32 Nordhavn, Copenhagen

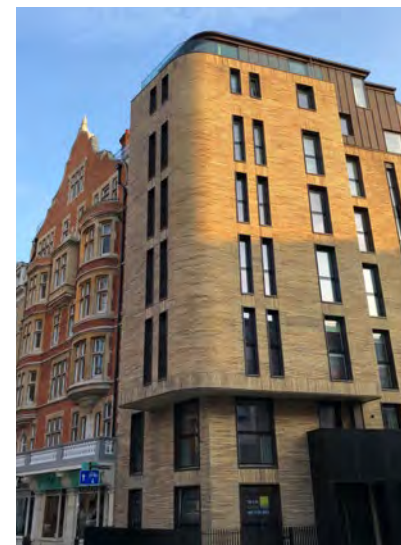


IMAGE 4.33 South Kensington

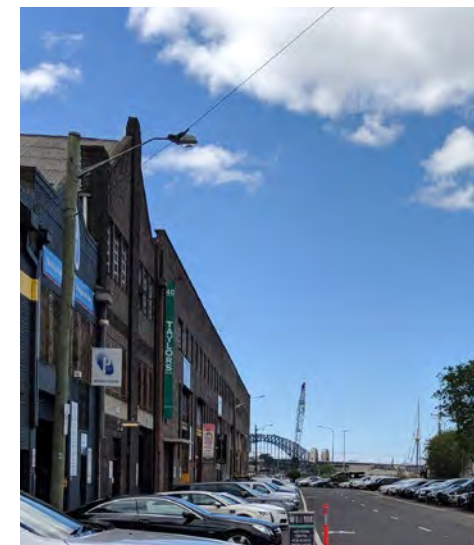


IMAGE 4.34 Robert Street



IMAGE 4.35 View from head of White Bay





# 4.4 GLEBE ISLAND SILOS



# Existing Place Character

## 4.4 GLEBE ISLAND SILOS

Glebe Island Silos is currently a working port zone, utilising the Grain silos. This sub-precinct also includes the Glebe Island Bridge, a key heritage landmark for the precinct. This sub-precinct has a number of level changes and topographic features including the sandstone outcrop, the highest point of Glebe Island.

### Land Use & Function:

- Glebe Island Silos is currently used for ports operations
- Gypsum Resources, Cement Australia and Sugar Australia are the current lease holders who utilise the silos and its surrounding hardstand and water berth zones.

### Design of Places & Spaces - Built Form:

- The key built form in this sub-precinct is the Glebe Island Silos, a key heritage landmark.
- There are a number of smaller buildings attached and associated with the silos but hold no heritage significance.

### Design of Places & Spaces - Public Domain:

- There is no public access to Glebe Island Silos, both to the land or to the foreshore
- The sandstone outcrop is a key feature of this sub-precinct, with scattering of existing vegetation

### Transport & Movement:

- Glebe Island East is accessed by the Ports access road which runs through to White Bay
- High truck movement in and out of Glebe Island
- Access to Glebe Island is restricted to Ports operators only
- There is an existing pedestrian and cycle off ramp/stair from Anzac Bridge which links down to Rozelle Bay

### Heritage & Culture:

- The Glebe Island Silos are a key heritage item in this sub-precinct.
- The Glebe Island Bridge is another key heritage item
- Other heritage items in this precinct include the last remaining former warehouse shed on foreshore at the head of White Bay and the Glebe Island Dyke exposure.

### LEGEND

- ▬ Glebe Island Silos Sub-Precinct Boundary
- Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- Sloped Rubble Edge
- Timber Wharf Edge
- Concrete Hardstand Edge
- ||||| No Foreshore Access
- Key Landform
- Green Space

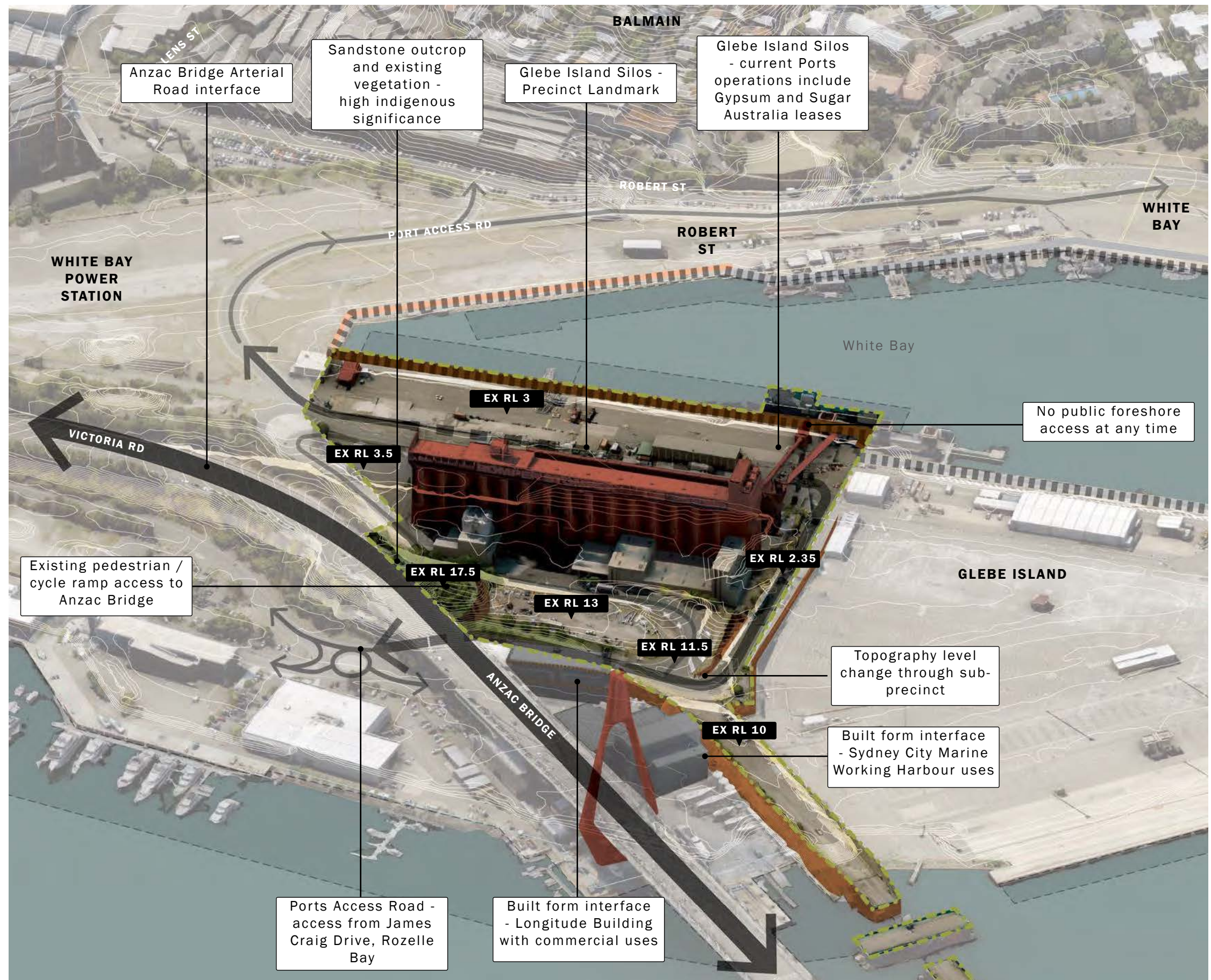


DIAGRAM 4.13 Glebe Island Silos Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.4 GLEBE ISLAND SILOS

### GLEBE ISLAND SILOS

*A celebration of landscape, culture, heritage, and people; the Silos as a living backdrop to a new ridgetop park and active waterfront...*

The Glebe Island Silos sub-precinct marks the natural high-point of the Precinct, and will express this by retaining and celebrating what remains of the former sandstone outcrop that was Glebe Island. Although no longer an island, the topography transition down to the water from a new ridgetop park will be expressed both at the ground plane and tapering of built form height towards the foreshore.

This area extends the local centre established within the White Bay Power Station (and Metro) sub-precinct, wrapping it around the head of White Bay and along a widened foreshore promenade. It will extend and promote the vibrant, 24/7 community and cultural hub of the Precinct, activating the waterfront with cafes, bars, and restaurants below mixed-use developments that offer housing and employment space, along with other social and community infrastructure.

In the future there is the opportunity to continue to transition the former grain silos that are the centrepiece of this sub-precinct as a marker of the Precinct's continuing history as a working port. With the renewal of this sub-precinct, the silos have been retained and can continue to be repurposed in line with their past evolution. Their strong structural presence in this new urban landscape could be softened through their occupation and the integration of new uses including cultural and community facilities. At ground level they can be more permeable, opening up to and activating the valley between them and the sandstone cutting, while also allowing access through and down towards the waterfront. New built form along the foreshore is sufficiently permeable to allow framed views back towards the silos, and is scaled in response to curtilage considerations.

The other key landmark element of this sub-precinct is the heritage listed Glebe Island Bridge. If reinstated, it could provide a key arrival gateway into the Precinct, re-establishing its connection to Pyrmont and providing a direct and high amenity active transport link for commuters to and from the CBD. As the bridge also serves as a way gateway to both Rozelle and Blackwattle Bays, and access will be retained to accommodate the movement of vessels to these areas. Other remnant structures and artefacts have been integrated into the site's renewed urban fabric, including the small heritage warehouse.



IMAGE 4.37 Vivid, Brewery Yard / Brett Boardman



IMAGE 4.38 Auckland Silos Park / Concrete Playground



IMAGE 4.39 Zeitz MOCAA / Iwan Baan



IMAGE 4.40 White Bay Power Station



IMAGE 4.41 The Wave / The Scarcity and Creativity Studio



IMAGE 4.42 Copenhagen Waterfront



IMAGE 4.43 South Bank, London



IMAGE 4.44 Nordhavn, Copenhagen



IMAGE 4.45 Glebe Island Sandstone Cutting





# 4.5

## GLEBE ISLAND CENTRAL



# Existing Place Character

## 4.5 GLEBE ISLAND CENTRAL

Glebe Island Central is currently a working port which is not accessible to the public. The Grain Silos and the Glebe Island Bridge are adjacent to this sub-precinct. Due to the industrial nature of the site, Glebe Island Central is a flat concrete deck.

### Land Use & Function:

- Land and water use primarily used for Ports operations.
- There are no designated berth zones along the foreshore of Glebe Island Central

### Design of Places & Spaces - Built Form:

- There is no significant built form in this sub-precinct
- Occasional temporary structures are erected on the concrete deck at Glebe Island Central

### Design of Places & Spaces - Public Domain:

- There is no public access to Glebe Island Central, both to the land or to the foreshore
- There are significant views to the surrounding precinct landmarks and to Sydney Harbour from Glebe Island Central
- There is a level change on the interface with Glebe Island Silos, with exposed sandstone cutting along that boundary

### Transport & Movement:

- Glebe Island Central is accessed by the Ports access road
- High truck movement in and out of Glebe Island
- Access to Glebe Island is restricted to Ports operators only

### Heritage & Culture:

- The Glebe Island Silos are a key heritage landmark for the precinct, sited adjacent to Glebe Island Central

### LEGEND

- Glebe Island Central Sub-Precinct Boundary
- Key Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- Sloped Rubble Edge
- Timber Wharf Edge
- Concrete Hardstand Edge
- No Foreshore Access
- Key Landform
- Green Space

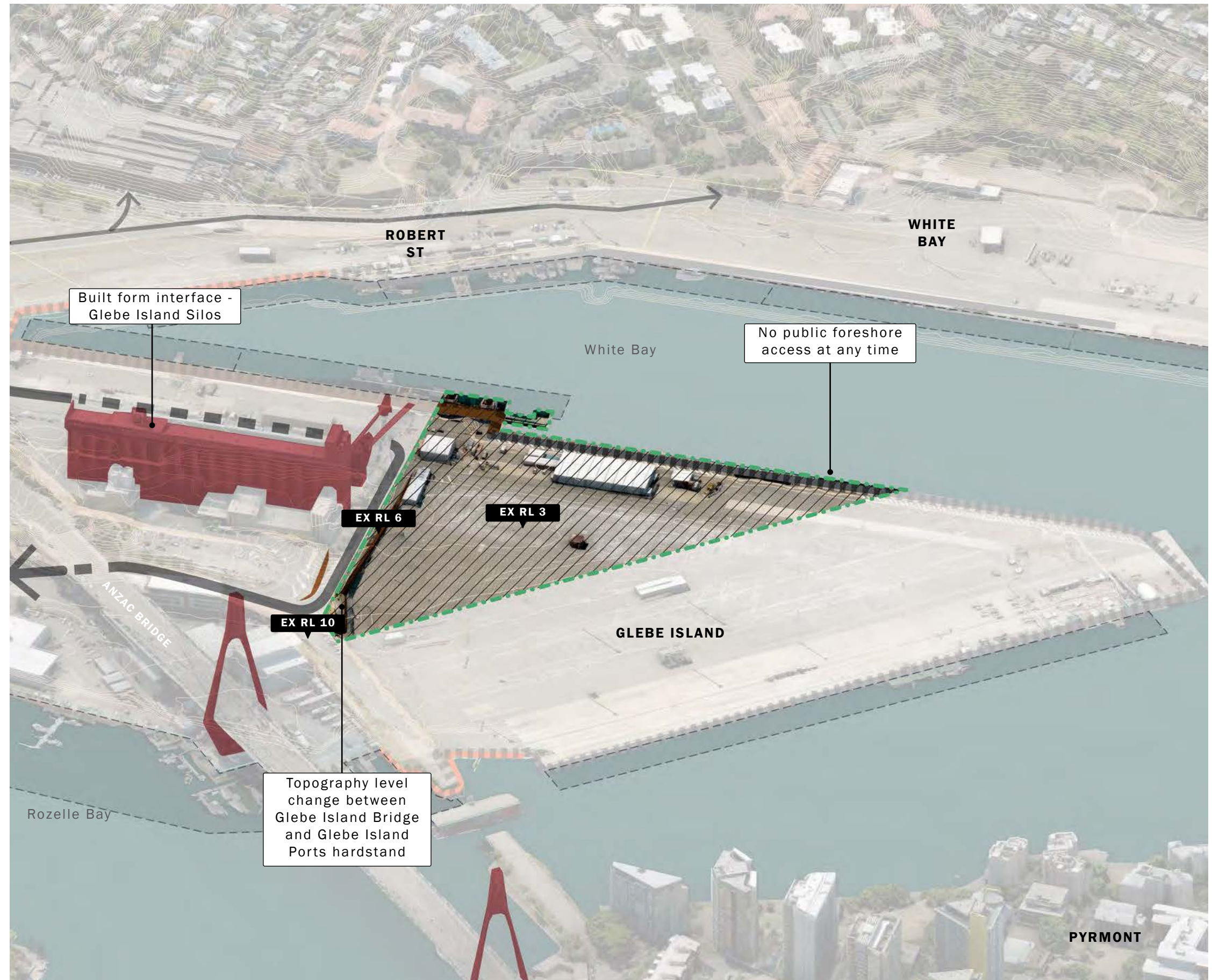


DIAGRAM 4.14 Glebe Island Central Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.5 GLEBE ISLAND CENTRAL

### GLEBE ISLAND CENTRAL

*A pocket of vibrant and innovative new development, expanding access to the water and creating a home for new industries...*

The Glebe Island Central sub-precinct will serve as the Precinct's epicentre of new development on Glebe Island. Its location allows it to incorporate a higher intensity and scale of built form, and the triangular site allows for some unique building geometries to be explored. Along the foreshore, the setback alignment of the adjacent Silos sub-precinct will be extended, and similar permeability and height transition requirements towards the water are imposed. Development lots not along the foreshore may pursue greater bulk and height, although overshadowing impacts towards public domain zones such as the Ports cap and Point Park in the adjacent Glebe Island East sub-precinct should be minimised.

At ground level, the site's edge condition is required to address a series of changes in topography. Along both its eastern and western boundaries, streets will transition down towards the waterfront from a high point aligned with the deck level of the Glebe Island Bridge. The western edge should provide level transitions across to the existing roadway within the Silos sub-precinct, and the eastern edge will engage with the new integrated ports facility, incorporating sleeved built form opportunities and a new local street down towards the water. As the existing grading of this site is flat, this elevated ground plane will provide a significant basement opportunity (without the need to excavate further).

In addition to providing an uplift in new development for the Precinct, this site enables some of the transport and movement networks to operate without conflict. A consolidated basement (potentially accessed via the new ports facility) would minimise need for servicing vehicles to travel on local streets and allow laneways between building lots to be shared use or pedestrian only; increasing their amenity and promoting activation opportunities. Bus services needing to terminate at the Precinct could also make use of this basement zone to turn-around or for layover parking.

The waterfront edge of the site will provide a further extension of the foreshore promenade from the head of White Bay out to the new Point Park at the tip of Glebe Island. Its northern aspect will facilitate high amenity waterside public domain and recreation opportunities.



IMAGE 4.47 Glebe Island North East Timber Wharf



IMAGE 4.48 Hafencity, Hamburg

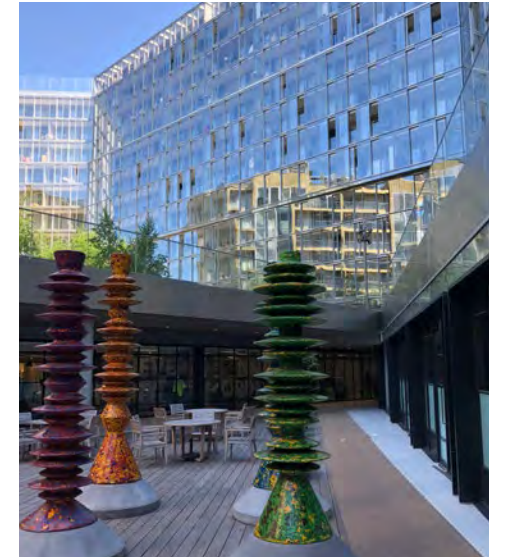


IMAGE 4.49 Battersea Development



IMAGE 4.50 Orchard Road / The Straits Times



IMAGE 4.51 Hafencity, Hamburg



IMAGE 4.52 Hafencity, Hamburg

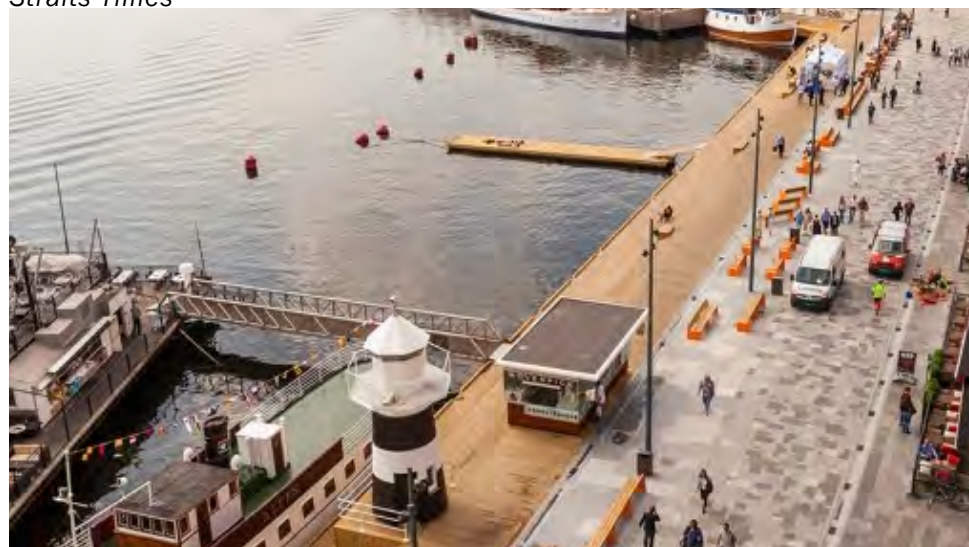


IMAGE 4.53 Aker Brygge / Thomasz Majewski

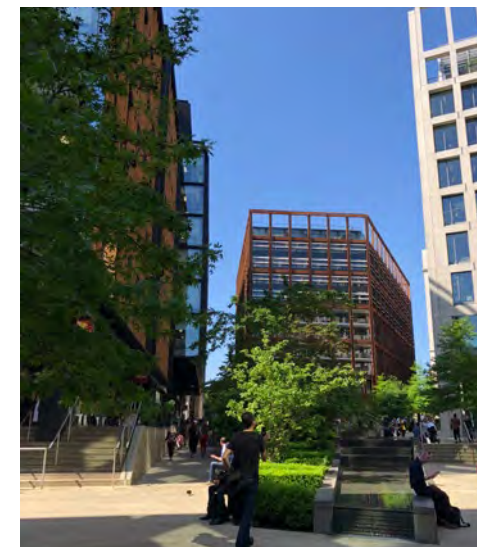


IMAGE 4.54 Kings Cross, London



IMAGE 4.55 Malmo, Sweden





# 4.6

## GLEBE ISLAND EAST



# Existing Place Character

Glebe Island East is currently a working port which is not accessible to the public. There are several functioning berths with large ships docked along the eastern foreshore at various times during the week. Due to the industrial nature of the site, the land is flat and surrounded by water.

## Land Use & Function:

- Glebe Island East is used as Ports land utilising both the land and water
- A batching plant and multi-user facility are proposed for this zone

## Design of Places & Spaces - Built Form:

- There is no significant built form within Glebe Island East, only a scattering of small ports buildings
- Adjacent built form at Jackson's landing consists mostly of high rise apartment buildings.

## Design of Places & Spaces - Public Domain:

- There is no public access to Glebe Island East, both to the land or to the foreshore
- There are significant views to the surrounding precinct landmarks and to Sydney Harbour from Glebe Island East
- Key landform features and topography changes at the interface to Glebe Island Bridge and the Glebe Island Silos sub-precinct

## Transport & Movement:

- Glebe Island East is accessed by the Ports road that is a continuation from James Craig Drive.
- High truck movement in and out of Glebe Island
- Access to Glebe Island is restricted to Ports operators only

## Heritage & Culture:

- Glebe Island Bridge is a key heritage item for the precinct as is directly adjacent to the sub-precinct, currently separated by a level change of approx. 7 meters.

## LEGEND

- Glebe Island East Sub-Precinct Boundary
- Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- Sloped Rubble Edge
- Timber Wharf Edge
- Concrete Hardstand Edge
- No Foreshore Access
- Concrete Batching Plant
- Multi-use facility
- Key Landform
- Green Space

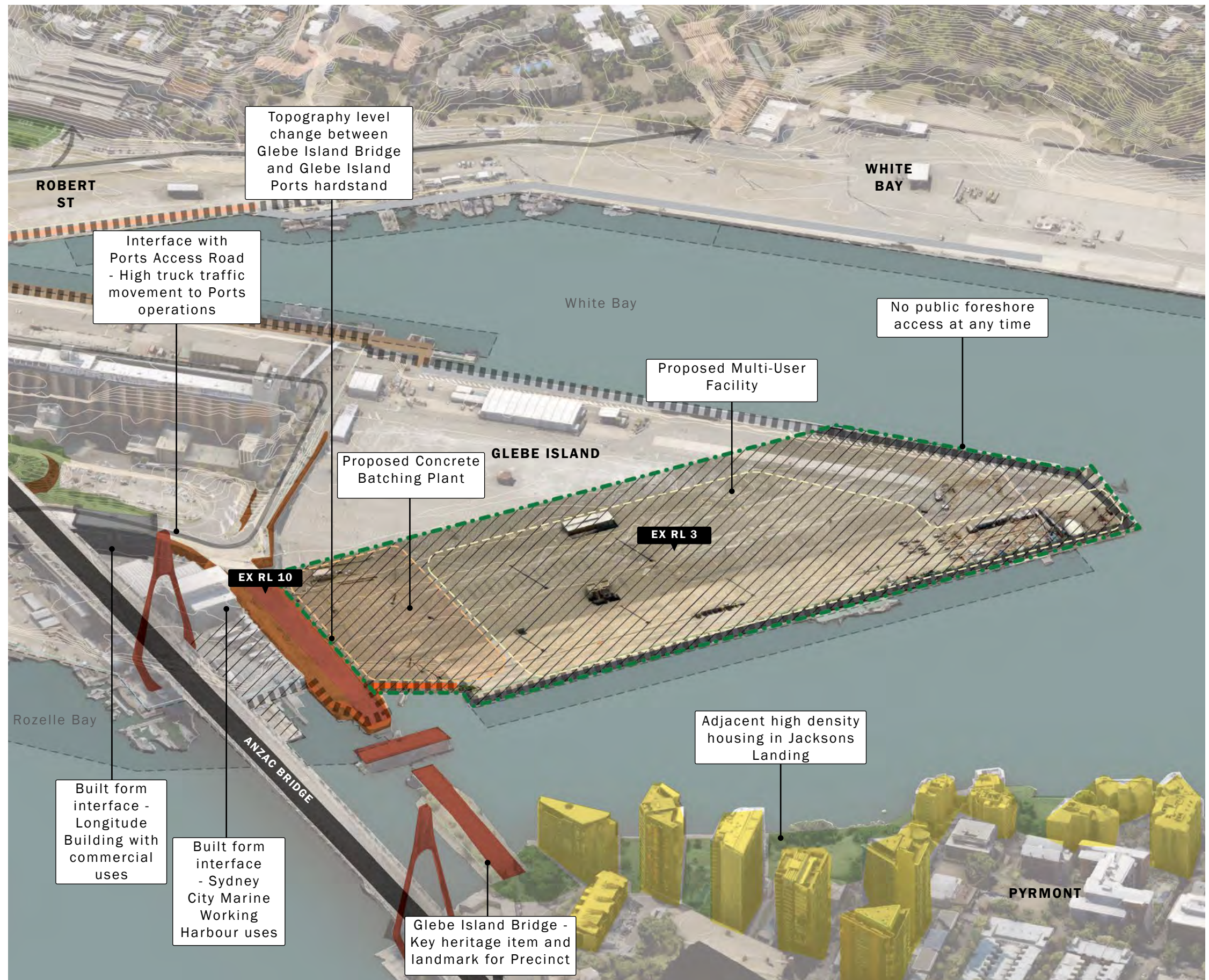


DIAGRAM 4.15 Glebe Island East Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.6 GLEBE ISLAND EAST

### GLEBE ISLAND EAST

*An integrated multi-user port operations facility for Sydney harbour, which provides new opportunities for Bays West...*

The Glebe Island East sub-precinct can see the existing bulk goods importation and handling operations on Glebe Island consolidated and relocated into a new purpose built facility along its eastern shoreline. This is a prime site to facilitate the retention of these activities within the harbour, as it offers deep-water berthing for two large vessels against high load bearing reinforced concrete wharfage.

The location minimises impacts to the walkability and place objectives of the broader Precinct. All heavy vehicle movements associated with these operations would be via Rozelle Bay, along the existing James Craig Road, with access to the new port facility via the existing Glebe Island Bridge underpass. Prioritising safety, by avoiding clashes with high pedestrian traffic areas and key public domain zones.

The topography transitions required to navigate between high points, such as the ridgetop park and Glebe Island Bridge approach, down to the water provides a unique opportunity to integrate this facility into the development. One solution for further investigation is installing a cap over the top of the facility, capable of accommodating other public uses and amenities, such as playing fields/courts and community gardens. The change in ground level can be navigated by sleeving the western edge of the facility with built form, integrating public access and activation to the new deck level and the street below.

While the ongoing port operations may limit public access to the waterfront, the need to transition up to the height of Glebe Island Bridge from Rozelle Bay would facilitate an elevated extension of the foreshore promenade along the edge of the proposed Ports cap. This could terminate at a grand outdoor public amphitheatre, terracing down to the new Point Park on the tip of Glebe Island, and continuing along the western foreshore back towards the head of White Bay and the Power Station.

The peninsula point park is a signature element of Sydney Harbour and its public urban landscapes. The new Point Park will represent the return of this part of Sydney's foreshore to the public, allowing it to be enjoyed by all while still accommodating the strategic maritime uses.



IMAGE 4.57 Vessel Berth at Glebe Island East / DPIE



IMAGE 4.58 Rooftop Farm / NYC Food Policy



IMAGE 4.59 Here East / London Connected



IMAGE 4.60 Glebe Island Bridge



IMAGE 4.61 Brooklyn Bridge Park / WFUV.org



IMAGE 4.62 Vessel Berth at Glebe Island East / DPIE



IMAGE 4.63 Musical on Sydney Harbour / Hamilton Lund



IMAGE 4.64 Wynyard Quarter, Auckland / Heart of the City



IMAGE 4.65 Hafencity, Hamburg





# 4.7 ROZELLE BAY EAST





# Existing Place Character

## 4.7 ROZELLE BAY EAST

Rozelle Bay East is currently characterised by the working harbour zones located under the Anzac Bridge and the TfNSW (formerly RMS) offices, hardstand and marina zone. The foreshore is accessible only at the Superyacht marina and partially in front of the TfNSW (formerly RMS) office building.

### Land Use & Function:

- TfNSW (formerly RMS) offices, hardstand marina and boat ramp are key land and water uses in Rozelle Bay East
- Superyacht marina multi-storey carpark and ground level carparks for TfNSW (formerly RMS)
- Working harbour elements include the Sydney City Marine lot which is to remain in its current location

### Design of Places & Spaces - Built Form:

- TfNSW (formerly RMS) office building and on ground carpark
- Superyacht marina multi-storey carpark
- Sydney City Marine building situated under Anzac Bridge
- Longitude building situated under Anzac Bridge

### Design of Places & Spaces - Public Domain:

- Existing vegetation buffer between Rozelle Bay East and Anzac Bridge on ramp
- Topography change between high arterial roads to the north of the sub-precinct, as well as to Glebe Island
- Key sandstone cuttings at the interface to Glebe Island

### Transport & Movement:

- No public transport offering in this sub-precinct
- High truck movement along James Craig Drive to Glebe Island and into TfNSW (formerly RMS) secure zone
- Only active transport network along roads
- Pedestrian and cycle access to Anzac Bridge via ramp + stair

### Heritage & Culture:

- The Anzac Bridge is a key landmark for the precinct which is sits above Rozelle Bay East
- The Anzac Bridge pylon sits between the TfNSW (formerly RMS) lot and Sydney City Marine

### LEGEND

- Rozelle Bay East Sub-Precinct Boundary
- Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- Sloped Rubble Edge
- Timber Wharf Edge
- Concrete Hardstand Edge
- No Foreshore Access
- Public Foreshore Access
- Rowing Course
- Key Landform
- Green Space

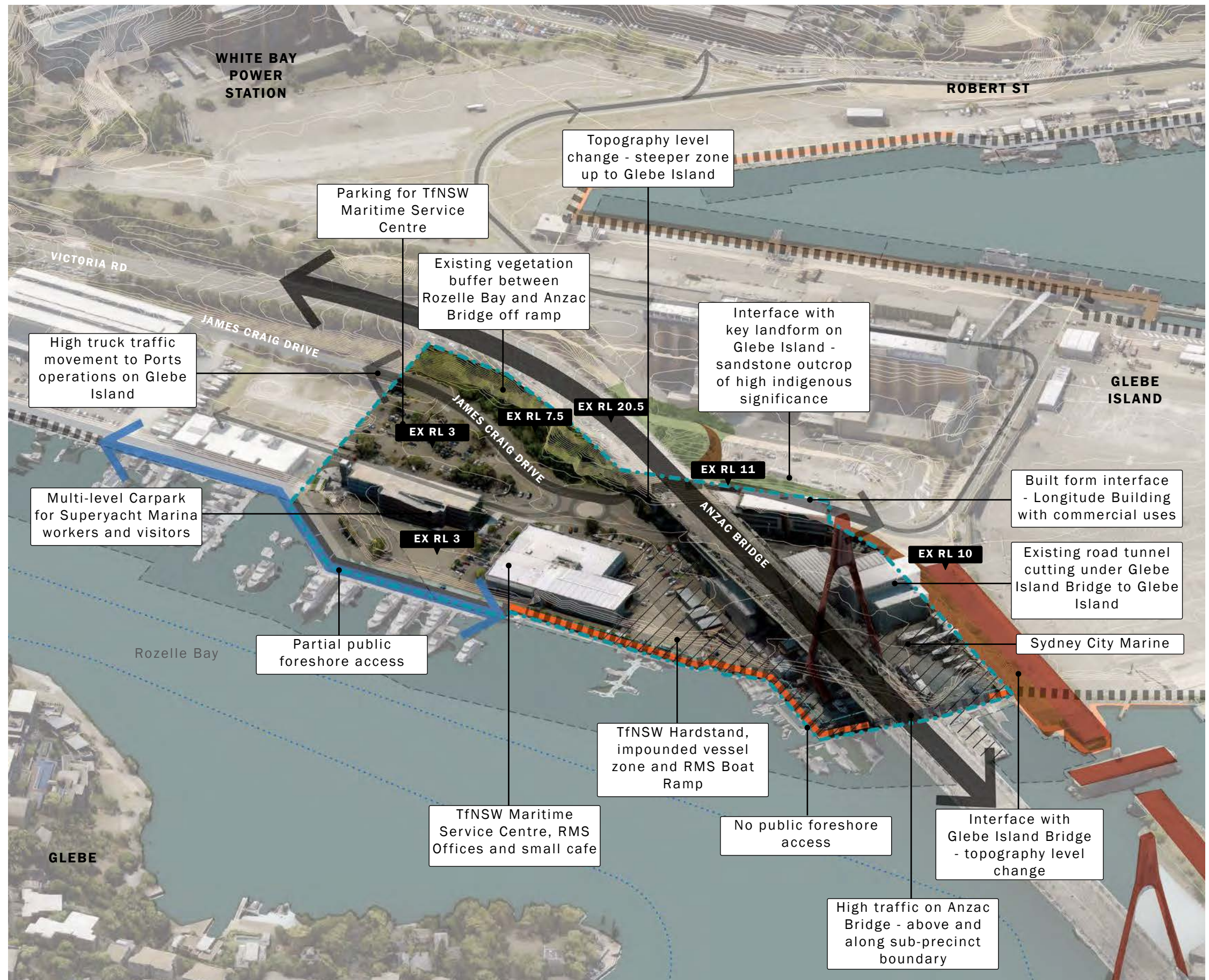


DIAGRAM 4.16 Rozelle Bay East Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.7 ROZELLE BAY EAST

### ROZELLE BAY EAST

an expanded home for the Precinct’s critical maritime and working harbour operations, and a complex web of movement networks...

The Rozelle Bay East sub-precinct represents one of the more challenging areas of the Precinct to stitch into the renewed public realm, due to its existing topographic and operational conditions. Its boundary is defined by a series of transport infrastructure elements, namely the Anzac Bridge on-ramp and the western abutment of the Glebe Island Bridge, with its waterfront edge already supporting a number of critical maritime operations and servicing contractors. As such, a complex stage of refinement and specific interventions are required to develop this site and align its place outcomes with the broader Precinct.

The eastern zone of the site, under the Anzac Bridge, will be consolidated into a multi-user marine contractors and vessel servicing operations hub. This allows the long-term retention of these critical operators within Rozelle Bay, co-locating them east of the new Rozelle Bay pedestrian bridge to minimise impact to their daily operations. Heavy vehicle access to this part of the site is accommodated on James Craig Drive, with the existing tunnel cutting under Glebe Island Bridge allowing vehicles to feed to and from the new integrated ports facility on Glebe Island.

New mixed-use development will populate this part of the Precinct, while providing noise and visual buffering between the waterfront and James Craig Drive. Built form generally tapers down towards the water, with curtilage and viewsheds observed to the Anzac Bridge, Silos, and White Bay Power Station. Buildings along the waterfront will provide activation opportunities to both the marina and foreshore promenade, and intuitively divert the public away from the water, up and over the new marine contractor facility to the level of the Glebe Island Bridge.

While the renewal of this part of the Precinct will see it refined to better accommodate public access and the integration of other uses, it must retain its working harbour character. The Precinct’s evolution and continuing history as a place of industrial and maritime significance should be embraced and celebrated in its renewal. Feature landform elements that reflect this should be retained and expressed, such as the steep sandstone cuttings as remnants of the former Glebe Island. Additionally, there is an opportunity to address the environmental impacts of some of these interventions.



IMAGE 4.67 Rozelle Bay East



IMAGE 4.68 NAB Headquarters / Globetrekimages, Flickr



IMAGE 4.69 Anzac Bridge Suport



IMAGE 4.70 King Street / Domain



IMAGE 4.71 Underside of Anzac Bridge



IMAGE 4.72 Rozelle Bay Marine Contractors



IMAGE 4.73 Delfshaven, Rotterdam

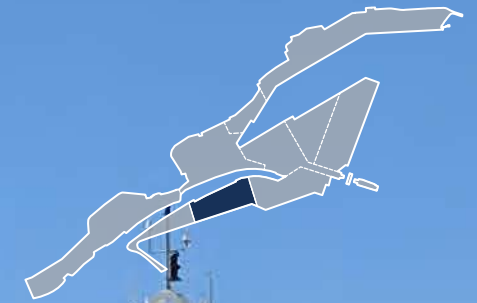


IMAGE 4.74 Rozelle Bay Sandstone



IMAGE 4.75 Entrance to Battersea Development, London





# 4.8

## ROZELLE BAY CENTRAL



# Existing Place Character

## 4.8 ROZELLE BAY CENTRAL

Rozelle Bay Central is currently characterised by the Superyacht marina, dry boat storage and commercial activity at the marina. The foreshore is accessible to the public only in front of the Superyacht marina and associated building.

### Land Use & Function:

- Superyacht marina uses include marina, commercial spaces (cafe) and offices
- The dry boat storage is located in Rozelle Bay Central with a current capacity of 300 boats, with associated hardstand, wet land marina and berths.
- Marine contractors also use the land and water within Rozelle Bay Central

### Design of Places & Spaces - Built Form:

- Superyacht marina building and multi-storey carpark
- Dry boat storage is the largest built form element in the sub-precinct, with no opportunity for integrated development in the future
- Small scale built form associated with marine contractors

### Design of Places & Spaces - Public Domain:

- Only public foreshore access in Rozelle Bay Central is in front of Superyacht Marina
- Low level existing vegetation buffer

### Transport & Movement:

- High traffic truck and large vehicle movement along James Craig Drive
- Sub-precinct constrained by major arterial roads
- Active transport network only along existing roads

### Heritage & Culture:

- White Bay Power Station Outlet Canal is a key heritage item in this sub-precinct

### LEGEND

- Rozelle Bay Central Sub-Precinct Boundary
- Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- No Foreshore Access
- Sloped Rubble Edge
- Timber Wharf Edge
- Concrete Hardstand Edge
- Existing Foreshore Promenade
- Public Foreshore Access
- Rowing Course
- Clement Marine Construction
- Sydney Heritage Fleet
- Key Landform
- Green Space

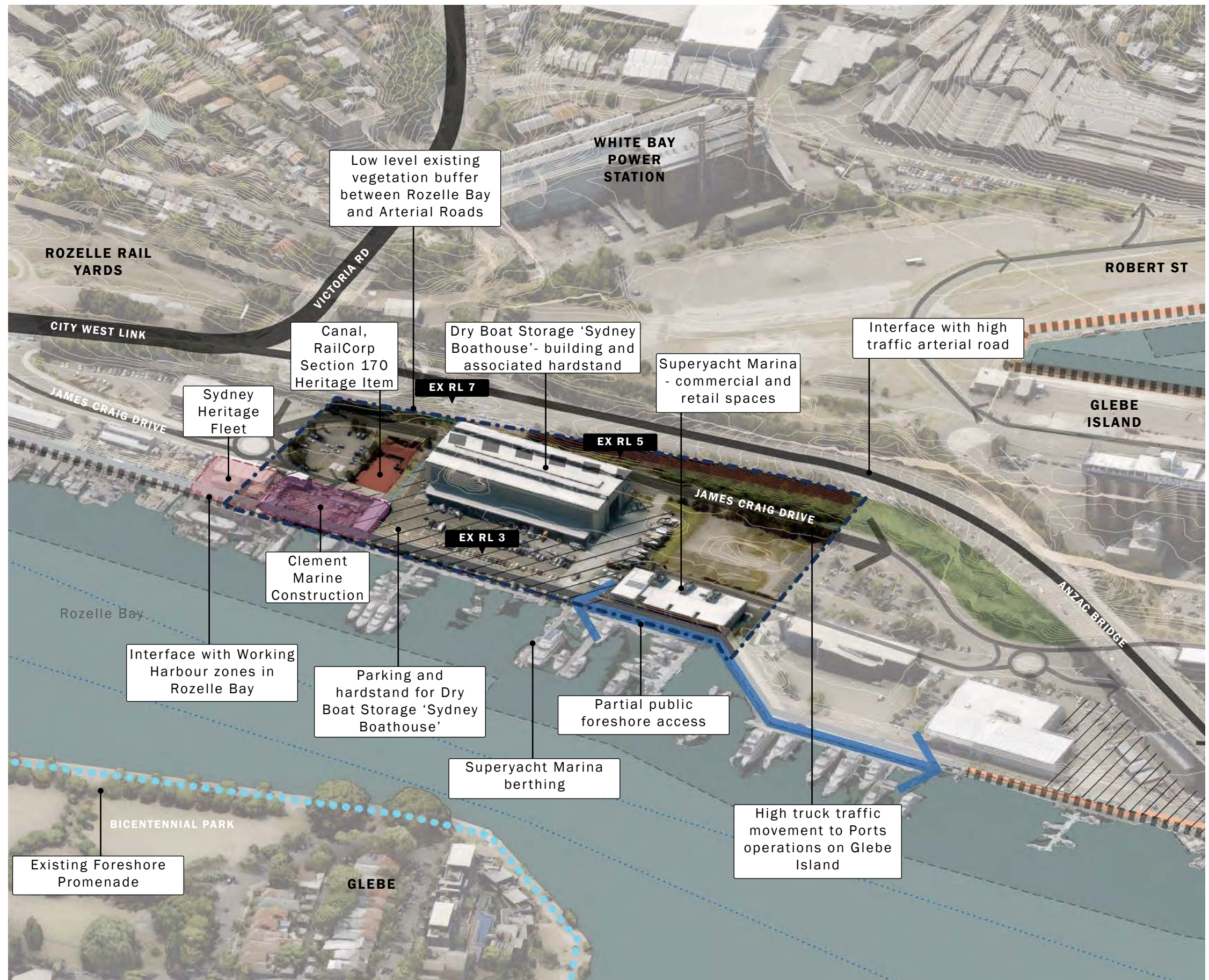


DIAGRAM 4.17 Rozelle Bay Central Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.8 ROZELLE BAY CENTRAL

### ROZELLE BAY CENTRAL

*A new point of arrival into the Precinct, stitching the history of former industry into the bustling heart of Rozelle Bay...*

The Rozelle Bay Central sub-precinct will be revitalised and its role in the Precinct elevated to serve as a key new point of arrival for Bays West. A new tunnelled connection under the Anzac Bridge on-ramp will provide a direct road and active transport link into the White Bay Power Station sub-precinct, feeding people to/from metro and the broader Precinct. The tunnel link will also offer a line of sight connection between the Rozelle Bay foreshore and the head of White Bay.

A future pedestrian bridge connection over Rozelle Bay would provide an additional access node for Bays West, linking the end of Glebe Point Road with the Precinct and expanding the walking catchment of the new metro station. Additional elevated active transport linking opportunities that bridge City West Link and connect Rozelle Bay to the White Bay Power Station site and the new Rozelle Railyards Parklands should also be considered.

In addition to City West Link, James Craig Drive will serve as the primary vehicular access corridor in the Precinct, and both will require new built form to act as a visual and noise buffer to public domain zones and the foreshore promenade. Built form will taper in height from east down towards the new Canal Park and towards the waterfront. Solar access to the foreshore is a key challenge, given the orientation, and the Canal Park offers a moment to provide this all year round.

The waterfront will maximise public access, the existing marina and dry boat storage facility will refine their operations to better integrate with the public domain and not preclude access to boat/vessel owners. Public berthings will be incorporated at the marina, and the Sydney Heritage Fleet will continue to operate from within the site - adjacent to Canal Park - with the public encouraged to visit and engage with the restoration works and tour the heritage vessels.

The new Canal Park will revitalise the heavily degraded heritage timber wharf structures around the existing canal. It will embed interpretations overlays to inform the public of the history of the site, and its significance in relation to the harbour and the White Bay Power Station.



IMAGE 4.77 Super Yacht Marina / Scott Carver



IMAGE 4.78 White Bay Power Station Outlet Canal, Rozelle Bay

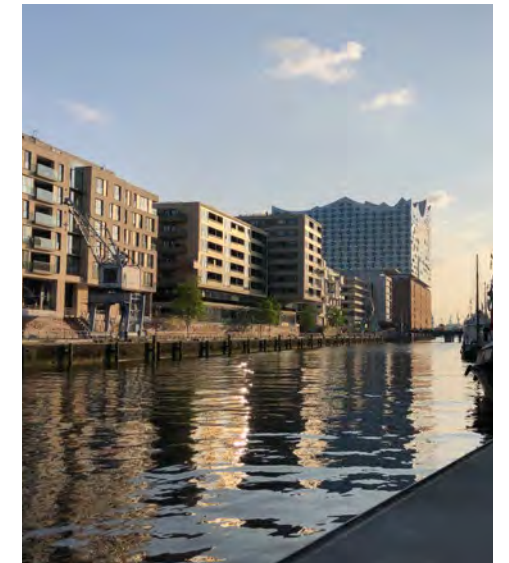


IMAGE 4.79 Hafencity, Hamburg



IMAGE 4.80 Copenhagen Waterfront



IMAGE 4.81 Park'n'Play / Rasmus Hjortshøj



IMAGE 4.82 Cuyperpassage, Cycle-Pedestrian Tunnel / Jannes Linders

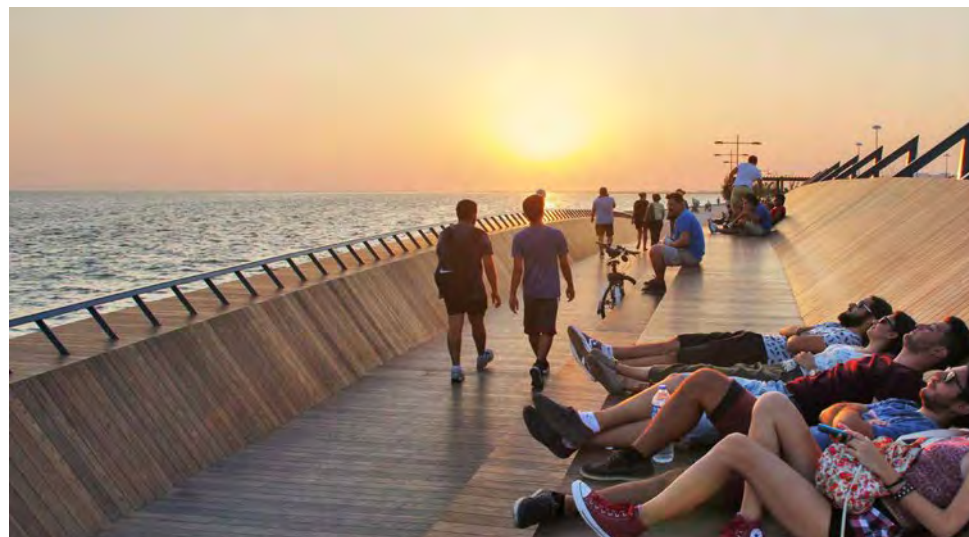


IMAGE 4.83 Bostanlı Footbridge / ZM Yasa

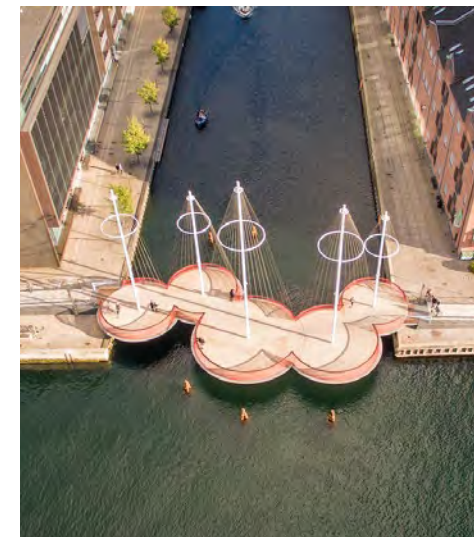


IMAGE 4.84 Cirkelbroen Bridge / Anders Sune Berg



IMAGE 4.85 Christianshavn Bridge / Thomas Rousing





# 4.9

## ROZELLE BAY WEST



# Existing Place Character

Rozelle Bay West is currently characterised by the marine contractors working harbour zone, the head of Rozelle Bay shallow water zone. The foreshore is not accessible to the public in this sub-precinct.

## Land Use & Function:

- Marine contractors are the primary users of both land and water in Rozelle Bay West
- On the western foreshore, a small boat hire and recreational vessel company is located
- TfNSW (formerly RMS) Drivers Aid is located within Rozelle Bay West, being critically located between James Craig Drive and City West Link.

## Design of Places & Spaces - Built Form:

- Existing built form in Rozelle Bay West are associated with the marine contractors, consisting of workshop sheds and office spaces
- Residential interface along The Crescent with light industrial / commercial buildings to the southern end

## Design of Places & Spaces - Public Domain:

- No public domain located with Rozelle Bay West
- Large green open space located adjacent to Rozelle Bay West at Federal / Bicentennial / Jubilee Park
- An existing rowing course is located in Rozelle Bay
- No public foreshore access in Rozelle Bay West
- Current active transport network along roads only

## Transport & Movement:

- Rozelle Bay Light Rail Station located adjacent
- Sub-precinct surrounded by high traffic arterial roads (with future WHT/WCX infrastructure to increase traffic volume)

## Heritage & Culture:

- The adjacent Johnston Street Railway Bridge / Viaduct and Annandale railway Bridge are the only listed heritage item surrounding the sub-precinct

## LEGEND

- Rozelle Bay West Sub-Precinct Boundary
- Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- No Foreshore Access
- Sloped Rubble Edge
- Timber Wharf Edge
- Concrete Hardstand Edge
- Existing Foreshore Promenade
- Rowing Course
- Key Landform
- Green Space
- Ⓛ Light Rail

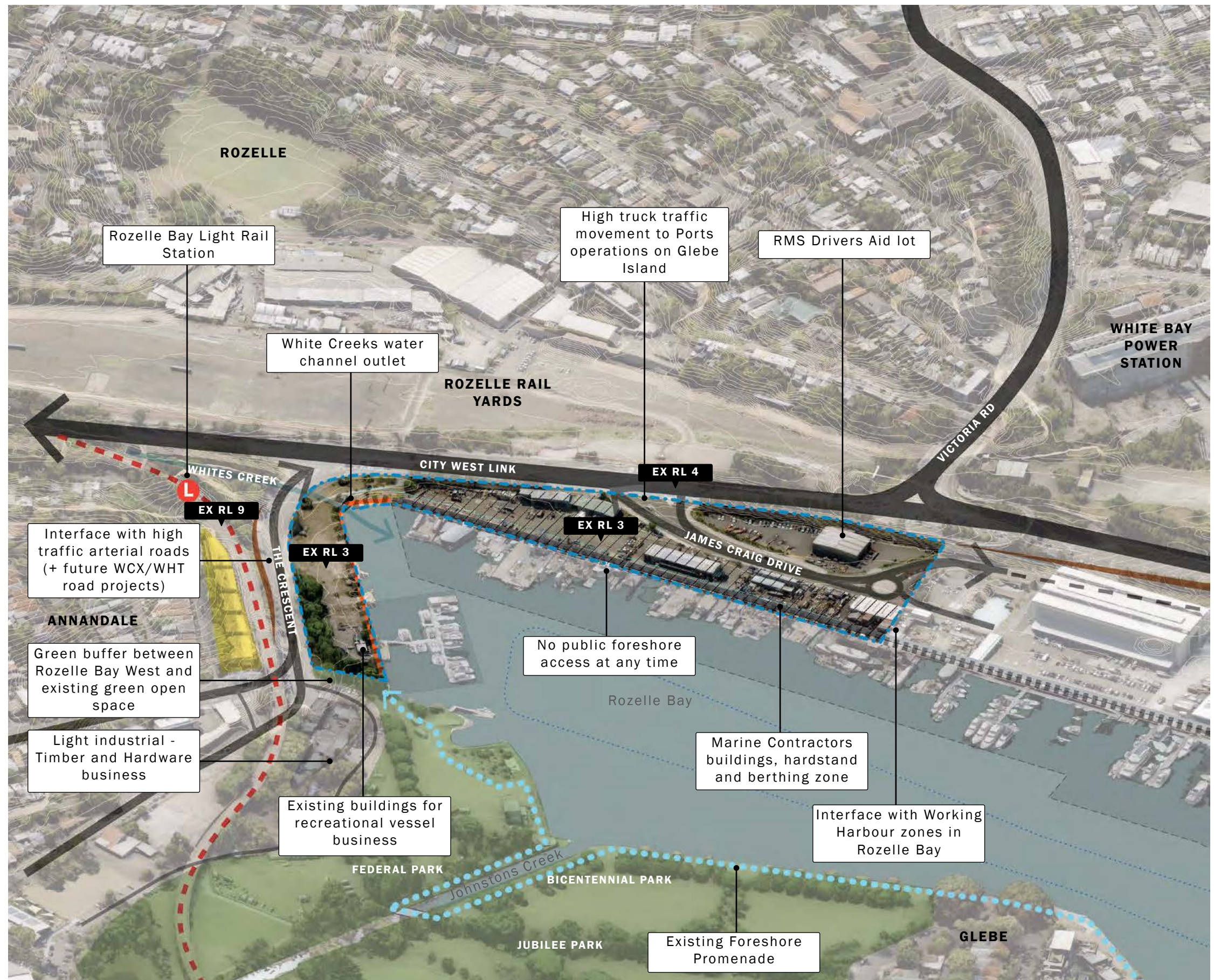


DIAGRAM 4.18 Rozelle Bay West Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.9 ROZELLE BAY WEST

### ROZELLE BAY WEST

*A place of restored natural beauty and ecological balance; creating opportunities for people to reconnect with the harbour...*

The Rozelle Bay West sub-precinct wraps along the foreshore and around the head of Rozelle Bay, separating the waterfront from the adjacent arterial roads. The renewal of this site provides a once-in-a-generation opportunity to address significant degradation of the former land/water ecologies and rebalance the natural systems in this part of the Precinct, restoring habitat zones and improving the general water quality of the bay.

With the relocation of existing marine contractors out of the head of the bay into their new consolidated facility under the Anzac Bridge pylon (Rozelle Bay East), new mixed-use development can act as a buffer along the City West Link. New built form will transition down in scale from east to west, with staggered alignments along the now publicly accessible foreshore and a fine grain character to buildings, reflective of the grit and textured quality of adjacent suburbs. Land-based operations (such as RMS Driver Aid) can be retained on the site and integrated into new development, allowing a direct feed onto James Craig Drive, the Precinct's primary vehicular access point.

At the head of the bay, a layering of environmental interventions provide a sheltered, restorative experience and breathe new life back into harbour ecologies. A dense vegetation buffer filters vehicle noise and emissions from The Crescent, and visually shields the promenade walk from the roadway. Water sensitive urban design initiatives have allowed the slowing and filtration of water discharging into the bay from Whites Creek and the new Rozelle Rail Yards Parklands. A boardwalk network over the water provides multiple opportunities for public engagement and direct access to the harbour.

This experience continues along the southern edge of Rozelle Bay, connecting the foreshore promenade into the existing network. The shallow water in this area allows for the reintroduction of sea grasses and other habitat restoration initiatives, reflecting the site's former estuary landscape and drawing on local Aboriginal knowledge and stories. Other opportunities to connect directly with the water include the use of small non-motorised water craft in this part of the bay, such as paddle boards, kayaks, and access to the existing rowing course.



IMAGE 4.87 Nordhavn, Copenhagen



IMAGE 4.88 Sydney Heritage Fleet



IMAGE 4.89 Landschaftspark, Duisburg-Nord / Thomas Berns



IMAGE 4.90 Hafencity, Hamburg



IMAGE 4.91 Rozelle Bay



IMAGE 4.92 Houtan Park / Tourenscape



IMAGE 4.93



IMAGE 4.94 Paddleboarding / Sydney Charter Boat Hire



IMAGE 4.95 Nordhavn, Copenhagen





# 4.10 WHITE BAY

IMAGE 4.96 Aerial view of White Bay Cruise Terminal / NEW  
BAYS WEST URBAN DESIGN FRAMEWORK  
MARCH 18, 2021



# Existing Place Character

White Bay is characterised by the working harbour uses, cruise terminal and boat storage. There is a long sandstone escarpment behind the Cruise Terminal which is of significant value to the character of the site.

**Land Use & Function:**

- White Bay is a working port, operated by Port Authority NSW
- The cruise terminal has both a primary and back up cruise berth water area
- All other land and water use in the White Bay sub-precinct are primarily working harbour operations - these include marine contractors, vessel refueling, special events/major construction project zone and charter vessels.

**Design of Places & Spaces - Built Form:**

- The White Bay cruise terminal is a key built landmark
- Other existing built form in White Bay are associated with Ports operations.
- Residential house sit on the northern interface to White Bay above the sandstone escarpment.

**Design of Places & Spaces - Public Domain:**

- No public foreshore access at any time
- Long sandstone escarpments along the northern boundary separate White Bay from the Balmain Peninsular above
- Birrung Park is a key green space on the northern interface

**Transport & Movement:**

- When the Cruise Terminal is in use it generates high traffic movement in and out of the sub-precinct via Robert St
- Ferry stop located in White Bay for Cruise Passenger transport, only available on Cruise days
- Road access via Robert St only - road closed off on non-cruise days and operated by Ports

**Heritage & Culture:**

- Former Lever Brother Factory (now part of apartment complex) creates a key heritage zone adjacent to White Bay and an opportunity to connect White Bay to Balmain.

**LEGEND**

- White Bay Sub-Precinct Boundary
- HERITAGE:**
- Key Heritage Landmark
- Road
- Industrial Buildings
- Residential Housing
- Berthing Zones
- No Foreshore Access
- Sloped Rubble Edge
- Timber Wharf Edge
- Concrete Hardstand Edge
- Key Landform
- Green Space



DIAGRAM 4.19 White Bay Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.10 WHITE BAY

### WHITE BAY

*A strategically significant land/water interface, enabling greater public access and new use opportunities for future Sydney...*

The White Bay sub-precinct will retain its current role as one of Sydney Harbour's last active common-user wharf facilities with deep-water berthing. Specific land and water uses will continue to evolve over time, with an emphasis on retention of the Precinct's working harbour character and the flexibility to accommodate the strategic ports and maritime operations long into the future.

The signature structure of the White Bay sub-precinct is the Cruise Terminal building, which will continue to provide local and international incoming and outgoing activity for cruise vessels. This facility increases the capacity for cruise liners in Sydney Harbour. The Cruise Terminal can also expand its events mode, hosting a wide variety of public and private events and more directly integrated into the higher amenity provided across Bays West.

The advent of metro within the Precinct provides a dramatic mode shift with new opportunities to transport people to and from the site and the Sydney CBD. An on-demand shuttle bus connection between the metro station and the Cruise Terminal would provide cruise passengers with a direct and convenient public transport option, and dramatically reduce reliance on private vehicle drop-off/pick-up. On non-cruise days, there is an opportunity to increase the utilisation of the terminal building, accommodating functions and special events, and opening up the foreshore and carpark to the public.

The eastern tip of the site will continue to operate as an active commercial marine servicing yard and on-water refuelling point. The relocation of other existing marine contractors to this zone could free up the western end of the foreshore and increase public access opportunities, including new links to Balmain. The western half of the site provides common-user berthing and hardstand areas, which when not utilised for ports operations and logistics handling, would allow the extension of the foreshore promenade from the head of White Bay out towards the Cruise Terminal.

The northern edge of the site is defined by the steep sandstone escarpment, reflective of the site's history of man-made interventions. There are significant opportunities to increase active transport connections with the surrounding areas and create a green corridor on a section of the existing Robert Road.



IMAGE 4.97 White Bay Cruise Terminal / Record Doors



IMAGE 4.98 Aerial view of White Bay Cruise Terminal / TTW

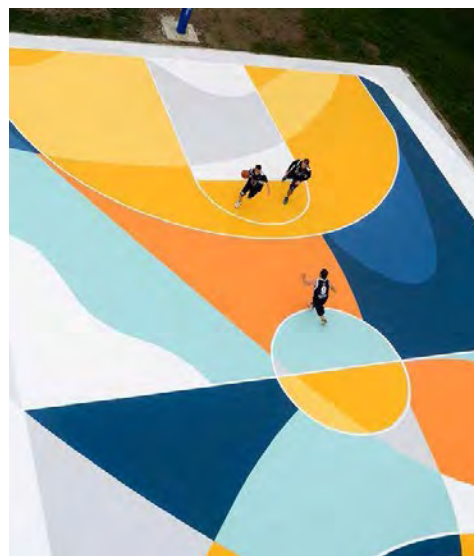


IMAGE 4.99 Playground / Ugo Galassi



IMAGE 4.100 White Bay waterfront and CBD



IMAGE 4.101 Berthed Cruise Liner at Cruise Terminal viewed from Balmain East

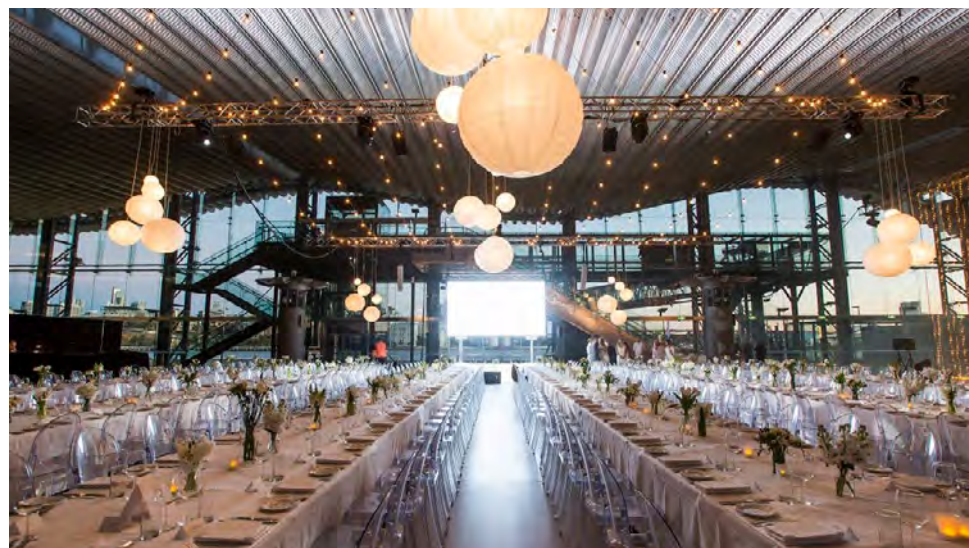


IMAGE 4.102 White Bay Cruise Terminal Interior set for event / Laissez-faire

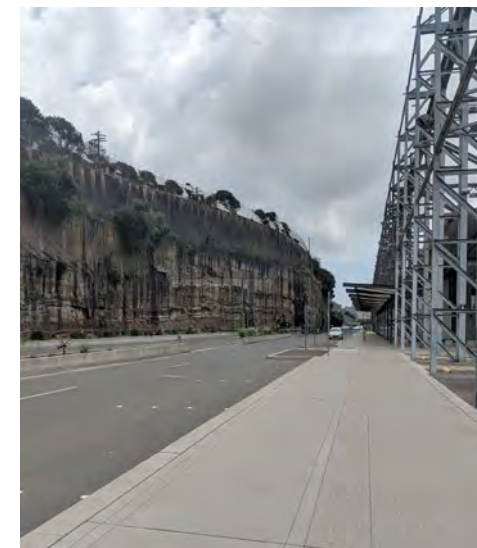


IMAGE 4.103 Escarpment at White Bay



IMAGE 4.104 White Bay Cruise Terminal / Record Doors





# 4.11 ROZELLE RAIL YARDS



# Existing Place Character

## 4.11 ROZELLE RAIL YARDS

Rozelle Rail Yards will see a WestConnex portal within the site and the length of the former rail yards have been cleared to accommodate this. It is envisioned that a park will be decked above the WestConnex infrastructure.

### Land Use & Function:

- Rozelle Rail Yards sub-precinct will be a large green open space for the public, delivered by WCX
- Existing light rail stabling yard located to the west of RRY sub-precinct

### Design of Places & Spaces - Built Form:

- Residential terrace houses and small density apartment buildings interface Rozelle Rail Yards along Lilyfield Road
- Light industrial and commercial uses also interface the sub-precinct

### Design of Places & Spaces - Public Domain:

- Adjacent Easton Park provides green open space, sports fields and a playground
- Steep topography and level changes along the sub-precinct boundary at Lilyfield Rd

### Transport & Movement:

- Major arterial roads constrain access from the southern boundary
- WCX to provide future pedestrian connections over City West Link into the precinct and to Rozelle Bay
- Key access connection under Victoria Rd to WBPS sub-precinct
- Lilyfield Light Rail stop is located to the west of RRY sub-precinct, and Rozelle Bay Light rail Stop to the south.
- Rozelle Bay Light Rail stop is envisaged to be connected to RRY sub-precinct via a pedestrian and cycle bridge.

### Heritage & Culture:

- Former rail tracks are to be reinstated and integrated into new public domain

### Infrastructure Delivery & Governance

- The proposed green open space in Rozelle Rail Yards sub-precinct will be delivered by WCX
- Social infrastructure, including sports fields and courts, will be delivered as part of the RRY sub-precinct green space

### LEGEND

- Rozelle Rail Yards Sub-Precinct Boundary
- Key Heritage Landmark
- Road
- Residential Housing
- Key Landform
- Green Space

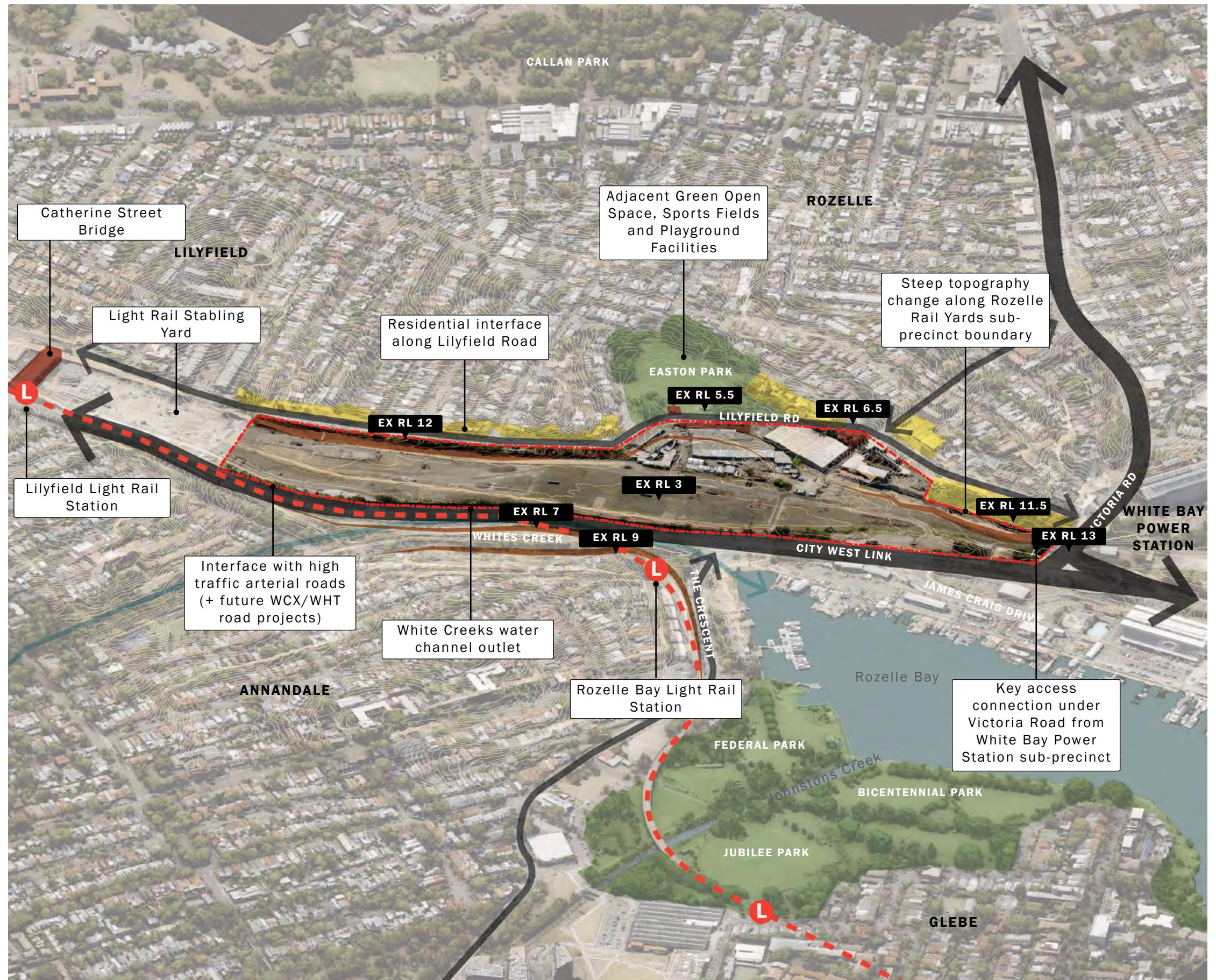


DIAGRAM 4.20 Rozelle Rail Yards Sub-Precinct Existing Conditions (image from July 2018)



# Future Place Character

## 4.11 ROZELLE RAIL YARDS

### ROZELLE RAIL YARDS

*A green oasis; an urban parkland offering a variety of restorative green and open spaces for new and existing communities to thrive...*

The Rozelle Rail Yards sub-precinct will be redeveloped as part of the WestConnex Rozelle Interchange project, and upon completion, nine hectares of active public parklands will be delivered. The parklands will offer a diverse collection of passive and active green spaces, targeting habitat rehabilitation and tree canopy cover, and improved water quality run-off into the adjacent Rozelle Bay. The site will cater to the varying needs of the local community, including the provision of large, open and level turfed areas for playing fields, while also accommodating some of the active recreation demands of new development within the Bays West Precinct.

Restorative spaces are a key focus of the parklands, both in rebalancing the site's natural systems and providing opportunities for the public to unwind and refuel. Consideration of all age groups and demographics have resulted in the integration of kids play zones, fitness and nature trails, playing fields/courts, as well as walking and cycling paths. The parklands seek to improve the health and wellbeing of local communities, by encouraging a variety of outdoor activities. Accessibility and safety are prioritised through the provision of good lighting, intuitive wayfinding, and passive surveillance. Buffering of the adjacent road infrastructure is achieved with dense vegetation zones, reducing the impact to park users.

A series of key connections are provided through the parklands, including a continuous level connection along Lilyfield Road, a link through to White Bay Power Station and the new metro station via the Victoria Road underpass, and a new land bridge connection over City West Link to the existing light rail stop. An additional direct link between the parklands and the foreshore at Rozelle Bay should be paired with any new development.

The parklands will pursue opportunities to engage community with on-Country stories and cultural practices. The site will preference endemic species and preserve the existing sandstone cuttings to its north edge. Dual naming and public artworks will allow the further involvement of local Aboriginal people (eg. Victoria Road underpass murals). Heritage interpretations and overlays within the park will also extend to the integration of preserved artefacts associated with its former use as a railway yard.



IMAGE 4.106 Abandoned Train Wheels, Rozelle Rail Yard



IMAGE 4.107 Ecodistrict, Paris / Construction21



IMAGE 4.108 Lohspark, Hamburg



IMAGE 4.109 Sydney Park / Turf Design Studio



IMAGE 4.110 Zeche Zollverein Park, Germany



IMAGE 4.111 NZS sports facility / Architektura Krusec



IMAGE 4.112 Rail tracks and gantries in Rozelle Rail Yards

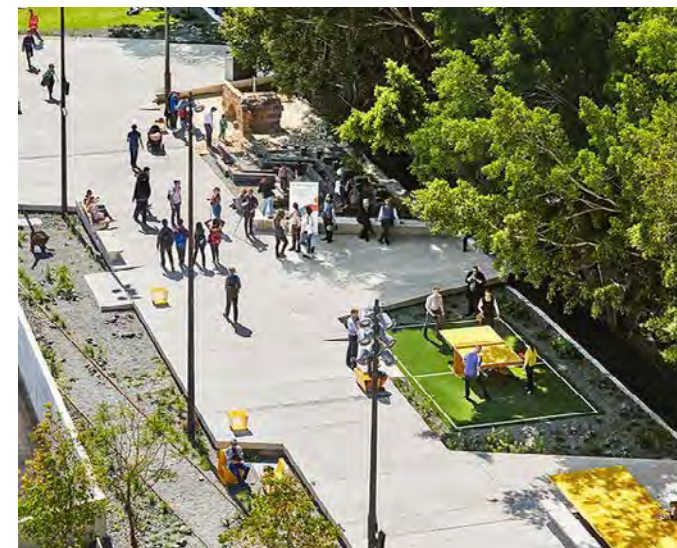


IMAGE 4.113 The Goods Line / Florian Groehn

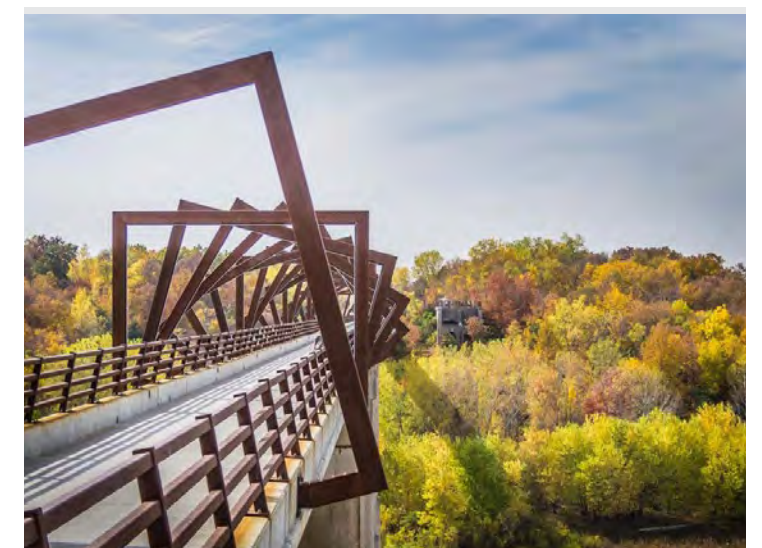


IMAGE 4.114 High Trestle Trail, US / DSM Cyclist



# 5

## IMPLEMENTATION



# 5.1

## IMPLEMENTATION ACTIONS & UDF RELATIONSHIP



# Bays West Implementation Actions

## 5.1 IMPLEMENTATION ACTIONS & UDF RELATIONSHIP

### INTRODUCTION

The preparation of the *Draft Bays West Place Strategy* is the first in a series of steps over several years to realise the vision for the precinct. The various elements of the precinct will be delivered by a range of agencies and proponents over the coming decades.

Sydney Metro and Transport for NSW (TfNSW) will progress the station planning, design and construction, the opening of which is the catalyst for the development of the precinct.

The Department of Planning, Industry and Environment will progress master planning for each sub precinct, beginning with the White Bay Power Station (and Metro Station) sub-precinct. Master planning will be supported by further precinct-wide investigations such as a transport plan, connecting with Country and a design excellence program.

Masterplans will support rezoning of parts of the precinct to guide and support the ultimate development form. Development control requirements are likely to appear, ultimately, in the local

environmental plan, supported by development control plans and appropriate infrastructure contribution plans.

The initial stage to undergo redevelopment will be the land around the new The Bays Metro Station. Sydney Metro and DPIE will undertake master planning of the White Bay Power Station and Metro sub-precinct, which will inform subsequent rezoning of the site. This master planning will include investigating open space and urban design, staging, traffic and transport, heritage, connecting with Country, sustainability, design excellence, and infrastructure and utilities.

This will then allow the subsequent state-significant development applications to be lodged by Sydney Metro.

### EVOLUTION OF THE FRAMEWORK

As part of this process, the *Urban Design Framework* will be updated. This will be in response to ongoing inputs from the more detailed work that is undertaken as a result of the Implementation Actions. These updates will be triggered by both the precinct-wide studies and strategies and the working harbour study as well as the master planning itself. During the master planning process there will be ongoing engagement with the precinct stakeholders which will also inform refinement of the document.

Over the following pages the table of Implementation Actions has been expanded to include the sections of the UDF that are anticipated will be updated as this information become available. There is the potential that some of the sections of Part 4 the Urban Design Framework may no longer be needed once the precinct-wide studies and strategies have been completed.



# Implementation Actions

## 5.1 IMPLEMENTATION ACTIONS & UDF RELATIONSHIP

Bays West Precinct Wide Actions		Timeframe	Responsibility	UDF Section to update as actions occur
1	Evolve the urban design framework as sub-precincts are master planned	Ongoing	DPIE (Department of Planning, Industry and Environment)	Part 2: Place Principles Part 3: Precinct-Wide Framework Part 4: Sub-Precinct Framework Part 5: Implementation
2	Undertake precinct-wide studies and strategies including (but not limited to): <ul style="list-style-type: none"> <li>Transport Plan</li> <li>Connecting with Country Framework</li> <li>Sustainability Strategy and Climate Adaption Plan</li> <li>Public Domain Manual</li> <li>Heritage and Interpretations Plan</li> <li>Implementation and Delivery Strategy</li> <li>Supporting infrastructure Plan</li> <li>Economic, Employment and Innovation Strategy</li> </ul>	2 years	DPIE with support of relevant agencies	Part 3: Precinct-Wide Framework  Note that Part 3 (and/or parts thereof) may be removed as the precinct-wide studies and strategies are completed
3	Establish a precinct wide approach to design excellence and design excellence strategies for specific projects to guide design quality for planning, development and delivery of the precinct	2 years (initial stage) and ongoing	DPIE (Government Architect NSW)	Part 3.7: Design Excellence
4	Undertake master-planning for each sub-precinct, beginning with the White Bay Power Station (and Metro) sub-precinct	1 year (initial stage) and then ongoing	DPIE, TfNSW (Sub-precinct 10)	Part 2: Place Principles Part 3: Precinct-Wide Framework Part 4: Sub-Precinct Framework
5	Progress the Sydney Harbour Strategy	2 years	NSW Government	Part 2: Place Principles Part 3.2: Land Use and Function Part 4: Sub-Precinct Framework
6	Undertake rezoning of land to facilitate development, beginning with the Metro Station site and land immediately surrounding	2 years (initial stage) and then ongoing	DPIE	Part 5.1: Implementation Actions
7	Develop an infrastructure contributions plan(s)	3 years	DPIE	Part 3.6: Infrastructure Delivery & Governance
8	Develop the detailed urban design framework and appropriate development controls to accompany each stage of re-zoning	Ongoing	DPIE	Part 2: Place Principles Part 3: Precinct-Wide Framework Part 4: Sub-Precinct Framework

TABLE 5.1



# Implementation Actions

## 5.1 IMPLEMENTATION ACTIONS & UDF RELATIONSHIP

Actions to Implement the Big Moves		Timeframe	Responsibility	UDF Section to update as actions occur
<b>2a</b>	The Bays West Transport Plan will guide the future renewal of Bays West to ensure that walking, cycling and public transport are the primary travel modes	2 years	DPIE with support of TfNSW	Part 2: Place Principles Part 3.4: Transport and Movement Part 4: Sub-Precinct Framework
<b>2b</b>	The Bays West Heritage and Interpretations Plan to consider how existing heritage fabric will be preserved and adaptively reused to support the future character of Bays West, including the need to respect the prominence and landmark qualities of the White Bay Power Station.	2 years	DPIE	Part 2: Place Principles Part 3.5: Heritage and Culture Part 4: Sub-Precinct Framework
<b>2c</b>	The Bays West Supporting Infrastructure Plan will be developed to ensure that appropriate critical infrastructure is delivered in a timely manner to support new and existing communities	2 years	DPIE	Part 3.6: Infrastructure Delivery and Governance  Note this may also have further impacts as locations are identified
<b>4a</b>	Master planning for each sub-precinct to consider and demonstrate how it supports the innovation corridor and connected centres along the Sydney Metro West Line	1 year (initial stage) and then ongoing	DPIE	Part 4: Sub-Precinct Framework
<b>4b</b>	Master planning for each sub-precinct to facilitate the delivery of an appropriate amount of residential dwellings, ensuring diversity of types and tenure, including affordable housing	1 year (initial stage) and then ongoing	DPIE	Part 4: Sub-Precinct Framework
<b>9</b>	Continue to work with Port Authority of NSW and other NSW Government Stakeholders to explore how the strategically important ports and maritime activities are integrated into the future Bays West, including exploring options for innovative combinations of a wide variety of land uses to strengthen the blue economy.	Ongoing	DPIE and Port Authority of NSW	Part 2: Place Principles Part 3.2: Land Use and Function Part 4: Sub-Precinct Framework
<b>10</b>	Develop Performance Criteria to measure the quality of public or open spaces as they are designed and planned	5 years	DPIE	Part 4: Sub-Precinct Framework

TABLE 5.2



# Implementation Actions

## 5.1 IMPLEMENTATION ACTIONS & UDF RELATIONSHIP

Actions to Implement the Big Moves		Timeframe	Responsibility	UDF Section to update as actions occur
<b>2d</b>	Prepare a local bus services network plan as part of the precinct- wide transport plan	2 years	DPIE, TfNSW	Part 2: Place Principles Part 3.4: Transport and Movement Part 4: Sub-Precinct Framework
<b>2e</b>	During the development of the precinct-wide transport plan, the City of Sydney and Inner West Council to consider how active travel links might connect into their local active travel network	2 years and ongoing	DPIE, City of Sydney, Inner West Council	Part 2: Place Principles Part 3.4: Transport and Movement Part 4: Sub-Precinct Framework
<b>4c</b>	Master planning for the White Bay Power Station (and Metro) sub-precinct to: <ul style="list-style-type: none"> <li>integrate the Power Station with the wider Bays West precinct</li> <li>consider the form and function of Robert Street appropriately, noting its diverse access needs</li> </ul>	5 years	DPIE	Part 4.2: White Bay Power Station
<b>4d</b>	Master planning for each sub-precinct to ensure an active travel connection through the precinct	1 year (initial stage) and then ongoing	DPIE	Part 4: Sub-Precinct Framework
<b>4e</b>	Master planning for each sub-precinct to consider the working harbour and port operational needs for the opening of the Metro station, and for the long term.	1 year (initial stage) and then ongoing	DPIE	Part 4: Sub-Precinct Framework
<b>4f</b>	Master-planning for each sub-precinct to consider and plan for the provision of connected public access to the waterfront	Ongoing	DPIE	Part 4: Sub-Precinct Framework
<b>12</b>	Place Management NSW to develop a plan to remediate and rehabilitate the White Bay Power Station structure	5 years	DPIE (Place Management)	Part 4.2: White Bay Power Station
<b>13</b>	Seek proposals for the White Bay Power Station including temporary activation opportunities	5 years	DPIE (Place Management)	Part 4.2: White Bay Power Station
<b>14</b>	TfNSW to undertake necessary maintenance and repair of the Glebe Island Bridge to ensure it remains safe and intact, and its heritage features are preserved.	Ongoing	TfNSW	Nil
<b>15</b>	Develop a user-needs assessment for the port and working harbour with community connectivity in mind	5 years	DPIE and Port Authority of NSW	Part 4.2: Land/Water Use and Function
<b>16</b>	Develop a user-needs assessment for public open space with port and harbour needs in mind.	5 years	DPIE and Port Authority of NSW	Part 4.4: Public Domain
<b>17</b>	Design in an east-west active travel path from the opening of The Bays Metro Station	Metro Opening	TfNSW	Part 3.4: Transport and Movement Part 4: Sub-Precinct Framework
<b>18</b>	Work with councils, landowners and their partners to secure any missing links along Sydney Harbour, including beyond the Bays West precinct	Ongoing	DPIE	Part 3.4: Transport and Movement
<b>19</b>	NSW Government to investigate options for connectivity between Bays West and Pyrmont	5 years	NSW Government	Part 4.4: Transport and Movement Part 3.4 & 3.5: Glebe Island Silos & East

TABLE 5.3  
BAYS WEST URBAN DESIGN FRAMEWORK  
MARCH 19, 2021



S

**SUPPORTING FRAMEWORKS**



# S.1

## CONNECTING WITH COUNTRY FRAMEWORK



S.3

# SUSTAINABILITY FRAMEWORK



# A

## APPENDIX



# A.1

## STRATEGIC DOCUMENTS



# Viewsheds: Introduction

## A.1 VIEWSHEDS

### LANDMARKS

Dispersed within the existing landform, there are a series of significant landmarks relating to the industrial, maritime and infrastructural role of the precinct in the development of Sydney. These landmarks have become well-known and are linked to the character of the site. These features have the potential to set this precinct apart on both a local and global scale.

The character of these landmarks varies from large scale elements such as White Bay Power Station, Glebe Island Silos and the Anzac Bridge, to smaller more localised elements such as Glebe Island Bridge and the working harbour areas. The large elements help to signify the precinct from afar and act as visual markers and gateways on the journey between the Inner West and the CBD.

These landmarks form part of the living narrative of the place allowing community attachment and positive association to the place. These are iconic elements that have the potential to unlock the future of the site, engage the community and define the future character of Bays West. This is a truly unique opportunity to build on the existing character through opening these landmarks to the public and integrating them into the land use and activity of Bays West.

The three key landmarks - the White Bay Power Station, Anzac Bridge and the Silos - have been identified and critical viewsheds have been investigated to preserve the history and character of the place.



IMAGE A.1 View from Blackwattle Bay Park



IMAGE A.2 White Bay Power Station



IMAGE A.3 White Bay Power Station from top of Glebe Island Silos / PANSW



IMAGE A.4 White Bay Power Station from Victoria Road



IMAGE A.5 White Bay Power Station



IMAGE A.6 Anzac Bridge from Blackwattle Bay



IMAGE A.7 Glebe Island from Jackson's Landing



IMAGE A.8 White Bay Power Station from Robert St



IMAGE A.9 Glebe Island Silos and Anzac Bridge from Buchanan St



# Viewsheds: White Bay Power Station

White Bay Power Station (WBPS) is a major industrial landmark and icon of the Bays West Precinct. Therefore, any development proposed in the vicinity of the White Bay Power Station must carefully consider its bulk, scale, and placement, in order to respect its visibility and prominence as a harbour-side landmark.

Key view corridors to the White Bay Power Station were first identified in the WBPS Conservation Management Plan. This document identified 13 views as the major axial and general views which also form the Visual Curtilage of the Power Station. This document specifically contains policy requirements in regards to the retention of these views and the siting of any development in the vicinity of the WBPS that needs to be considered in the master planning process of each sub-precinct.

These views have been more specifically mapped into a clarified set of six viewsheds. These have been determined as being of critical importance, in terms of the connection of White Bay Power Station with local neighbourhoods and the broader city, preserving views from key public locations. These range across all scales including district, local and precinct scales.

These viewsheds have been described in detail to indicate the view characteristics including the viewing position, the width of view and necessary curtilage to preserve, the distance/scale of view along with the particular details of the Power Station that are important to retain in the view as a minimum. Each viewshed sets a specific cap on built form height within its zone of influence, establishing mandatory constraints for future development proposals within Bays West. However, the scale of the Precinct itself means other significant internal views exist and should be considered and integrated where possible.

LEGEND

- Bays West Site Boundary
- █ Mandatory Minimum View Retention
- █ Additional Curtilage Associated with Viewshed
- █ Full Foreshore & Water View
- █ Partial Foreshore & Water View

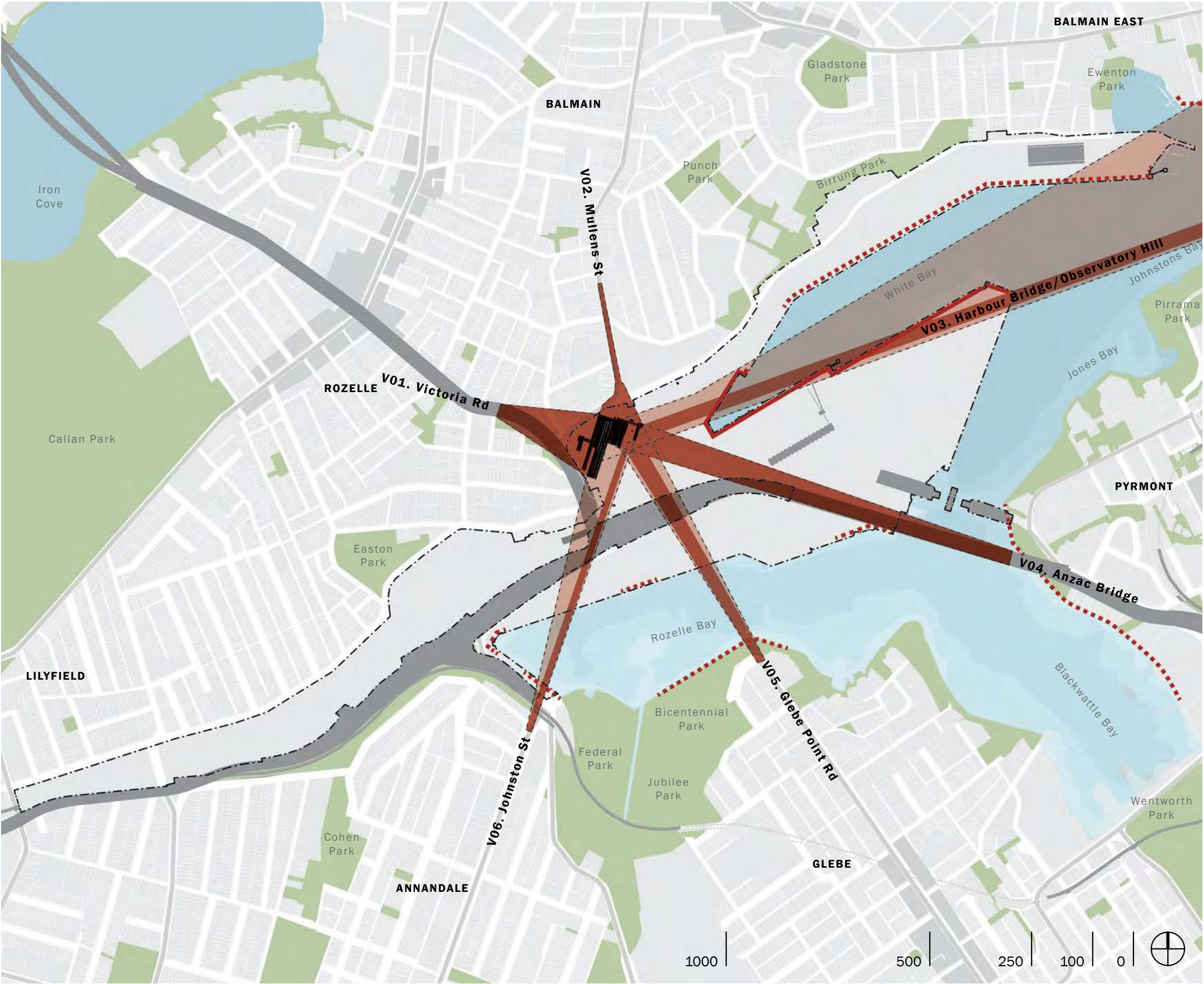


DIAGRAM A.1 Plan of the White Bay Power Station Viewsheds



# White Bay Power Station

## A.1 VIEWSHEDS

### V01 VICTORIA ROAD

- Characteristics of View:
- Gateway view on approach to the city
  - Layering and stepping of roof lines and parapets is clearly displayed
  - Chimneys visible as landmark
  - View corridor shifts in scale on approach from a district view to a local view
- Restrictions:
- No impact on legibility of layering and stepping of roof lines and parapets is permitted
  - Reading of all existing building elements highlighted must be maintained

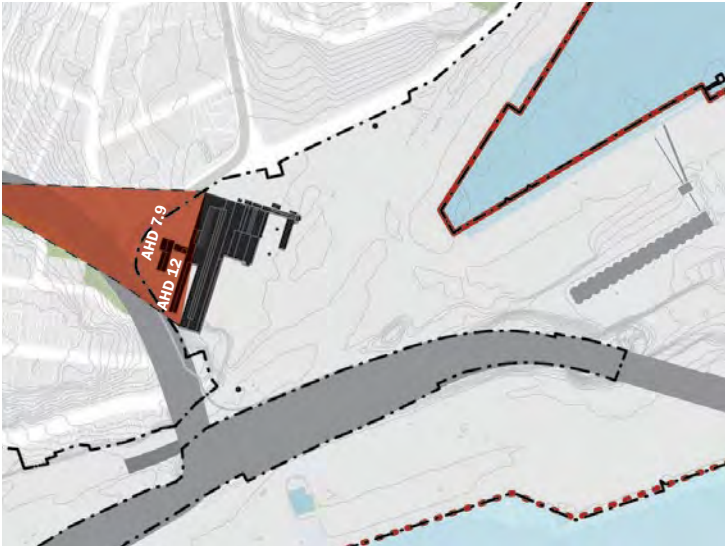


DIAGRAM A.2 Victoria Road Viewshed Plan

AHD 7.9

#### EXTENT OF RETENTION



IMAGE A.10 PT.A Chimneys & West Facade Viewshed  
33°51'59" S 151°10'29" E



IMAGE A.12 PT.B Chimneys & West Facade Viewshed  
33°52'00" S 151°10'32" E

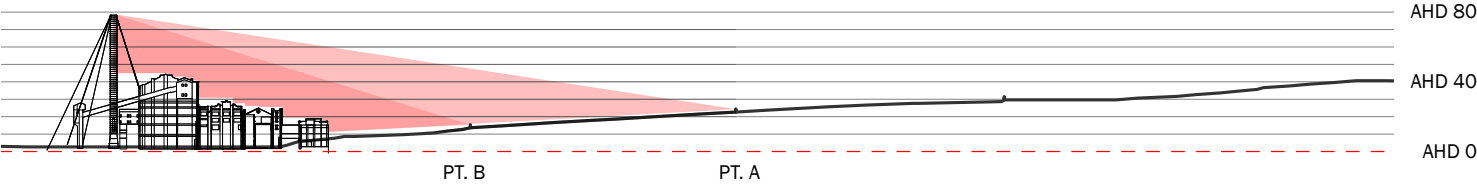


DIAGRAM A.3 Site Section for Victoria Road Viewshed

#### LEGEND

- Mandatory Minimum View Retention (all AHD levels refer to bottom plane of view corridor)
- Curtlidge Associated with Viewshed
- Glebe Island Silos associated elements
- Anzac Bridge associated elements

### V02 MULLENS STREET

- Characteristics of View:
- Approach along Mullens Street allows reading of northern elevation
  - Facade indicates former Power Station machinery processes within the building. The facade allows an interpretation of the linear power production process given the direct relationship between facade and the machinery accommodated beyond
  - Along Mullens St the view is framed by the building and landscape street elements opening up to a full vista at the intersection with Robert St
  - View corridor is local in scale
- Restrictions:
- Reading of all existing building elements highlighted must be maintained

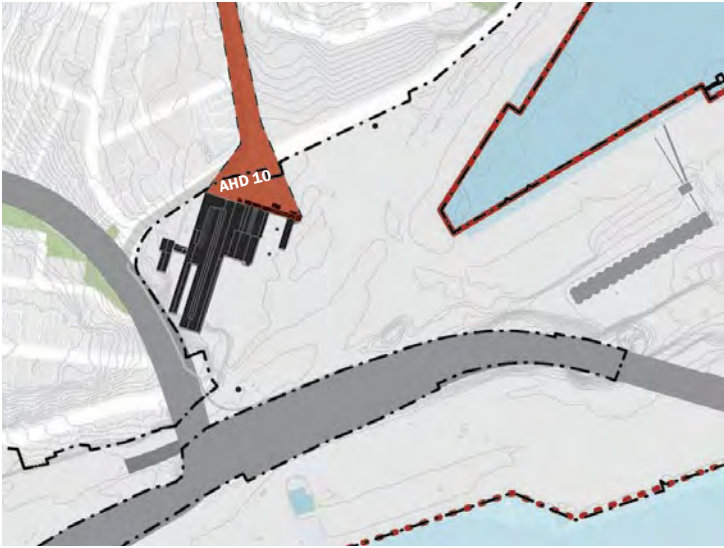


DIAGRAM A.4 Mullens Street Viewshed Plan

#### EXTENT OF RETENTION



IMAGE A.11 PT.A Chimneys & Machinery Viewshed  
33°51'53" S 151°10'36" E



IMAGE A.13 PT.B Machinery Sequence Viewshed  
33°51'59" S 151°10'36" E

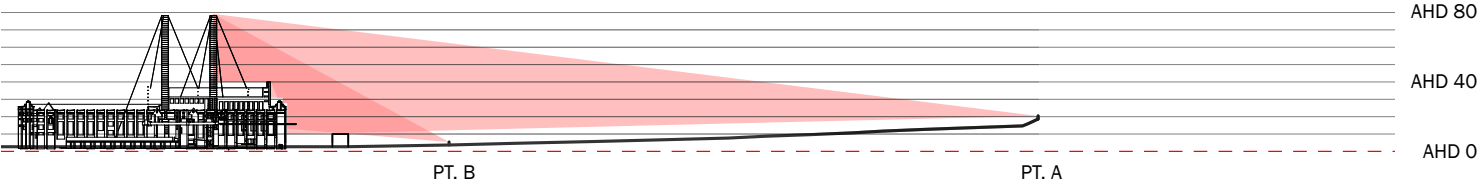


DIAGRAM A.5 Site Section for Mullens Street Viewshed



# White Bay Power Station

## A.1 VIEWSHEDS

### V03 HARBOUR VIEWS

Characteristics of View:

- Chimneys read as a silhouette on the skyline over ridgeline beyond
- Chimneys visible as landmark
- View corridor is city-wide in scale
- View is also significant within precinct as visible from water, foreshore and major public domain space.

Restrictions:

- Reading of 2 no. chimneys on skyline uncrowded by new buildings, maintained from key public viewpoints (specifically Observatory Hill and Sydney Harbour Bridge)
- Within the precinct the angled view to northern and eastern facades of WBPS should be a major feature for public domain and foreshore spaces

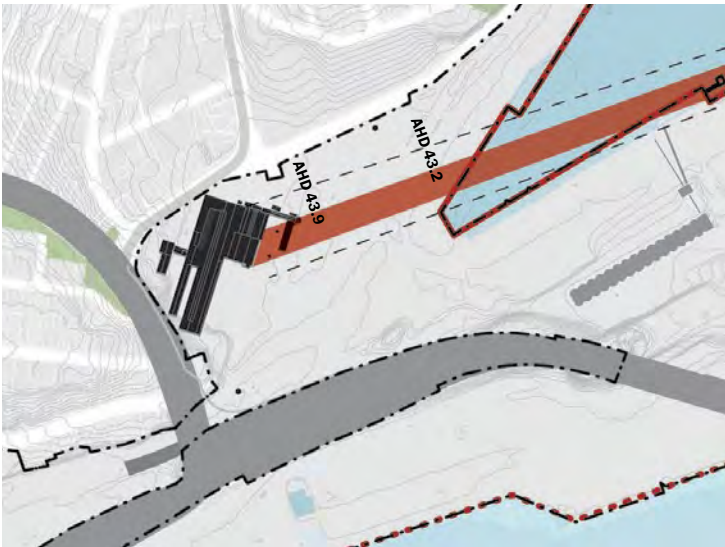


DIAGRAM A.6 Harbour Viewshed Plan

### V04 ANZAC BRIDGE

Characteristics of View:

- Gateway view on exit from the city to the west
- Chimneys visible as landmark with Boiler House facade form and detail visible above the Coal Handling Shed ridgeline
- View corridor shifts in scale on approach from a district view to a local view

Restrictions:

- Reading of 2 no. chimneys on skyline must be maintained, uncrowded by new buildings
- Reading of all existing building elements highlighted must be maintained

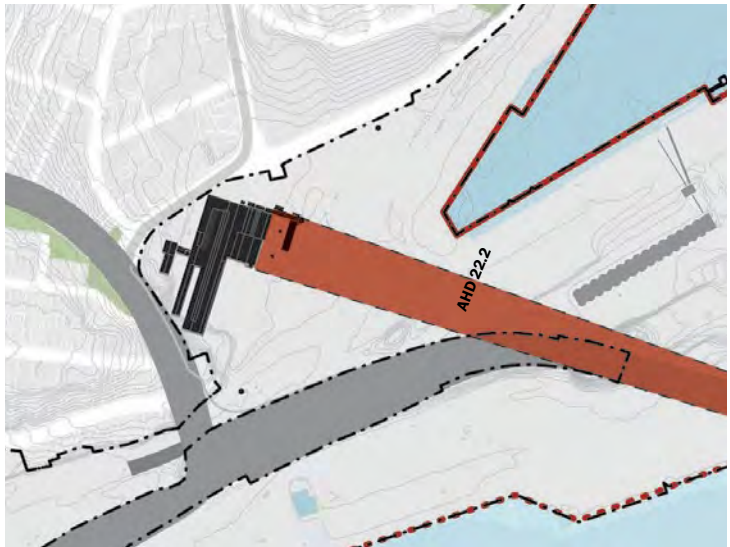


DIAGRAM A.9 Anzac Bridge Viewshed Plan

#### EXTENT OF RETENTION



IMAGE A.14 PT.A Harbour Bridge - Chimney Profiles & Breathing Room Viewshed

33°51'17" S 151°12'32" E



IMAGE A.15 PT.B Observatory Hill - Chimney Profiles & Breathing Room Viewshed

33°51'32" S 151°12'15" E

#### EXTENT OF RETENTION



IMAGE A.16 PT.A Boiler House Facade, Chimneys & Visual Curtilage Viewshed

33°52'05" S 151°10'57" E



IMAGE A.17 PT.B Boiler House Facade, Chimneys & Visual Curtilage Viewshed

33°52'03" S 151°10'49" E

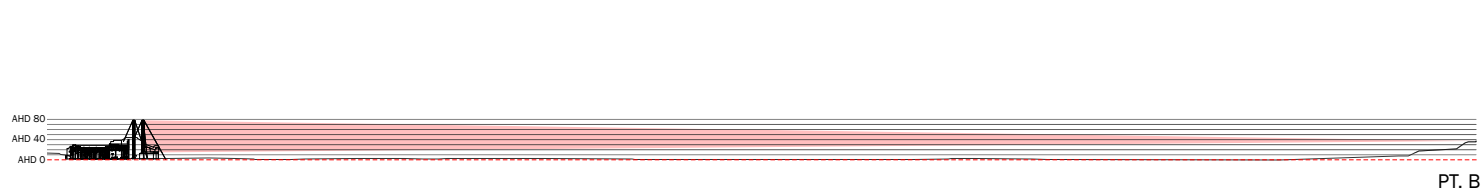


DIAGRAM A.7 Site Section for Observatory Hill Viewshed

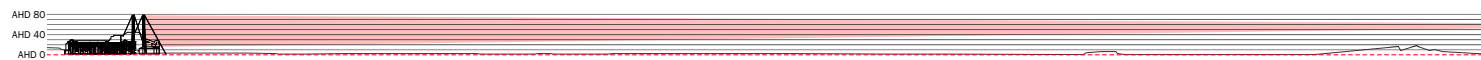


DIAGRAM A.8 Site Section for Harbour Bridge Viewshed

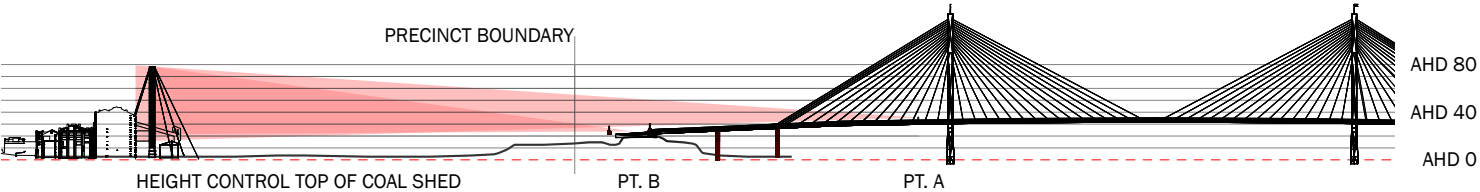


DIAGRAM A.10 Site Section for Anzac Bridge Viewshed

#### LEGEND

- Mandatory Minimum View Retention (all AHD levels refer to bottom plane of view corridor)
- Curtilage Associated with Viewshed
- Glebe Island Silos associated elements
- Anzac Bridge associated elements



# White Bay Power Station

## A.1 VIEWSHEDS

### V05 GLEBE POINT ROAD

Characteristics of View:

- Chimneys read as a silhouette on the skyline along key roadway
- Chimneys visible as landmark
- View corridor is district in scale

Restrictions:

- Reading of 2 no. chimneys on skyline must be maintained, uncrowded by new buildings
- Reading of all existing building elements highlighted must be maintained



DIAGRAM A.11 Glebe Point Road Viewshed Plan

### V06 JOHNSON STREET

Characteristics of View:

- Chimneys read as a silhouette on the skyline along key roadway
- Chimneys visible as landmark
- View corridor is district in scale

Restrictions:

- Reading of 2 no. chimneys on skyline must be maintained, uncrowded by new buildings
- Reading of all existing building elements highlighted must be maintained

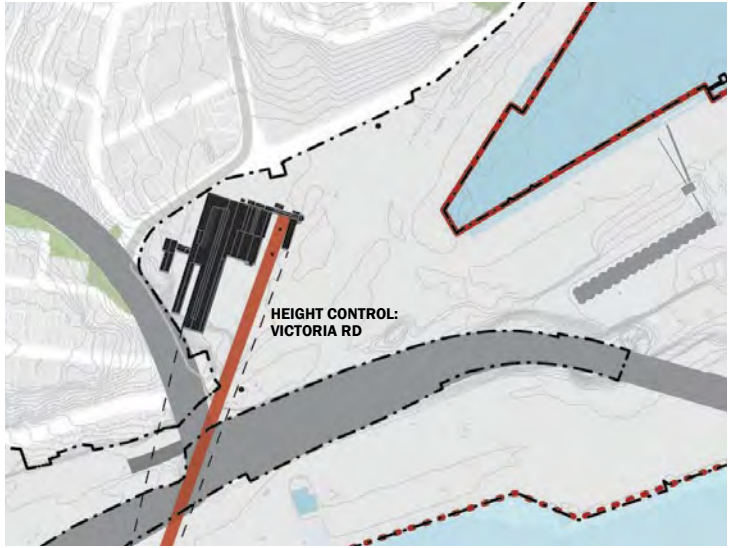


DIAGRAM A.13 Johnson Street Viewshed Plan

#### EXTENT OF RETENTION



IMAGE A.18 PT.A Chimneys & Breathing Room Viewshed  
33°52'25" S 151°10'55" E



IMAGE A.19 PT.B Chimneys & Breathing Room Viewshed  
33°52'17" S 151°10'49" E

#### EXTENT OF RETENTION



IMAGE A.20 PT.A Chimneys & Breathing Room Viewshed  
33°52'27" S 151°10'26" E



IMAGE A.21 PT.B Machinery Sequence Viewshed  
33°52'23" S 151°10'28" E

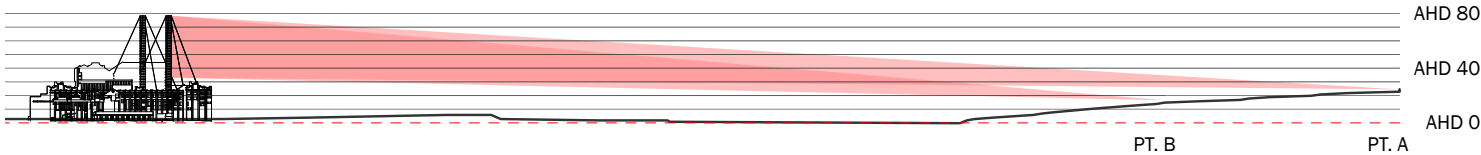


DIAGRAM A.12 Site Section for Glebe Point Road Viewshed

#### LEGEND

- Mandatory Minimum View Retention (all AHD levels refer to bottom plane of view corridor)
- Curtlidge Associated with Viewshed
- Glebe Island Silos associated elements
- Anzac Bridge associated elements

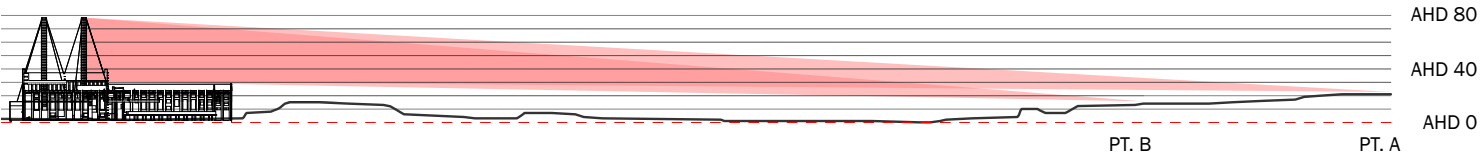


DIAGRAM A.14 Site Section for Johnson Street Viewshed



# Glebe Island Silos

## A.1 VIEWSHEDS

A detailed assessment of the Precinct’s surrounding public sites has identified areas that currently provide a view of the Glebe Island Silos. This work has expanded on the Glebe Island Silos Heritage Assessment undertaken by Urbis and concurs that the most significant visual curtilage zone for the Silos predominantly being of the local scale and significantly from the parks and foreshore areas to the north of the Anzac Bridge alignment where the longer facades of the Silos can be seen. Additional view outside this zone have been identified include:

- V09-Blackwattle Bay Park allows a view of both the Silos and the Anzac Bridge
- V10-City West Link allows the Silos to still be seen on approach to the precinct form various points along the approach to Anzac Bridge
- V11-Victoria Rd ‘Moushole’ allows a view of the south-western end of the Silos to be seen upon entering the precinct and along the new precinct road

Impact across all viewsheds should be considered in the future development of sub-precinct masterplans. However, unlike the White Bay Power Station, the approach to these viewsheds is more flexible and guidelines have been outlined. A balanced approach is considered suitable, with retention of partial or framed views through new built form accepted from some aspects. These more flexible parameters have been outlined in further detail for each of the views identified.

There are also a number of opportunities to consider precinct views to the Silos, particularly from key public domain zones. These include many of the major green spaces and the proposed Foreshore Promenade.

### LEGEND

- Bays West Site Boundary
- Significant Views for Retention
- ▨ Other Views Desirable for Retention
- ▨ Curtlidge Associated with Viewshed
- Full Foreshore & Water View
- - - Partial Foreshore & Water View

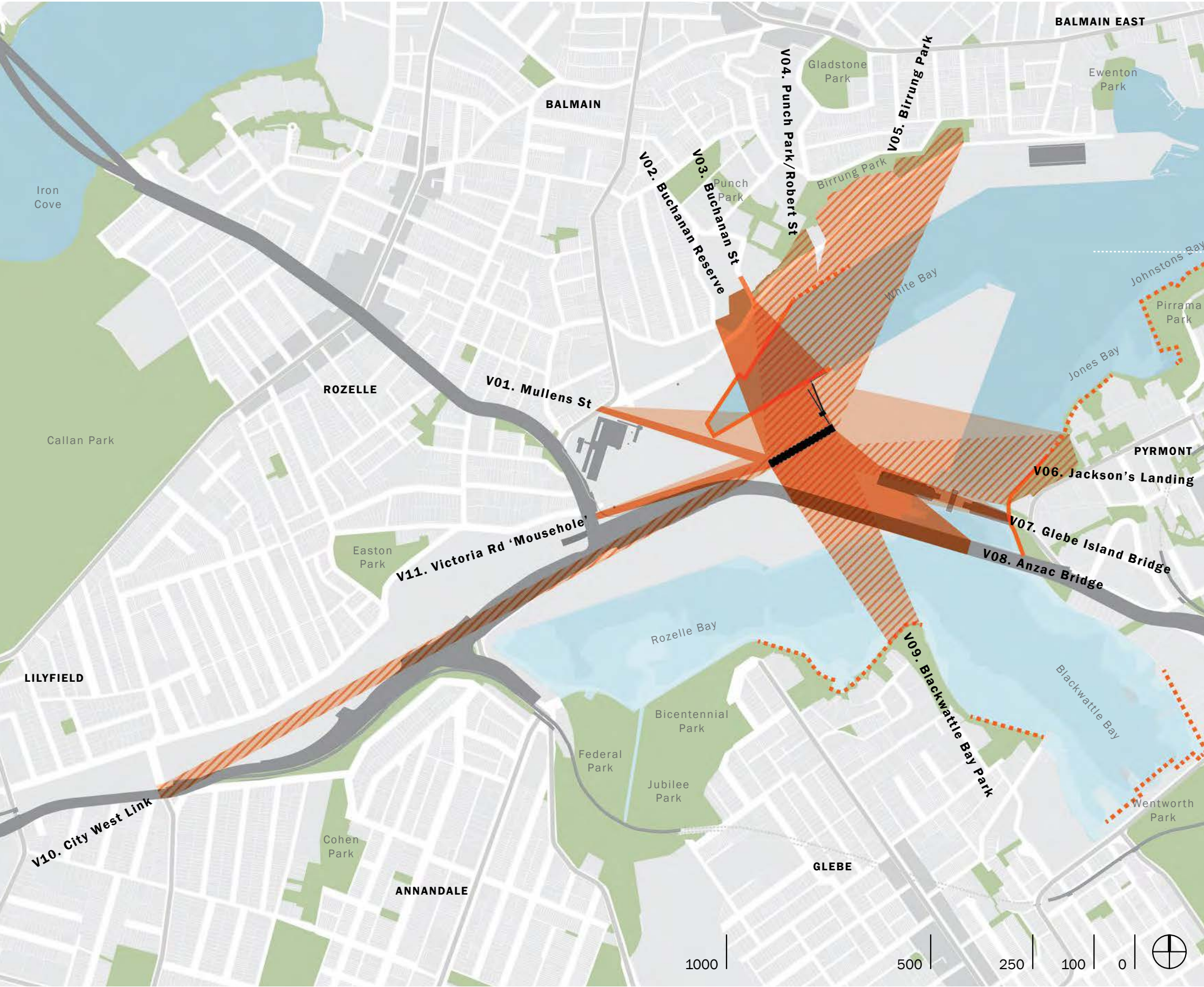


DIAGRAM A.15 Plan of the Glebe Island Silos Viewsheds

Scale 1:10,000 @ A3  
258



# Glebe Island Silos

## A.1 VIEWSHEDS

### V01 MULLENS ST

Characteristics of View:

- Arrival point to sub-precinct at the intersection of Robert's Street gives a significant interpretation of the living character, layers, and stories of place associated with the industrial, maritime and transport infrastructure of Sydney given the view encompasses the Glebe Island Silos together with the Anzac Bridge and the White Bay Power Station.
- View corridor is local in scale

Guidelines:

- Partial view retention deemed acceptable
- Ensure, at a minimum, visibility of the top half of the south-western end of the Glebe Island Silos is framed in relation to the adjacent landmark items

EXISTING VIEW EXTENT



IMAGE A.23 Mullens St Viewshed  
33°51'57.7"S 151°10'36.5"E

LEGEND

- Glebe Island Silos Guideline Minimum View Retention
- Glebe Island Silos Indicative Form
- Anzac Bridge Associated Elements
- White Bay Power Associated Elements

### V02 BUCHANAN RESERVE

Characteristics of View:

- Elevated vantage of Buchanan Reserve gives a rare but clear and powerful reading of the Glebe Island Silos north elevation and it's conveyor infrastructure in-front
- View is district in scale, relating the landmark to the waters edge from which it operates

Guidelines:

- Ensure continuous view of the top ~20% of the silo array, the north-eastern end, and it's conveyor infrastructure is maintained
- Minimum of two flexible partial view-slots (aligned to apex of the cylindrical form) must be allowed for in the built-form layout in-front to maintain full silo height and geometry legibility
- Note: only single view-slot need be legible from any given location along Robert Street

EXISTING VIEW EXTENT

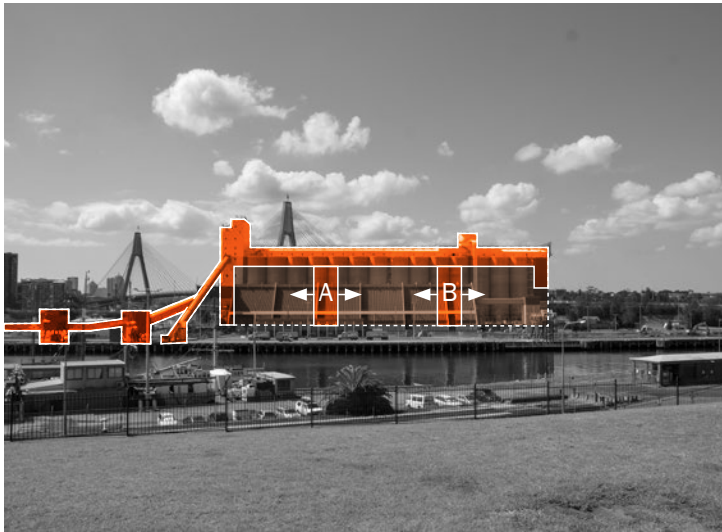


IMAGE A.24 Buchanan Reserve Viewshed  
33°51'51.1"S 151°10'48.0"E

### V03 BUCHANAN ST

Characteristics of View:

- Approach from Buchanan Street gives a clear and powerful reading of the Glebe Island Silos north elevation and it's conveyor infrastructure in-front
- View corridor shifts from district to local in scale on approach

Guidelines:

- Ensure continuous view of the top ~20% of the silo array, the north-eastern end, and it's conveyor infrastructure is maintained
- Minimum of two flexible partial-view slots (aligned to apex of the cylindrical form) must be allowed for in the built-form layout in-front to maintain full silo height and geometry legibility
- Note: only single view-slot need be legible from any given location along Robert Street

EXISTING VIEW EXTENT



IMAGE A.25 Buchanan Street Viewshed  
33°51'49.4"S 151°10'50.0"E

### V04 PUNCH PARK / ROBERT ST

Characteristics of View:

- Elevated vantage from the public domain of Punch Park gives the strongest reading of the Glebe Island Silos' significance as a seminal site in the historical development of one of Australia's most important primary industries as it allows an interpretation of the linear bulk export and storage processes given the direct relationship between silo array and it's conveyor infrastructure
- View is district in scale, relating the landmark to the waters edge from which it operates

Guidelines:

- Continuous view of the top ~20% of the silo array, the north-eastern end, and it's conveyor infrastructure must remain visible

EXISTING VIEW EXTENT



IMAGE A.26 Punch Park / Robert St Viewshed  
33°51'45.8"S 151°10'54.7"E



# Glebe Island Silos

## A.1 VIEWSHEDS

### V05 BIRRUNG PARK

Characteristics of View:

- Elevated vantage from the public domain of Punch Park allows the reading of the Glebe Island Silos' linear bulk export and storage processes given the direct relationship between silo array and it's conveyor infrastructure
- View is district in scale and a key vantage over the Bays West precinct

Guidelines:

- Continuous view of the top ~20% of the silo array, the top north-western 25% of the north-eastern end, and it's conveyor infrastructure must remain visible
- Note: View from Peacock Point excluded as north-eastern end not distinct

#### EXISTING VIEW EXTENT



IMAGE A.27 Birrung Park Viewshed  
33°51'38.1"S 151°11'07.5"E

#### LEGEND

- Glebe Island Silos Guideline Minimum View Retention
- Glebe Island Silos Indicative Form
- Anzac Bridge Associated Elements
- White Bay Power Station Associated Elements

### V06 JACKSON'S LANDING

Characteristics of View:

- The stepped-level view from the public domain and foreshore promenade at Jackson's Landing allows an interpretation of the living character, layers, and stories of place associated with the historical and future maritime and transport infrastructure of Sydney given the view encompasses the Glebe Island Silos together with the Anzac Bridge and future working port
- View corridor is district in scale

Guidelines:

- Flexible partial view retention deemed acceptable
- Ensure at least ~30% of the south-western elevation is visible from all public domain and foreshore zones in Jackson's landing
- Note: Viewable area may be offset if accommodated from other view positions

#### EXISTING VIEW EXTENT



IMAGE A.28 Jackson's Landing Viewshed  
33°52'03.9"S 151°11'19.3"E

### V07 GLEBE ISLAND BRIDGE

Characteristics of View:

- Powerful gateway view on exit from Pyrmont over the Glebe Island Bridge where approach is on axis with the Glebe Island Silos' southern elevation which is read as a significant landmark indicating the stories of place associated with the historical and future maritime and transport infrastructure of Sydney given the view also encompasses the Anzac Bridge
- View corridor shifts in scale on approach from district view to local view

Guidelines:

- Partial view retention deemed acceptable
- Ensure full silo-height legibility of ~50% of the middle portion of this elevation is framed from the Glebe Island Bridge, accounting for low-scale development of the future working port

#### EXISTING VIEW EXTENT



IMAGE A.29 Glebe Island Bridge Viewshed  
33°52'07.5"S 151°11'15.7"E

### V08 ANZAC BRIDGE

Characteristics of View:

- The most visible and well known vantage framing the Glebe Island Silos as a gateway view on exit from the city travelling west along the key roadway of the Anzac Bridge which allows an interpretation of the living character, layers, and stories of place associated with the industrial, maritime and transport infrastructure of Sydney given the view encompasses this landmark item with the Anzac Bridge and the White Bay Power Station
- View corridor shifts in scale on approach from district view to local view

Guidelines:

- Flexible view retention deemed acceptable
- Ensure built form does not block more than ~20% of the southern elevation and the western end of the Glebe Island Silos remains legible

#### EXISTING VIEW EXTENT

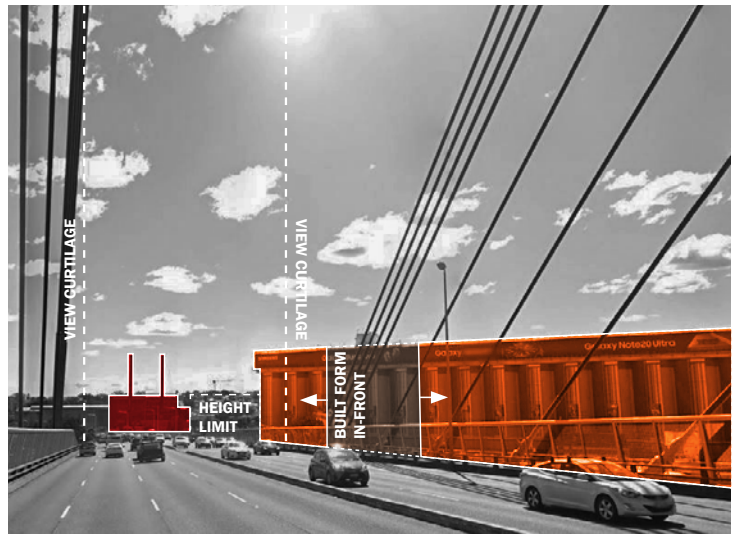


IMAGE A.30 Anzac Bridge  
33°52'08.6"S 151°11'08.7"E



# Glebe Island Silos

## V09 BLACKWATTLE BAY PARK

Characteristics of View:

- The panoramic vista from the point of Blackwattle Bay Park allows an interpretation of the living character, layers, and stories of place associated with the maritime and transport infrastructure of Sydney given the view encompasses the southern elevation of the Glebe Island Silos together with the Anzac Bridge
- View corridor is district in scale

Guidelines:

- Flexible partial view retention deemed acceptable,
- Ensure, at a minimum, visibility of the Glebe Island Silos upper portion (above the road deck of the Anzac Bridge) is maintained from this position and built form in front does not account for more than ~20% of its elevation

### EXISTING VIEW EXTENT

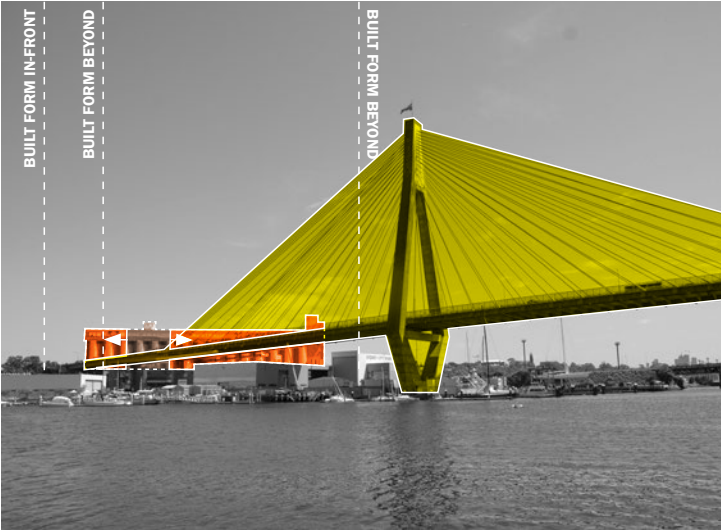


IMAGE A.31 Blackwattle Bay Park Viewshed  
33°52'15.3"S 151°11'05.0"E

### LEGEND

- Orange rectangle: Glebe Island Silos Guideline Minimum View Retention
- Yellow rectangle: Glebe Island Silos Indicative Form
- Yellow line: Anzac Bridge Associated Elements
- Red line: White Bay Power Station Associated Elements

## V10 CITY WEST LINK

Characteristics of View:

- The Glebe Island Silos are visible as a silhouette on the skyline and help to signify the precinct from afar together with the White Bay Power Station chimneys, the Anzac Bridge as well as the Sydney Harbour Bridge and CBD, and readable as a visual marker in relation to the Rozelle Railyards
- View corridor is city-wide in scale

Guidelines:

- Partial view deemed acceptable
- Ensure, at a minimum, the face of the western end of the Glebe Island Silos remains visible from this position

### EXISTING VIEW EXTENT



IMAGE A.32 City West Link Viewshed  
33°52'26.9"S 151°09'54.7"E

## V11 VICTORIA ROAD 'MOUSEHOLE'

Characteristics of View:

- Key gateway view on approach from the Rozelle Railyards where the Glebe Island Silos are readable as a significant landmark of the area and allows an interpretation of the living character, layers, and stories of place associated with the industrial and maritime infrastructure of Sydney
- View corridor shifts on approach from district to local in scale

Guidelines:

- Partial view retention deemed acceptable
- Ensure, at a minimum the face of the western end and silo geometry remains legible from this position

### EXISTING VIEW EXTENT



IMAGE A.33 Victoria Rd 'Mousehole' Viewshed  
33°52'06.4"S 151°10'35.6"E



# Anzac Bridge

## A.1 VIEWSHEDS







A review of the public sites which currently have views of the Anzac Bridge was undertaken and a series of viewsheds were identified to consider the visual curtlidge of this element within the context of Bays West development. These have been determined as being of significant importance, in terms of the connection of Anzac Bridge with local neighbourhoods and the broader city, preserving views from key public locations. These range across all scales including district, local and precinct scales.

The critical views identified are generally those which view the bridge in profile with the pylon and cable elements visible. District views from areas of Sydney which see the profile of the bridge running east west are to the south and north of the site and many of these views will be preserved by the position of the bridge relative to existing water bodies. There are also some axial views of the Pylons which are significant.

There are also a number of opportunities to consider precinct views to the Silos, particularly from key public domain zones. These include many of the major green spaces and the proposed Foreshore Promenade, where views to this landmark should be considered in the placement and design of future development. The Rozelle Bay and eastern portion of Glebe Island need particular attention.

Impact across all viewsheds should be considered in the future development of sub-precinct masterplans. However, unlike the White Bay Power Station, the approach to these viewsheds is more flexible given there has not yet been a formal heritage assessment which has included a study. Guidelines have been outlined and it recommended that a more definitive position is established to protect the views of this landmark.

### LEGEND

-  Bays West Site Boundary
-  Significant Views for Retention
-  Other Views Desirable for Retention
-  Curtlidge Associated with Viewshed
-  Full Foreshore & Water View
-  Partial Foreshore & Water View

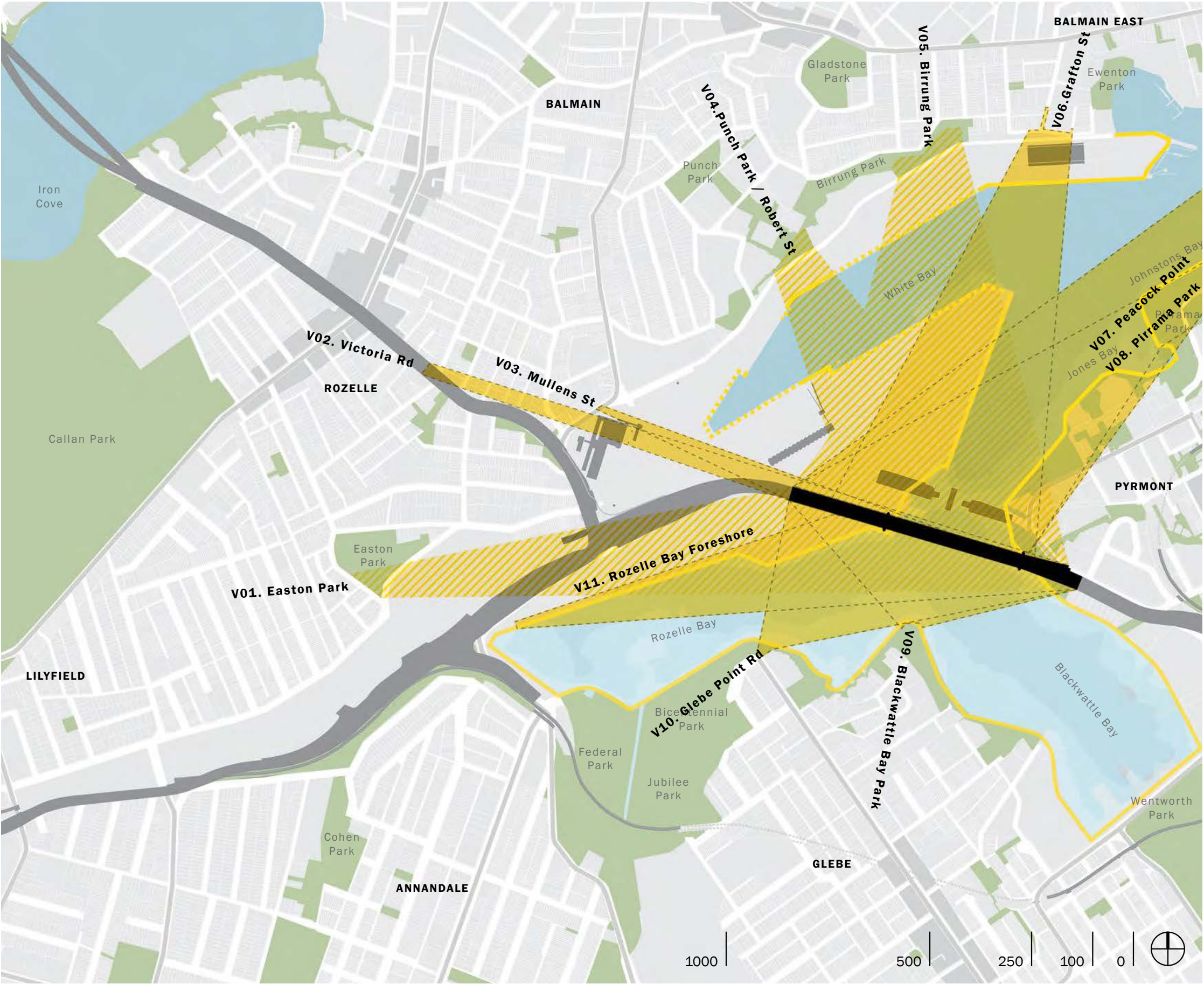


DIAGRAM A.16 Plan of the Anzac Bridge Viewsheds



# Anzac Bridge

## A.1 VIEWSHEDS

### V01 EASTON PARK

Characteristics of View:

- Elevated vantage of Easton Park allows the reading of both of the Anzac Bridge pylons and stay cables, Sydney Tower. The Anzac Bridge is read as a silhouette on the skyline of the CBD. Framed landscaping, furniture, and gentle topography of the park frames a key and contemplative view of the Anzac Bridge as a significant landmark in the area
- View corridor is district in scale

Guidelines:

- Partial visibility deemed acceptable
- Ensure visibility of both pylons and Sydney Tower from this location, uncrowded by new built form

#### EXISTING VIEW EXTENT



IMAGE A.34 Easton Park Viewshed  
33°52'10.6"S 151°10'11.9"E

#### LEGEND

- Anzac Bridge Guideline Minimum View Retention
- Anzac Bridge Indicative Form
- Glebe Island Silos Associated Elements
- White Bay Power Station Associated Elements

### V02 VICTORIA ROAD

Characteristics of View:

- The Anzac Bridge is read as a landmark on approach to the city from the west along the key roadway
- Western pylon and stay cables of the Anzac Bridge visible above the roofline of the existing White Bay Power Station
- View corridor shifts from city-wide to district in scale

Guidelines:

- Retain extent of existing visibility from this location, uncrowded by new built form
- Curtilage zone defined by southern chimney (WBPS) and end of Transfer House (see below)

#### EXISTING VIEW EXTENT



IMAGE A.35 Victoria Rd Viewshed  
33°51'57.4"S 151°10'24.0"E

### V03 MULLENS ST

Characteristics of View:

- Arrival point to sub-precinct at the intersection of Robert's Street
- View corridor encompasses the Glebe Island Silos together with the Anzac Bridge and White Bay Power Station machinery
- View corridor is local in scale

Guidelines:

- Ensure view is maintained

#### EXISTING VIEW EXTENT



IMAGE A.36 Mullens St Viewshed  
33°51'57.7"S 151°10'36.5"E

### V04 PUNCH PARK / ROBERT STREET

Characteristics of View:

- Partial visibility of both pylons and stay cables of the Anzac Bridge above Silos and through conveyor system
- Tops of both pylons exist as high points in skyline from this viewpoint, act as wayfinding landmarks

Guidelines / Restrictions:

- Retain visibility of top of western pylon and stay cables as framed by the Silos
- Avoid overcrowding with new built form so as to remain distinct as a wayfinding landmark
- Loss of western pylon visibility considered acceptable from this location in favour of retaining greater visibility of Anzac Bridge from other key viewpoints identified

#### EXISTING VIEW EXTENT



IMAGE A.37 Punch Park / Robert St Viewshed  
33°51'45.8"S 151°10'54.7"E



# Anzac Bridge

## A.1 VIEWSHEDS

### V05 BIRRUNG PARK

Characteristics of View:

- The elevated location of Birrung Park allows an undisturbed view of the east-west profile of the Anzac Bridge, looking across White Bay and over the Precinct
- Both pylons act as key wayfinding landmarks

Gudelines:

- Future development on Glebe Island will result in a level of reduce visibility of the Anzac Bridge from this location
- Priority is given to retaining clear and uncrowded views to the upper half of both pylons
- At a minimum, a clear reading of the eastern pylon and stay cables (top half) is required to be achieved with new built form

EXISTING VIEW EXTENT



IMAGE A.38 Birrung Park Viewshed  
33°51'38.1"S 151°11'07.5"E

LEGEND

- Anzac Bridge Guideline Minimum View Retention
- Anzac Bridge Indicative Form
- Glebe Island Silos Associated Elements
- White Bay Power Station Associated Elements

### V06 GRAFTON STREET

Characteristics of View:

- East-west profile of the Anzac Bridge visible when looking across the water over the roof of the White Bay Cruise Terminal
- Partial obstruction of the cable stays and lower half of eastern pylon

Gudelines:

- Retain visibility of western pylon above height of bridge road deck, and full extent of road deck and stay cables between the two pylons
- Prominence of bridge profile on skyline to be retained, and not obstructed or crowded with new built form
- New built form within the curtilage zone to provide a clear view of the existing road deck (see below)

EXISTING VIEW EXTENT

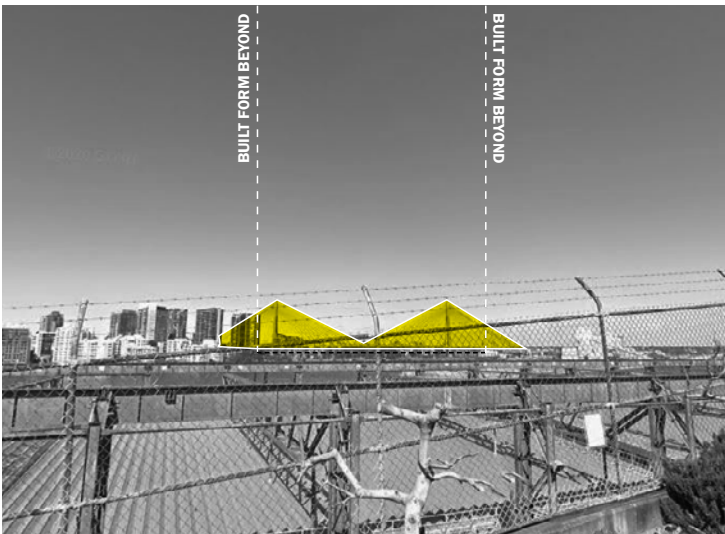


IMAGE A.39 Grafton Street Viewshed  
33°51'36.7"S 151°11'20.6"E

### V07 PEACOCK POINT

Characteristics of View:

- Undisturbed view of the western pylon and stay cables of the Anzac Bridge from across the water
- Eastern pylon and stay cables already obscured by existing buildings on Pyrmont side

Gudelines:

- Retain extent of existing visibility from this location, uncrowded by new built form
- Curtilage zone to extend to western end of bridge, beyond where stay cables meet the road deck
- New built form within the curtilage zone to allow a clear view of the existing road deck, with buildings behind also limited in height so the skyline profile of bridge is not adversely impacted

EXISTING VIEW EXTENT



IMAGE A.40 Peacock Point Viewshed  
33°51'35.5"S 151°11'45.2"E

### V08 PIRRAMA PARK

Characteristics of View:

- Undisturbed view of the western pylon and stay cables of the Anzac Bridge from across the water
- Eastern pylon and stay cables already partially obscured by existing buildings on Pyrmont side

Gudelines:

- Retain extent of existing visibility from this location, uncrowded by new built form
- Curtilage zone to extend to western end of bridge, beyond where stay cables meet the road deck
- New built form within the curtilage zone to allow a clear view of the existing road deck, with buildings behind also limited in height so the skyline profile of bridge is not adversely impacted

EXISTING VIEW EXTENT



IMAGE A.41 Pirrama Park  
33°51'50.2"S 151°11'27.3"E



# Anzac Bridge

## A.1 VIEWSHEDS

### V09 BLACKWATTLE BAY PARK

Characteristics of View:

- Undisturbed view of the western pylon, stay cables, and road deck of the Anzac Bridge from across the water, stretching back past the Silos
- Full height of pylon and east-west profile of the bridge clearly visible and landmark on skyline

Gudelines:

- Retain extent of existing visibility from this location, with all new built form of medium or high scale sitting beyond the bridge
- Height of new built form on Glebe Island to sit clustered behind the bridge's western pylon and taper down towards the water, maximising the sky views through the stay cables and under the central section of road deck (between pylons)

#### EXISTING VIEW EXTENT

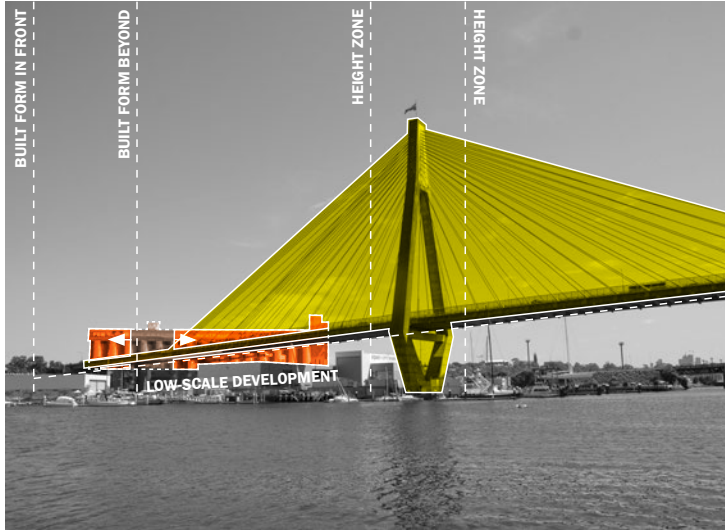


IMAGE A.42 Blackwattle Bay Park Viewshed  
33°52'15.5"S 151°11'04.9"E

#### LEGEND

- Anzac Bridge Guideline Minimum View Retention
- Anzac Bridge Indicative Form
- Glebe Island Silos Associated Elements
- White Bay Power Station Associated Elements

### V10 GLEBE POINT RD

Characteristics of View:

- Complete view of pylons, stay cables, and road deck of the Anzac Bridge along the Rozelle Bay foreshore
- Full extent of both pylons visible
- Bridge as landmark with CBD skyline as backdrop

Gudelines:

- Retain general extent of existing visibility from this location, uncrowded by new built form
- Curtilage zone to extend west of pylon to maintain its full and clear view along foreshore
- New built form may obscure stay cables outside curtilage zone and hide bridge on/off-ramp feeding to City West link

#### EXISTING VIEW EXTENT



IMAGE A.43 Glebe Point Rd Viewshed  
33°52'17.5"S 151°10'49.6"E

### V11 ROZELLE BAY FORESHORE

Characteristics of View:

- Complete view of pylons, stay cables, and road deck of the Anzac Bridge along the Rozelle Bay foreshore (from head of the Bay)
- Full extent of both pylons visible
- Bridge as landmark with CBD skyline as backdrop

Gudelines:

- Retain general extent of existing visibility from this location, uncrowded by new built form
- Curtilage zone to extend west of pylon to maintain its full and clear view along foreshore
- New built form may obscure stay cables outside curtilage zone and hide bridge on/off-ramp feeding to City West link

#### EXISTING VIEW EXTENT



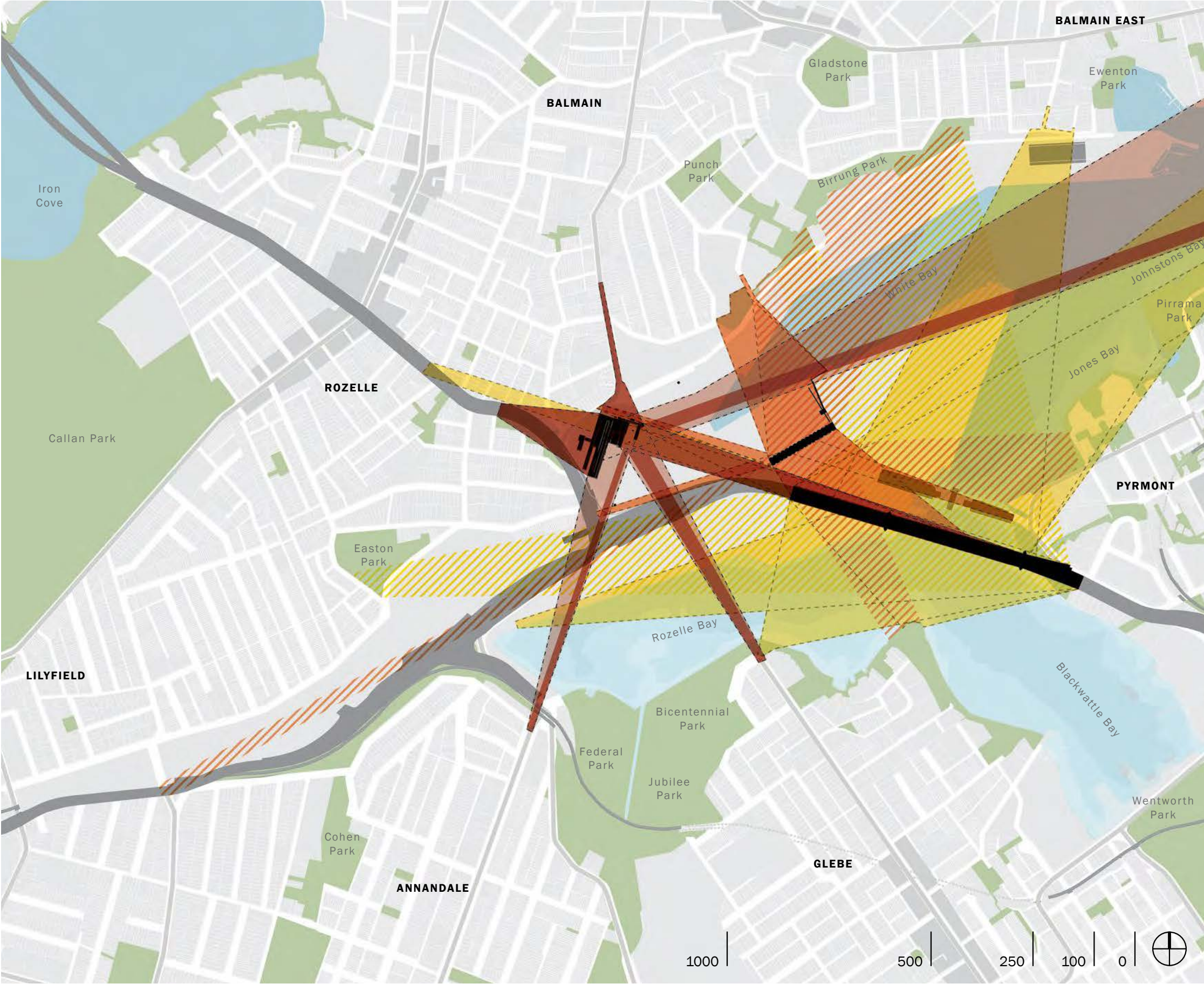
IMAGE A.44 Rozelle Bay Foreshore Viewshed  
33°52'15.6"S 151°10'24.9"E



# Combined Viewsheds

## A.1 VIEWSHEDS

Development capacities need to be carefully considered relative to the visual curtilage of these landmarks. Strong justification and visual evidence of improved outcomes should be provided where proposed master plans seek to vary from these, particularly the White Bay Power Station viewsheds contained within the endorsed Conservation Management Plan needs to be considered.



- LEGEND**
- Bays West Site Boundary
  - WBPS Mandatory Minimum View Retention
  - WBPS Curtilage Associated with Viewshed
  - Glebe Island Silos Significant Views for Retention
  - Glebe Island Silos Other Views Desirable for Retention
  - Anzac Bridge Significant Views for Retention
  - Anzac Bridge Other Views Desirable for Retention

DIAGRAM A.17 Combined plan of landmark item viewsheds



A.2

# PUBLIC ACCESS PARAMETERS



# WBPS Public Access Parameters

## A.2 PUBLIC ACCESS PARAMETERS

The White Bay Power Station is the only remaining example of an early twentieth century power station in NSW to retain a substantially intact and representative set of buildings, structures and in-situ machinery that demonstrate the complete operating systems and processes of coal fired power generation and supply. It is extremely rare and has national significance. It contains raw industrial spaces, both externally and internally of a scale, quality and configuration that are increasingly rare and which inspire visitors and users alike. Their significance is greatly enhanced by, and in most cases dependent on their associated in-situ machinery.

The power station must be conserved and adapted for an appropriate new use or uses that retain and respect its cultural significance and importantly, allow for public access.

Specifically in regard to this mandatory condition, public access to significant machinery and spaces as follows:

### General Public Access

/ Physical and visual access must be open to the general public to all parts of that zone.  
Examples may include public foyer spaces or food and beverage or innovative uses which allow public access benefit. Some level of control may be required to address safety, vulnerability and security.

### Visual Public Access

/ Space and volumes can be viewed by the general public from key viewpoints or from areas of General Public Access. Physical access is not required.  
Examples may include auditoria, entry foyers, meeting spaces, offices or other uses.

### Limited Public Access

/ These spaces may be sensitive, fragile or require increased levels of security. Physical and visual public access to the spaces must be provided but can be accommodated as part of tours or special events. Access to these spaces and associated machinery may be within secure or restricted areas and/or occupied by lessees. Examples may include as part of tours or special events such as Sydney Opera House back stage tour or Sydney Open.

### PUBLIC ACCESS ZONES OVERVIEW

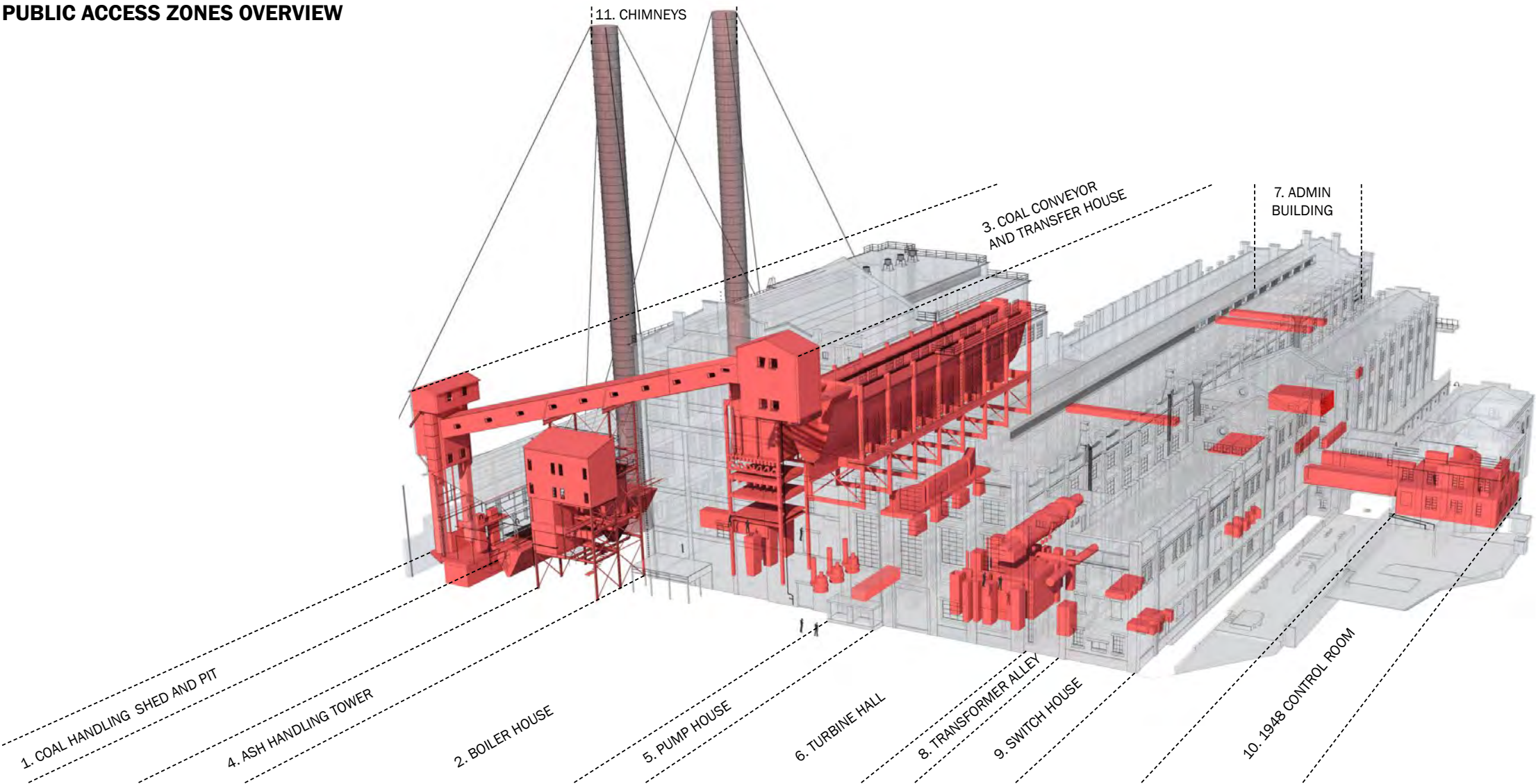


DIAGRAM A.18 Significant Features of White Bay Power Station identified for public access

#### 01 Coal Handling Shed And Pit

/ General Public Access to north end at ground floor.  
/ Visual Public Access to the remainder

#### 02 Boiler House

/ General Public Access to extant boiler 1 and void for boiler 2 at all levels.  
/ Limited Public Access to Boiler House control room.  
/ Visual public access to remainder of Boiler House  
/ Retain and preserve visual dominance of extant machinery including hoppers from General Public Access zones.

#### 03 Coal Conveyor and Transfer House

/ Retain as visually prominent elements.  
/ Public access to internal spaces not essential.

#### 04 Ash Handling Tower

/ Retain as externally dominant elements  
/ Internal physical public access not essential

#### 05 Pump House

/ General Public Access to the northern end from all Turbine Hall levels.  
/ Visual Public Access of extant machinery including water tanks at high level.

#### 06 Turbine Hall

/ General Public Access to all levels at the northern end including extant Machinery.  
/ Visual Public Access to the remainder of the Turbine Hall above the turbine deck including visual continuity along full length of the space.

/ Public access not required to levels below main turbine deck except at location of extant turbine 1.

#### 07 Administration Building

/ General Public Access to entry foyer off Victoria Road  
/ Limited Public Access encouraged to executive offices and laboratory on top level.  
/ Public access not required to spaces beyond.

#### 08 Transformer Alley

/ Visual Public Access from the northern end.

#### 09 Switch House

/ General Public Access preferred but may be Limited Public Access to the original

Control Room overlooking the Turbine Hall  
/ Limited Public Access to extant machinery and associated spaces cycle switches and adjacent generator/switch room, battery room and Entertainment Hall.  
/ Public access to remainder of Switch House not required.

#### 10 1948 Control Room

/ Limited Public Access to the Control Room  
/ Limited Public Access to the Cable Room and associated Cable Corridors.  
/ Public access to the Cycle Switch House is not required.

#### 11 Chimneys

/ Must remain as visually dominant elements on the site and General Public Access to the base.



# Public Access Parameters

## A.2 PUBLIC ACCESS PARAMETERS

### SIGNIFICANT MACHINERY & SPACES

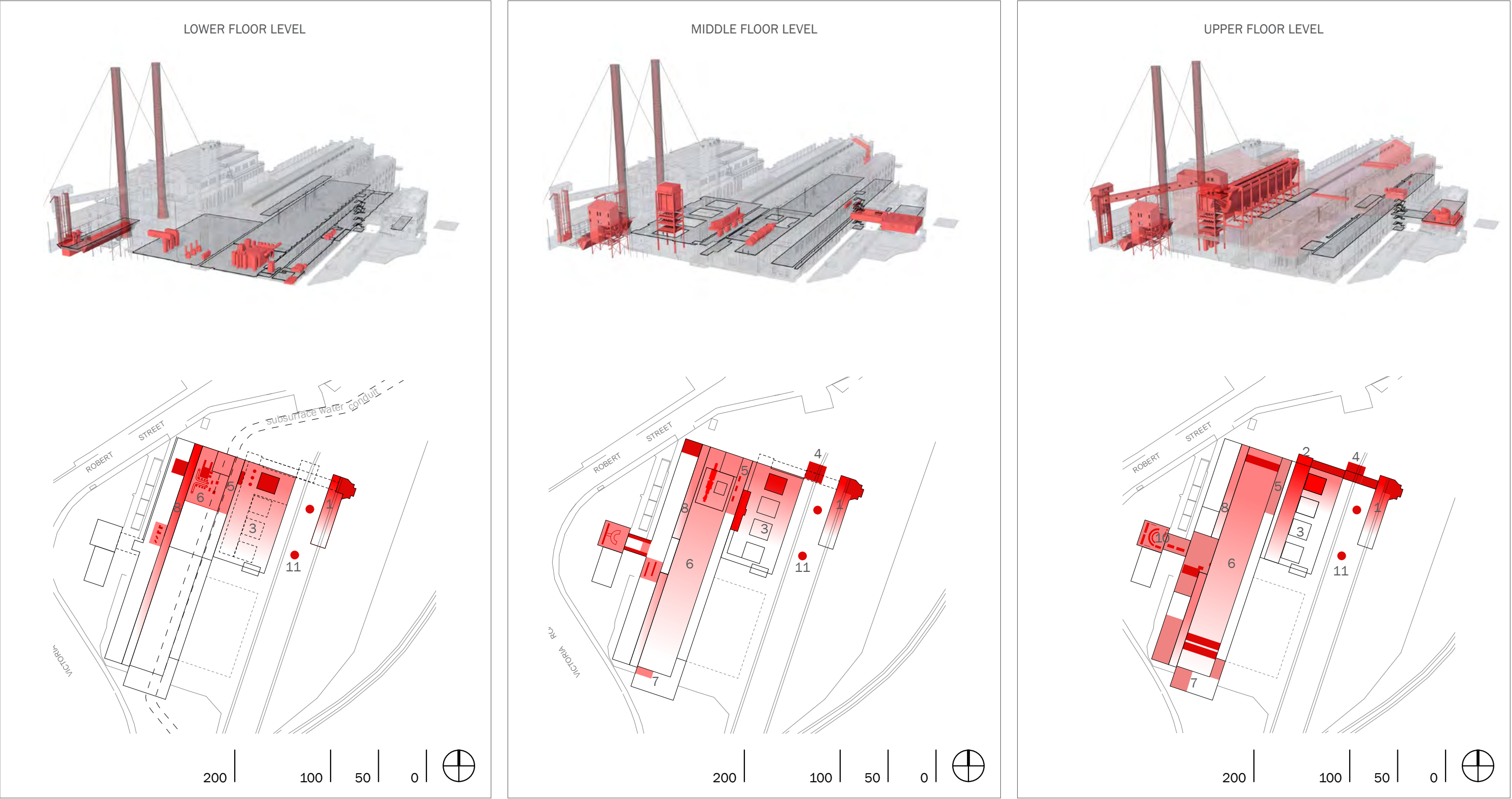


DIAGRAM A.19 Significant Machinery & Spaces across the lower, middle and upper levels of WBPS



