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Introduction

This Urban Design Report has been prepared to provide the architectural analysis and drawings to support the Planning Proposal for the Adina Hotel site (the site) within the Western Gateway Sub-Precinct of the Central Station State Significant Precinct (Central Precinct).

The purpose of this design report is to investigate the site's development potential for a mixed-use development which will include hotel, commercial office and retail.

The restoration of the heritage-listed building on the site together with the new elements including publicly accessible floorspace and public domain improvements will collectively contribute to the Government's vision for an iconic technology precinct and transport gateway. New public spaces will connect into the city and provide improved pedestrian connectivity for an efficient modal interchange aligned with the objectives of Transport for NSW (TfNSW).

The specific proposal for the site, submitted for inclusion within the TfNSW re-zoning application for the Western Gateway Sub-Precinct, is as follows:

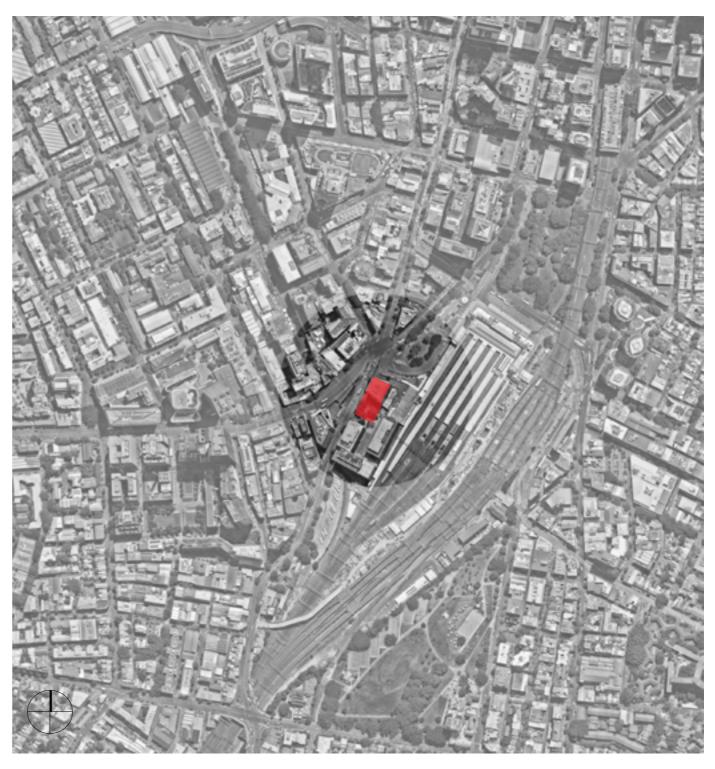
- Retention of the B8 Metropolitan Centre zoning of the site which enables the delivery of commercial premises, retail premises, business premises and hotel accommodation on the site
- Demolition of inappropriate additions to the existing Adina Hotel building
- Conservation and supporting works to the existing Adina Hotel building, and continued use of the building for retail premises and functions supporting the commercial and hospitality uses in the project
- Construction of a hotel and commercial office building above and adjacent to the Adina Hotel building
- A maximum building height of RL 211.981 including lift, plant & architectural roof feature

- defined by the Prince Alfred Park sun access plane
- The maximum gross floor area on the site includes 41,000sqm within the proposed tower envelope above and adjacent the former Parcels Post building with additional floor space below the upper ground level of the site to activate the Henry Deane Plaza and pedestrian links to a total maximum of 43,000sqm across Block C
- Delivery of a revitalised public domain across the site that is coordinated with adjacent development, including a new north-south thoroughfare adjacent to the YHA site, and an expanded and improved public space to the south
- The proposal will establish a planning framework to guide the future development of the site and surrounds to ensure an integrated public domain and development outcome is achieved across the Western Gateway Precinct

The project team undertook a series of investigations and researched the site and its environment. Through careful analysis and consideration, an approach was developed that will benefit the heritage element, contribute positively to the precinct by adding diversity to the proposed uses and provide an important contribution to the architectural composition for the precinct. The process has included extensive consultation with the SDRP (State Design Review Panel) and addressed comments and feedback as the process progressed.



/ Envelope for consideration. Refer page 35 for further details



/ Block C Context

Site Location

Located at the southern end of Sydney's Central Business District, the site (Block C) is bounded by Ambulance Avenue (future Central Walk West) to the north, Block A to the east, Block B to the South and Lee Street to the west.



/ City Location



/ Western Gateway Precinct Plan

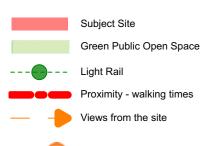
Design competition winning scheme massing shown for Block A and B $\,$

Contextual Analysis

Site Analysis

The site sits within the core of the city's eastern economic zone stretching from Sydney Kingsford Smith Airport to the south, to Chatswood to the north and taking in several tertiary education institutions, industry and key business districts. This zone is enabled by access to good public transport infrastructure, robust and modern power and communications infrastructure supporting as well as access to great leisure spaces. This highly connected zone attracts and supports higher education, research and technology uses including secondary supporting industries. The proposed development, located within the core of this zone and anchoring the southern end of the city spine of George Street, connects Railway Square through Town Hall and Martin Place to Circular Quay. The proposal enables further connectivity improvements in pedestrian circulation at this southwestern side of Central Station with passive provision made for future connections between the site and Surry Hills.





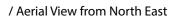
- Train Station
- Bus Stop

- 1. Block A
- Block B
- 3. Block C
- 4. Sydney Central
- 5. UTS Tower
- 6. One Central Park
- 7. The Mark
- 8. McKell Building
- 9. 15-31 Park Street
- 10. Mercure Sydney
- 11. Peak Apartments



Design competition winning scheme massing shown for Block A and B







/ Aerial View from East



/ Aerial View from West

Open Spaces

Sydney is characterised by a rich network of green public spaces. These are often within walking distance from each other and provide relief from the everyday intensity of the inner city. These various types of open space including playgrounds, sports & recreational areas, event spaces or ecological corridors, form the backdrop of Sydney life all year round.

The precinct sits at the intersection of several of these green links and lies adjacent to Belmore Park and across the rail, Prince Alfred Park which provides recreational and leisure amenities. This is the most significant of the adjacent areas of open space and solar access protections for this space are designated within planning controls.

To the north of the site is existing open space surrounded by the bus drop-off. This area is earmarked for a future plaza and combined with Railway Square forms part of the 'Third Square' at the south end of the George Street civic spine. To the South is Henry Deane Plaza which provides a pedestrian access into Central Station and a connection to Devonshire St and Surry Hills to the East.

Future public open space is anticipated to be delivered as part of any over station development proposal. The position and extent of these elements is currently not known.



/ Open Spaces

Site Survey

6

The site is located at the north western corner of the Western Gateway Precinct on the corner of Lee Street and the existing northern vehicular access to the precinct. 'Block C' is generally described as the land including the Adina Hotel building and Henry Deane Plaza and is legally defined as comprising:

Lot 30 in Deposited Plan 880518 (Adina Apartment Hotel)

Lot 13 in Deposited Plan 1062447 (Henry Deane Plaza)

Site Area: 5450m2

The allotments that comprise Block C are partially unlimited and partially limited in either height or depth. The land that comprises Block C and is under TOGA's control (either completely or by way of a lot that is limited in either height of depth) is illustrated above and comprises a total area of 5,450m2. The site includes the pedestrian through-site link from Lee Street to the Devonshire Street Tunnel connecting through the Central Station.

PHALE OF WALLS



/ Boundaries generally above RL 19.1



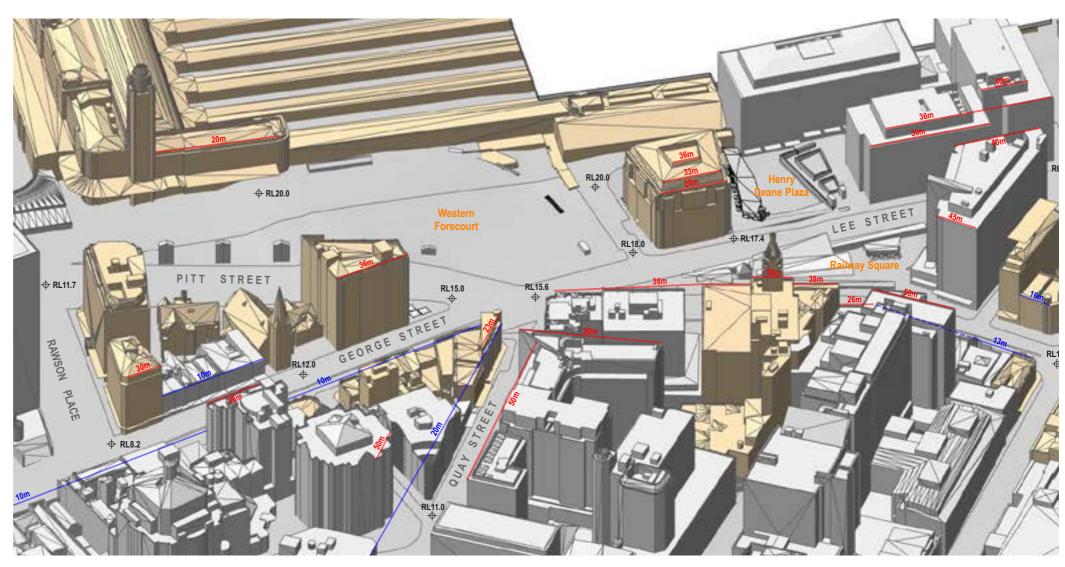


/ Boundaries generally below RL 19.1

Existing Building Envelope and Streetscape

There are a significant number of adjacent Heritage buildings as illustrated to the right. Refer to the heritage report for greater detail.

The development of the Parcels Post building into what is now the Adina Hotel was completed for the Sydney 2000 Olympics. Prior to this the building was derelict and what is currently the Henry Deane Plaza was still occupied by railway uses. The conversion of the Parcels Post building included the creation of a publicly accessible private open space to the south, which enabled better pedestrian connections to and from the Devonshire Street tunnel. This connectivity was further enhanced by a tunnel extension under Lee Street and Broadway, west to the Goods Line pedestrian link.



/ Topography and Street wall heights and Heritage



/ Looking south along George Street



/ Looking south along Pitt Street adjacent to Belmore Park



/ Looking east from the intersection Quay Road



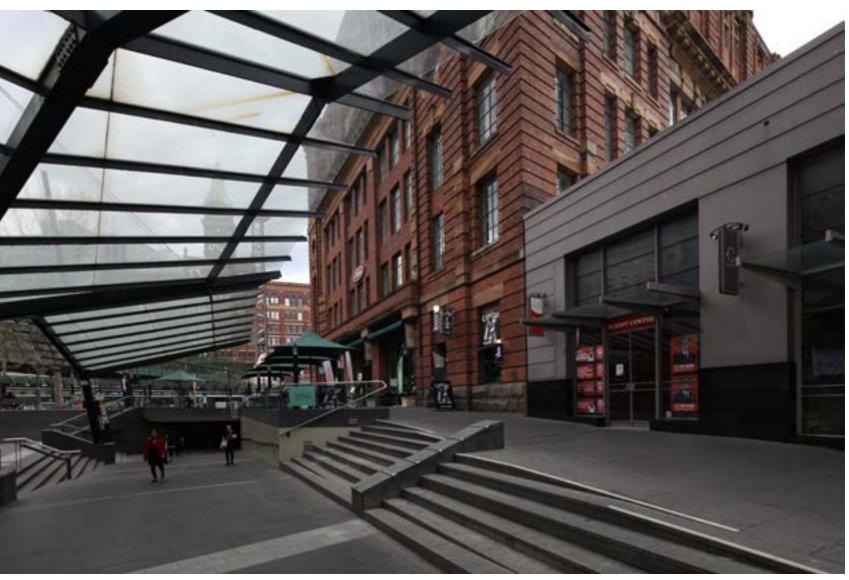
/ Looking north along Broadway near Chippendale Way



/ Looking south from Central Station concourse vehicle ramp

Henry Deane Plaza

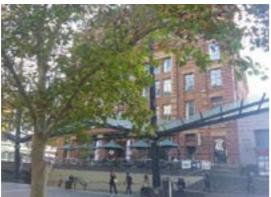
The use of Henry Deane Plaza is hampered by a series of level changes and lack of level access to street frontages. The retail offering primarily serves those transiting the site between rail and bus modes as well as to UTS and Surry Hills.



/ Looking West towards Lee Street



/ Looking north across Henry Deane Plaza



/ Looking North from Henry Deane Plaza



/ Looking at Adina Shopfronts on Southern Facade



/ Looking down towards Henry Deane Plaza from Lee Street



/ Looking towards Lee Street Tunnel Entrance from Henry Deane Plaza

Site Analysis - Public Transport







<u>Train</u>

The urban planning and design focuses on the precinct associated with Central Station and the Railway Square bus interchange, light rail and future metro. It is also a destination for many, distributing travelers to places of work and study nearby.

Metro

Central Station is a key nexus connecting Sydney through multiple modes of transport. The Sydney Metro are new additions currently under construction. The site is uniquely positioned to add amenity and connectivity between Railway Square and other destinations to the west of the Western Gateway sub-precinct and the Sydney Metro potential egress point.

Light rail

With the completion of the new light rail between Randwick and Circular Quay and the addition of the Metro platform including the Central Walk gateway just north of the precinct, renewing these north/south connections and improving the quality of the pedestrian experience east will be paramount. In addition, reinforcing good quality access across Lee Street to the bus interchange will improve connectivity to these services.

The intensity of these public transport services at this junction of the city, and the related capacity implies an opportunity for significantly increased density and economic activity supporting existing and potentially new complementary uses.







<u>Bus</u>

The study area is well connected with existing and proposed bus infrastructure

Bicycle

The City of Sydney bicycle pathway map shows a shared pathway arriving near the study precinct from the south along Regent Street.

With the addition of a better north/south shared way through the precinct, as well as realising future opportunities related to Integrated Station Development for additional over track connections between Surry Hills and Chippendale, a safe environment can be maintained for cyclists and pedestrians alike. This can be further supported with the introduction of a cycle hub, perhaps utilising the historic goods line tunnel for access connecting Lee Street and Regent Street with the goods line walk to Darling Harbour.

Pedestrians

Primary pedestrian movements are currently through the Devonshire Street Tunnel, connecting the railway to the interchange or beneath Lee Street and on to the University of Technology, Sydney, TAFE and other educational facilities. A smaller but still significant proportion of pedestrians belong to workers with smaller numbers relating to tourists and day- trippers.

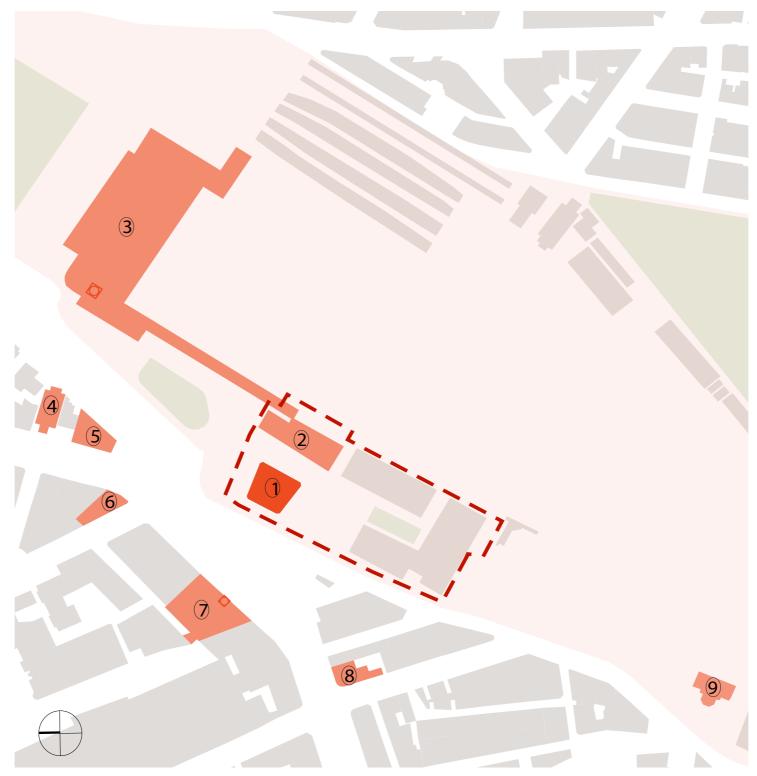
The future extension of Central Walk, emerging into what is currently Ambulance Lane, will move the centre of gravity for commuters to the north of the precinct study area. This is likely to have some impact on the numbers of people walking around the east and southern side of the former Parcels Post building or simply heading over ground to a scramble crossing at the junctions of Pitt, George, Quay streets and Broadway. This scenario also envisages the pedestrianisation of Lee Street which, except in inclement weather, will improve the quality and experience of the over ground journey for pedestrians.

Site History

The former Parcels Post building was designed in the NSW Government Architects office in 1910 and opened in 1913. It was constructed in response to the need for expansion of the parcels facility at the GPO which had become overcrowded. As the majority of parcels at that time were sent by rail, it was ideally situated next to central station, for the construction of a purpose built parcels post office. The Parcels Post worked in conjunction with the inward and outward parcels platforms in the western yard precinct (which includes the former inland parcels platform, now the YHA), with parcels being brought to and from the trains, via a network of underground passages. The scale of the building is indicative of the importance of shipping parcels by rail in the early twentieth century and the subject site documents the historical association of the site with Central station and railway postal services.

Henry Deane Plaza was historically the site of the Western Yard of the former Parcels precinct. That part of the yard contained a number of structures including the West Carriage Shed, support offices, demountable workshops; and a store. These buildings and features were demolished for the construction of Henry Deane Plaza which was constructed between 1998 and 2000. At the entrance to Devonshire Street Tunnel is a large public sculpture and a glazed structure covers the walkway leading into Railway Square. This area forms part of the busy pedestrian connection from Central Station to Railway Square and on to George and Pitt Streets, and pedestrian subways.

The area includes several notable buildings in various stages of repair and usage. Most notable is the Marcus Clark Building with its prominent tower, now re-purposed to a Technical College which reflects the changing character of this precinct as it pivots to support education and some tourism uses.



State Significant Buildings

- 1. Adina Hotel / Former Parcels Post Office
- 2. Inland Parcels Shed
- 3. Central Station
- 4. Christ Church St Laurence
- 5. 509 Pitt Street Wake Up Sydney Hotel
- 6. 140 George Street / 'Former Lottery Office'
- 7. Marcus Clark Building
- 8. 824-826 George Street / Former Bank of NSW
- 9. Regent Street railway station







2. YHA / Former Inland Parcels Shed



3. Central Station



4. Christ Church St Laurence



5. 509 Pitt Street Wake Up Sydney Hotel



6. 140 George Street / 'Former Lottery Office'



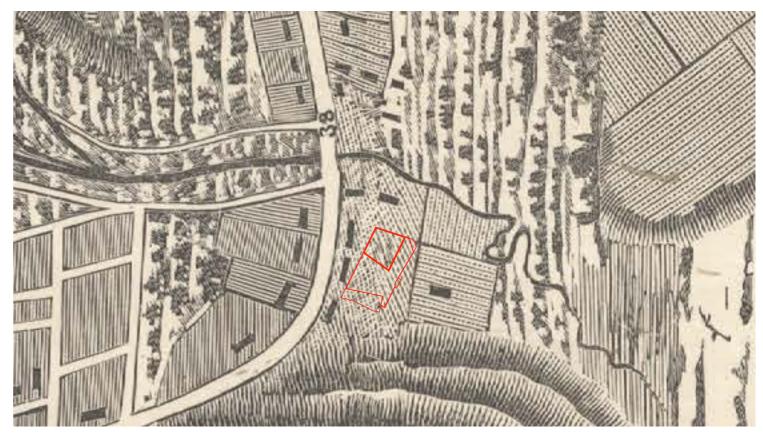
7. Marcus Clark Building



8. 824-826 George Street / Former Bank of NSW

Cultural Heritage and Site Morphological Evolution

The following images show the progression and evolution of land use from colonial times to the present day with the next phase being the Innovation and Technology Precinct



Site boundary overlay from 1802



Site boundary overlay from 2020





/ Extract of Charles Alexandre Lesueur's **1802** map of Sydney / Detail from plan of the town and suburbs of Sydney, **1822**



/ Extract of 'Plan de la ville de Sydney 1838 map



/ Plan of grounds to be appropriated for Sydney Railway, 1849



/ Extract 1854 'Map of the City of Sydney'



/ Map showing Sydney Terminal which opened in 1855



/ 1903 Extension of terminus incorporating the former cemetery. / Extract of Map of Sydney 1923





/ Extract 1949 Aerial Photographic Survey



/ Trams 1955



/ 1960



/ Railway Square 1967



/ Devonshire Tunnel Extension 1970



/ Railway Square Bus Station and Devonshire Tunnel 1979

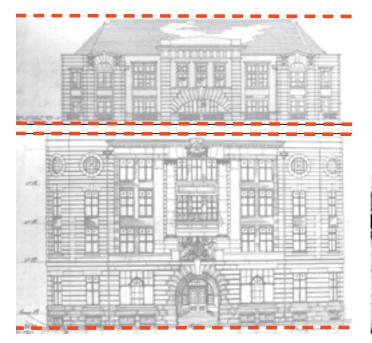


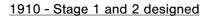
/Present



/ Extract of Map of Sydney 2020

Site History - Adina Timeline





- Stages 1 and 2 of the Parcels Post office designed by the Government Architects office NSW, Gorrie Mcleish Blair under supervision of Walter Liberty Vernon
- Example of Federation Free Classical style



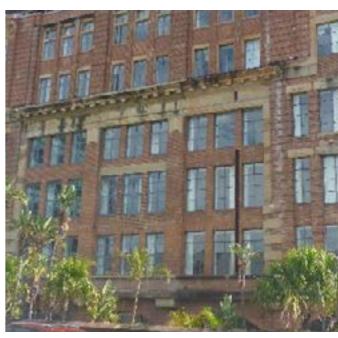
1913 - Building Completed

Building completed and opened with 6 stories and single basement



1915 - Parcel lifts added

 Two parcel lifts and a large rear extension added to the eastern facade



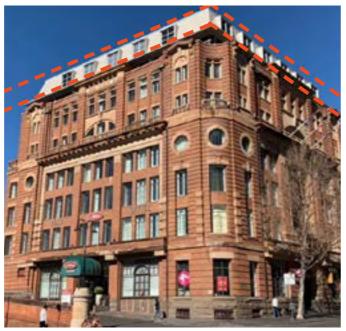
1969 - Parcel lifts removed

- Parcel lifts and rear extension removed
- Facade is reconstructed similar to the original
- New window frames installed as aluminium (originally steel)





 Building was largely left vacant after the Post Office closed at Railway Square



1999 - Hotel Conversion

Building is converted to a hotel with:

- Minor ground floor modifications
- Secondary access added to the northern facade for level access
- Original roof removed and mansard roof added
- Significant refurbishment to interior

In the late 1990's the original historic hipped roof form and roof lantern were replaced with a 2 storey mansard roof addition. Internally the addition is a single storey with loft. This was done in conjunction with the adaptive reuse into a hotel





2020 - Proposed future use

- Opportunity to restore and enhance heritage fabric
- Create a world class public domain
- Incorporate a vibrant mix of retail, commercial and hospitality uses
- Complement the aspirations for the Western Gateway Precinct

Existing Planning Framework

Greater Sydney Regional Plan

The site adjoins Central Station, the most connected station in NSW serving as an interchange between the new Sydney Metro, new CBD and South East Light Rail, and existing suburban, intercity and interstate train services.

The station sits within the Eastern City District of Sydney's CBD. This district along with the Harbour CBD are the current engine room of Greater Sydney's economy. Of the District's 904,500 jobs, 496,100 are in the Harbour CBD (2016 estimates). Almost half of these are in the finance, professional, business and knowledge sectors.

The Eastern Harbour City has Australia's global gateway and financial capital, the Harbour CBD, as its metropolitan centre. well-established, well serviced and highly accessible by its radial rail network, it has half a million jobs. The regional and district plans anticipate the ongoing strategic role and sustained jobs growth within the Eastern Harbour City over the next 20 years.

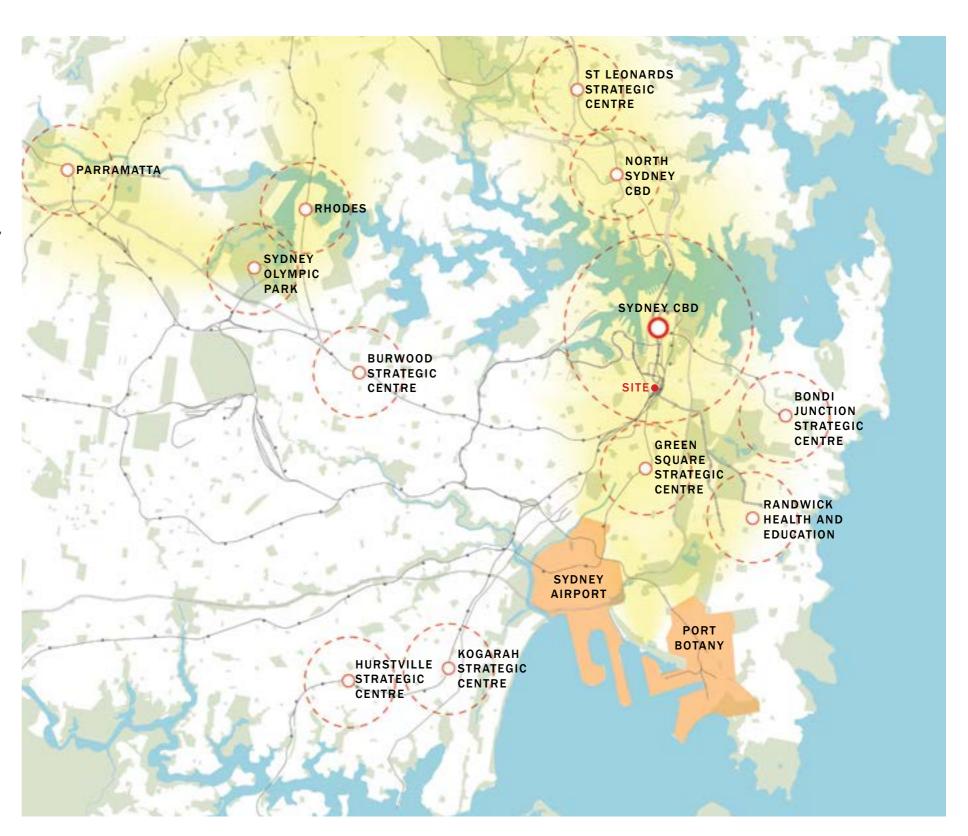


Image from Greater Sydney Regional Plan

Central Sydney Planning Strategy

The Draft Central Sydney Planning Strategy (CSPS) by the City of Sydney, unlocks economic opportunities and investment in jobs and supports public improvements that make Sydney an attractive place for business, workers, residents and visitors. The CSPS outlines 10 key moves which prioritize employment growth, increase capacity and ensure infrastructure keeps pace with growth, creating a more sustainable and vibrant public spaces. The CSPS is a 20 year growth strategy that revises previous planning controls and delivers on the City of Sydney's Sustainable Sydney 2030.

The CSPS, together with the guidelines for amending planning controls, will deliver 2.9 million sqm of new floor space in predominantly in four new tower clusters. The southern most of these relates to the Central Station renewal area created by the NSW government and referenced in the Sydney Innovation and Technology Precinct Panel Report.

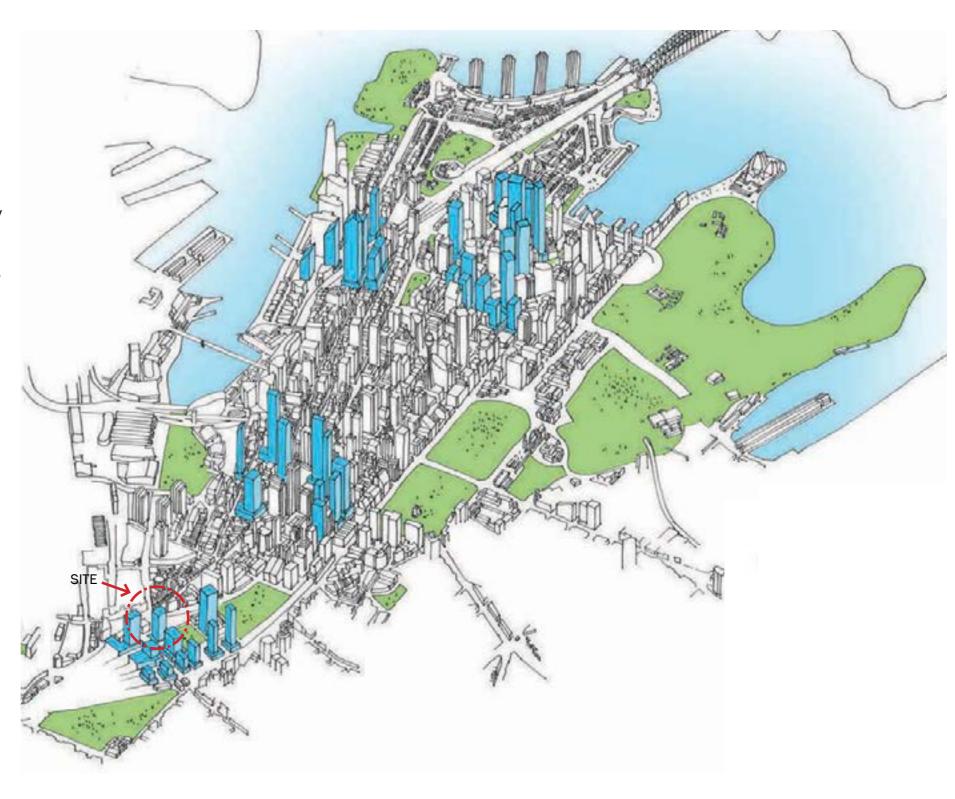


Image taken from Draft Central Sydney Strategy Planning Document

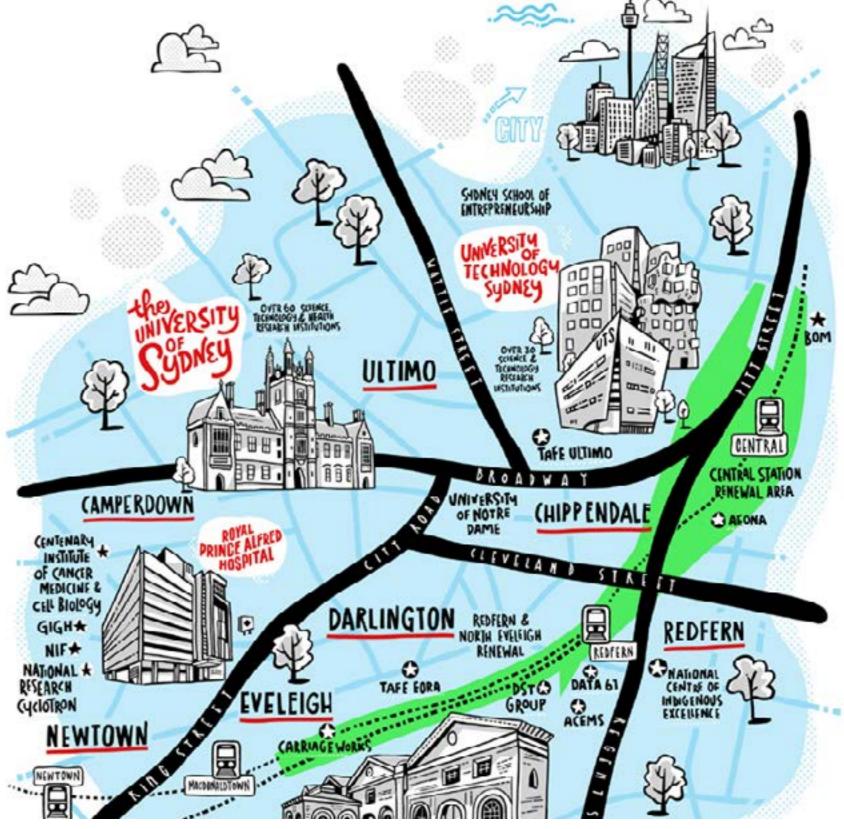
Tech Central

In August 2018 a Panel was formed to advise the NSW Government on a vision to support the development of an Innovation and Technology Precinct in the Central to Eveleigh area.

The panel, lead by David Thodey AO Chair of Jobs for NSW included representatives from industry, health, education, government agencies and key community members.

The vision for Tech Central has been developed by collaborating with the technology ecosystem, industry, health, education, and government. The Panel believes the Precinct will drive a sustainable and inclusive approach to the development and growth of the people, the industries and the communities who share its space.

Tech Central is a place where world-class universities, ambitious startups, high-tech giants and the community collaborate to solve problems, socialise and spark ideas that change our world. Tech Central will be underpinned by high quality physical and digital infrastructure.



Images taken from Sydney Innovation and Technology Precinct repot 2018

Central Station Revitalisation Metro

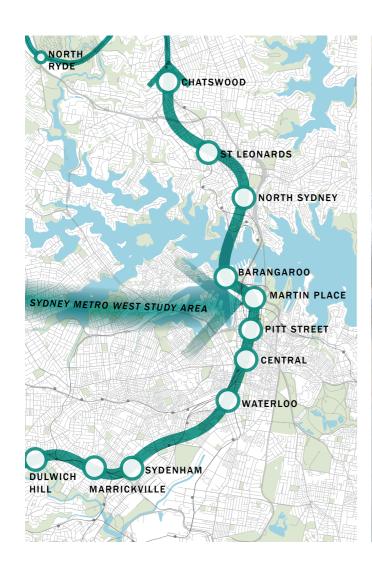
There are significant rail projects underway in the Eastern Harbour City to increase competitiveness, boost business-to-business connections and attract skilled workers with faster commuting times.

The new Sydney Metro link connects the Northwest Rail line from Chatswood to the Sydney Metro South West line at Sydenham in the South. Future connection to the planned Metro West line will also significantly change commute times to and from Greater Parramatta.

The Sydney Metro Line will interchange at Central with the existing Inner West light rail and the new CBD and South East Light Rail line. Revitalisation works on Central Station to improve connectivity between trains, buses, light rail and the new Metro will include a new 19m wide underground concourse (Central Walk). Additional access points to Train Platforms 16 to 23 boost capacity to cater to the projected 66% increase in daily customer demand.

Sydney Light Rail

The CBD and South East Light Rail is a new light rail network for Sydney. The 12 km route, currently under construction will include 19 stops, extending from Circular Quay in the North to Randwick and Kensington in the South. This new service will connect the major city spine of George Street all the way from Circular Quay along with major open spaces around Moore Park and the Randwick Health & Education Super Precinct with Central Station.









Built Form Analysis - Sydney LEP 2012

The following section reviews and compares the sites development potential under the current controls of the Sydney LEP 2012

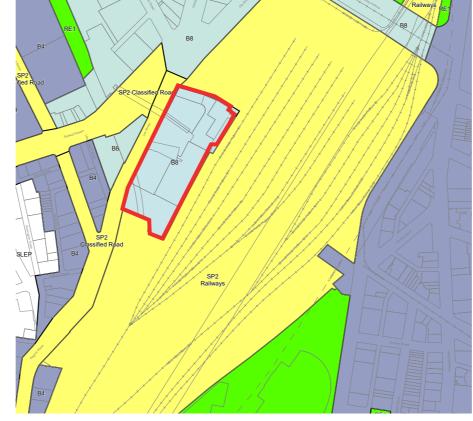
(updated August 2020 in Clause 6.53 to Part 6 Local provisions – height and floor space of the Sydney Local Environmental Plan 2012)

Sydney DCP 2012, and DRAFT CSPS.

In investigating the sites development potential the following site constraints and planning controls have been considered:

- Height
- Setback
- FSR
- Sun access
- Requirement for through site links
- Achievement of a Global Office Tower and aspiration to achieve PCA Premium Grade floor plate
- Views to and from Circular Quay

A key consideration in the assessment of the above controls and constraints was the relationship of the each envelope within the context of the following proposed indicative tower envelopes on neighbouring sites within the Western Gateway Precinct:







/ HEIGHT OF BUILDING - SYDNEY LEP 2012

Zoning - Sydney LEP 2012

Key Town Planning controls currently applying to the site include the following:

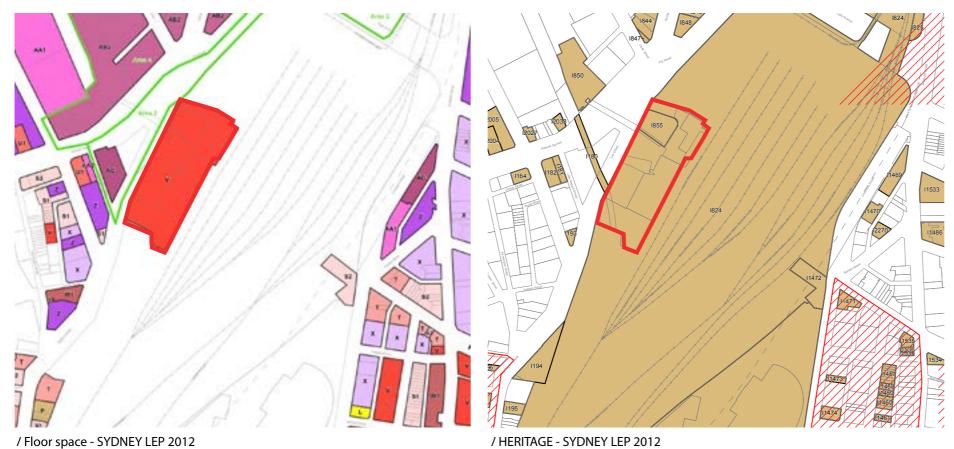
Zoned B8 Metropolitan Centre, with key objectives including:

- To recognise and provide for the pre-eminent role of business, office, retail, entertainment and tourist premises in Australia's participation in the global economy.
- To provide opportunities for an intensity of land uses commensurate with Sydney's global status.
- To permit a diversity of compatible land uses characteristic of Sydney's global status and that serve the Workforce, visitors and wider community.

Height of Building - Sydney LEP 2012

While this B8 Metropolitan Centre Zone permits most forms of land use. The Sydney LEP 2012 further limits development by:

- Maximum permissible building height of 30 metres; and
- Finding that no additional shadows are to be cast on Prince Alfred Park any day between 10am and 2:00pm.



/ HERITAGE - SYDNEY LEP 2012

Floor space - Sydney LEP 2012

Maximum permissible FSR of 3.0:1 may be exceeded if complying to Clause 6.53 (7)

Heritage - Sydney LEP 2012

Adina Hotel building is listed as an item of local heritage significance on a standalone basis.

The 'Sydney Terminal and Central Railway Stations Group' (including the Adina building) is listed on the State Heritage Register.



/ LOCALITY STATEMENT - SYDNEY DCP 2012

Locality Statement - Sydney DCP 2012

The northern part of the site is located within the Railway Square/Central Station Special Character Area LEP and DCP Maps.

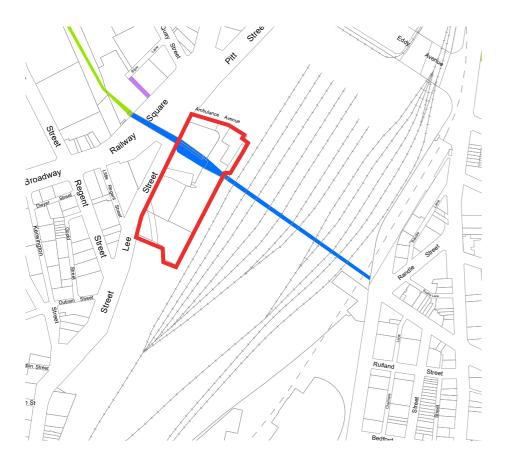
Key principles include:

- (a) Development must achieve and satisfy the outcomes expressed in the character statement and supporting principles.
- (b) Recognise the role of Railway Square as the western and southern gateway to Central Sydney.
- (c) Reinforce the significance of the existing heritage fabric and complement it with high quality contemporary fabric and contribute to the layers of meanings and content of the locality.
- (d) Maintain a high level of daylight access to Railway Square and its

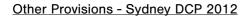
associated open spaces by restricting building height.

- (e) Maintain and enhance the visual prominence and landmark significance of the clock tower of Central Railway Station in the views and vistas from various points, particularly along Broadway and George Street, when approaching or departing the city.
- (f) New development is to maintain and enhance vistas to Central Railway Station.
- (g) Reinforce the urban character and scale of Railway Square by requiring new buildings surrounding the Square to:
- i. Be built to the street alignment;
- ii. Have street frontage heights consistent with the prevailing form of buildings adjacent to this Special Character Area; and

- iii. Have building setbacks above the street frontage heights.
- (h) Ensure that any development associated with the important public transport interchange provided at Railway Square is consistent with enhancement of the public domain of Railway Square.
- (i) Conserve and enhance the heritage significance and character of the nineteenth and twentieth century public and commercial buildings and their settings.
- (j) Enhance the pedestrian amenity of Railway Square and environs.



/ OTHER PROVISIONS - SYDNEY DCP 2012



Public domain:

• The site is affected by an existing underground through-site link (as marked in blue on the DCP map). A proposed through site link is shown in pink and an existing through site link shown in green affect the properties to the north

Transport and parking:

• on-site car parking spaces will be restricted based on public transport accessibility. Car share spaces will be required in addition to the maximum spaces, as well as bike parking and associated facilities

Late night trading:

• The site is located within a City Living Area. Base hours and extended hours apply to High Impact and Low Impact Premises. A Plan of Management will be required to be prepared and monitored

Development types:

• Detailed DCP requirements for specific land use activities within Central Sydney apply, including built form, setbacks, design, etc



/ Sun Access Protection map to Prince Alfred Park

Central State Significant Precinct

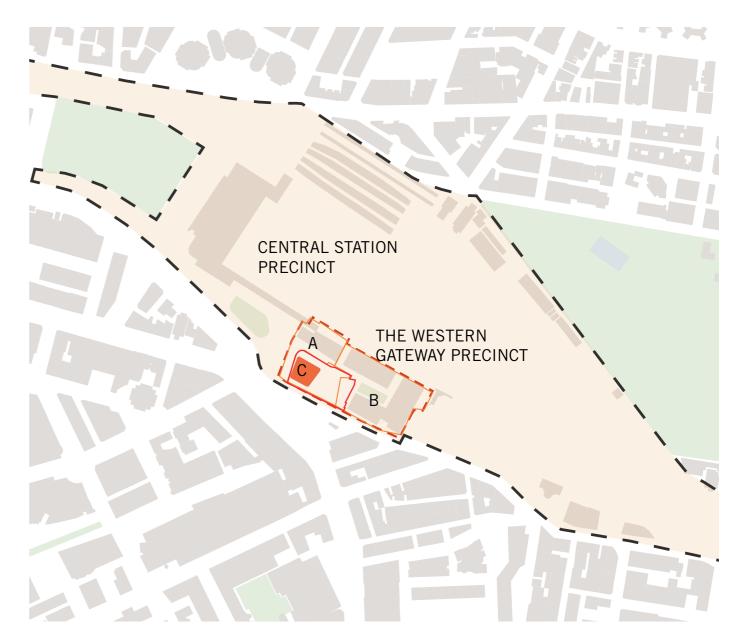
On 12 July 2019 the NSW DPIE announced that the Minister for Planning had declared the Central Station Precinct (Central Precinct) as a Nominated State Significant Precinct. The Central Precinct meets the relevant criteria for the declaration of a Nominated State Significant Precinct as it:

- Is a large area of land owned by the NSW Government and leased to a small number of private parties;
- Is of State or regional importance by having the capability to achieve Government policy objectives, particularly those relating to increasing delivery of housing and jobs; and
- Comprises items and areas of State or regional importance for heritage or historical significance.

The nominated Central Station Precinct is located at the southern end of the commercial core in the Harbour CBD. It extends to an area of approximately 24 hectares bounded by Pitt Street and Regent Street to the west, Cleveland Street to the south, Eddy Avenue, Hay Street and Goulburn Street to the north, and Elizabeth Street and Chalmers Street to the east (refer to Figure 6). The precinct includes:

- Central Station and surrounding NSW Government owned land along the rail corridor;
- Goulburn Street carpark; and
- A precinct along the Lee Street edge of the precinct, known as the Western Gateway (within which the subject site is located).
- TfNSW is developing a vision for the growth and development of the Central Station Precinct SSP and is preparing a strategic framework to guide future planning of the area. The vision is to:
- Create new public spaces for locals, visitors, businesses and customers;
- Better connect communities and businesses to surrounding neighbourhoods;
- Revitalise and expand Sydney's CBD's commercial core to the south; and

- Provide spaces for creative and technology businesses as part of the Sydney Innovation and Technology Precinct.
- The strategic framework will address key matters including vision, priorities, public space, strategic connections, and design excellence and identify sub-precincts for future detailed planning.
- As part of this strategic framework, TfNSW has requested that a sub-precinct within the Central Station SSP be considered for priority planning. The 'Western Gateway' sub-precinct includes the Central YHA site (Block A), Frasers/DEXUS Properties (Block B), and the Adina Hotel site (Block C) (as illustrated).

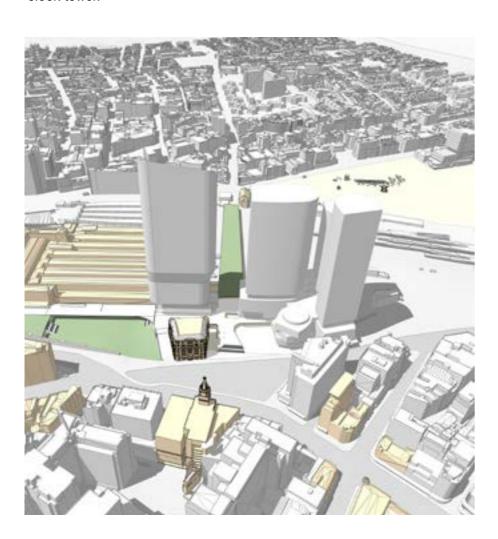


Solar Plane / New Urban Setting

As the city evolves to support a growing population and its demand for places to live, learn, work and play, the nature and character of the Central Western Precinct is also changing. Located within a fourth opportunity zone, incremental increases of density and building height are proposed to the north and west of the site as well as the proposed Central to Redfern development over Central Station Rail Yards.

More immediately to the east and south are neighbouring developments proposed by Atlassian and a partnership of Dexus and Frasers with heights defined by the City of Sydney no additional overshadowing solar plane to Prince Alfred Park (any day 10:00am - 2:00pm).

The proposed development will maintain key existing views from street level to nearby tree canopy of Belmore Park and heritage features of the Marcus Clark building and the Central Station Terminus Building and clock tower.







Design competition winning scheme massing shown for Block A and B $\,$

Strategic Principles

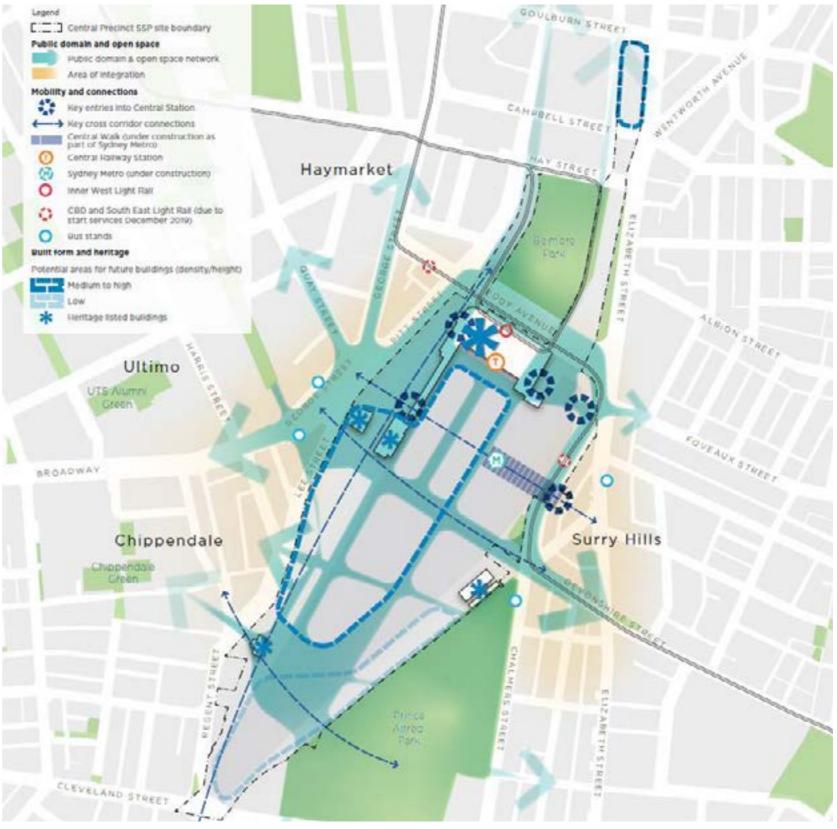
Pedestrian Movements

The Western Gateway sub-precinct anticipates a series of changes to the current pedestrian movement patterns.

- Central Walk the extension of the new major concourse element associated with the new Metro line and providing access to all the above ground platforms. Much of the current flow being redirected because of the current Metro construction will take advantage of this east west connector.
- North-South link from the Central Station country station forecourt to the north, via the Western Gateway Forecourt towards the southern precinct associated with the Mortuary Station. Connecting with the existing Devonshire Tunnel and Henry Deane Plaza.
- Western Interface point of the east-west connection across the future over station development as an extension of Devonshire Street.

The pedestrian flows associated with the Western Gateway sub-precinct will be considerable and it is anticipated that the vehicular traffic movements associated with public and private transport will be rationalised within the greater program to reduce congestion and safety issues.

The development of the Western Gateway sub-precinct will offer a range of opportunities to traverse Broadway and Railway Square.



/ Exhibited Preliminary Precinct Plan / Central Precinct Draft Strategic Plan

Public Domain

The Parcels Post building has a pivotal role in the developed concept for the Western Gateway Precinct. The building stands within the curtilage of what will become the Third Main Square for Sydney. Transforming the ground plane experience by reducing the amount of space dedicated to vehicle movements and unifying the public realm design is the aspiration for the space.

The proposal has the potential to meet the objectives of the Western Gateway Sub Precinct Public Domain strategy principles and objectives as listed below

- Clearly defines useable public spaces
- Multi-level pedestrian access
- Defines the unique heritage and cultural values
- Addresses wind, solar and daylight
- Prioritise East West movement

Vision

Central Precinct will be a vibrant and exciting place that unites a world class transport interchange with innovation and diverse businesses and high quality public spaces.

World class interchange

Central Station is Australia's largest and busiest transport hub. Its revitalisation will create a destination with celebrated heritage, increased amenity and a mix of uses

Sydney's 'Third' Square

Parcels Post Office along with Sydney Terminal and the former Inwards Parcels Shed will be focused around a new public square that will contribute to realising the City of Sydney's vision for three squares along George Street spine linking Central, Town Hall and Circular Quay

- Stitching back into the fabric of the city
- City making green and civic links connecting Central to the harbour, parks, recreation, universities and communities
- High value innovation and technology jobs anchoring Central Sydney south
- Expanding the City Centre to the South for Sydney's economy to grow and remain globally competitive
- An active destination with a 24 hour economy
- An iconic destination at the core of Southern Central Sydney

Principles

- Prioritise people
- A vibrant precinct
- Intuitive movement
- A green precinct
- Celebrated Heritage

Equitable and Inclusive

Provide full and equal access to all people of all levels of mobility

Work with TfNSW to develop a precinct wide strategy for social inclusion and equity

Henry Deane Plaza

Facilitate simple, direct movement between

modes of transport and changes of level

Pedestrian

Safe and comfortable pedestrian links and connections

Cyclists

Provide sufficient cycle parking and End of Trip to meet projected demands

Service Vehicle / Private car / Motorcycle

Comply with the 'TfNSW Requirements' to provide an integrated basement

Devonshire Street Tunnel

Retain the current levels, portal connection and operation of the Devonshire Street Tunnel

Comply with relevant TfNSW Requirements relating to the Devonshire Street Tunnel

Western Gateway Public Domain Review

TfNSW commissioned an independent urban designer to review the integrated site to achieve an optimised outcome, see following principles:

- Create a strong north south link at RL 21 that provides the address to lobbies and other functions as well as connecting into the east - west movement pattern
- Provide consolidated public open space at RL 21 connecting to Lee Street and the Over Station Development corridor to Devonshire Street
- Use the existing Heritage ramp to connect Lee Street to RL 21

TOGA is working collaboratively with TfNSW and adjoining owners to facilitate what is expected to be an updated Public Realm Strategy.

A considered approach to achieve a integrated public realm for the Western Gateway is welcomed. This principles once finalised will be incorporated into a future development approval application.





Heritage

Summary of Urbis Heritage report comments

Conservation Management Strategy (CMS) - Key Findings

Conservation Management Strategy

- Urbis has prepared a Conservation
 Management Strategy (CMS) which has
 informed the proposed concept. The
 CMS has considered the 1997 CMP
 prepared for the site by Heritage Group
 State Projects and the Conservation
 Management Plan prepared for Central
 Station
- The CMS has assessed the former Parcels Post to be of heritage significance for its historic, associative, aesthetic and representative values and rarity

Gradings of Significance

- The overall form and principle north, south and western facades are identified as being of high significance
- The eastern facade has been variously modified and reconstructed
- The interior steel encased structural grid including floors and columns has been graded as being of moderate significance

Condition

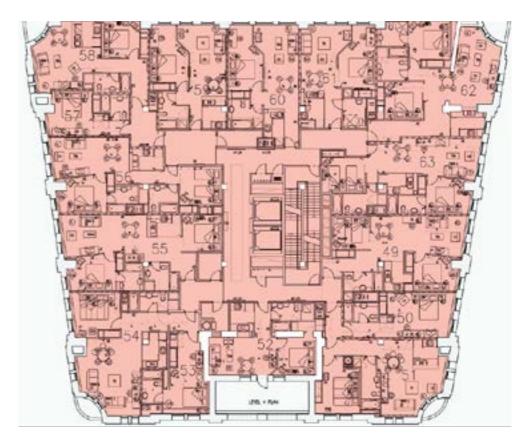
The building is generally in good condition however the facades and associated stone and brick work are in a overall poor condition. Emergency and ongoing conservation works have been identified and renewal of the site presents a significant opportunity to conserve and reconstruct the significant facades

Principles

- Restore the former parcels
 post building allowing it to be
 fully appreciated within the new
 architectural composition, in its role
 as the marker to the Third Square and
 greater central precinct renewal project
- Conserve the significant building facades
- Demonstrate the significance of the relationship between the Parcels Post, the parcels shed and the broader Central station precinct through physical and interpretive means
- Clearly delineate the heritage elements from the tower above and its neighbours
- New public uses being incorporated within the project and the public domain
- Differentiate the new structure and infrastructure from the original industrial character of the interiors and retrieve / expose the original interior structural components where possible.

Conservation and Reinstatement Works

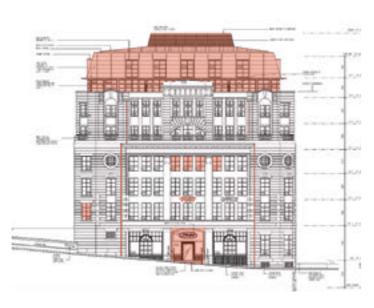
- Remove the later roof addition for the Hotel, reinstate and restore similar to the original design
- Retain significant portion of the internal grid and minimise structural intervention. Allow for reinforcement and reinterpretation of, rather than wholesale demolition of the structural grid
- Reinterpret the original character and spatial qualities of the building
- The majority of the eastern facade has been modified or reconstructed noting interventions for the parcel lifts (c.1915) and the later extension (c.1969). There is an opportunity for alterations in conjunction with an eastern core

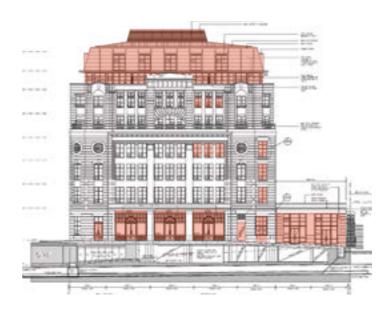


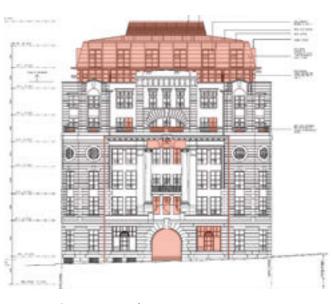
/ Current Typical Hotel Floor Plan

Modified Elements of Parcels Post

The areas shaded in red show modified elements from the original Parcels Post design including significant portions of the Eastern elevation and the mansard roof







/ Current North Elevation

/ Current East Elevation

/ Current South Elevation

/ Current West Elevation



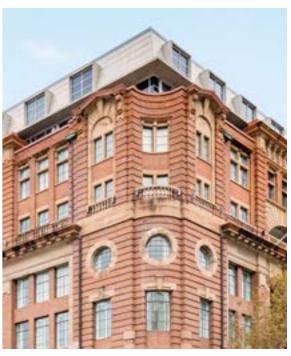
/ Lift attachments to Eastern elevation



/ Lift attachments to Eastern elevation



/ Current rebuilt Eastern elevation



/ Current North Western corner and mansard roof

Future Vision: 'Parcels' Post and Henry Deane Plaza

The Heritage 'Parcels' Post Building will be restored and reinvigorated and portions opened up for public access.

It will be a place of interpretation acknowledgement and celebration of our Aboriginal culture;

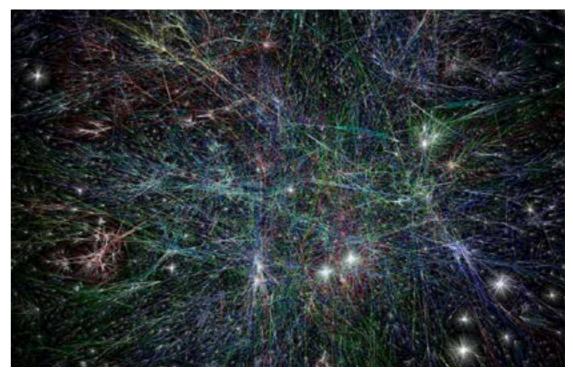
A place to acknowledge and recognise our colonial, industrial, transport and communications heritage;

A destination within the precinct for technological innovation collaboration and learning.



















Design Review Panel - 2020 Recommendations summary

<u>Heritage</u>

- Retain Parcels Post's civic presence and position as strong element in urban context
- Additions proposed should enhance its connection and relationship to the Western Gateway tower cluster
- Additions have significant setback from the Parcels
 Post to enable the 'heritage ensemble' of buildings
 at the George, Pitt, Quay St and Broadway
 intersections, including Marcus Clark, to read
 with distinction, with the new towers forming a
 backdrop
- Have elevational articulation from Parcels Post restored roof and underside of tower (ie setback vertical separation zone)

Public Domain

- Relationship with Henry Deane Plaza. The final location of the underpass is critical to resolving the relationship of the new core within the Plaza and coordinating with predicted and proposed pedestrian flows
- Interface of this link and extension to Parcels Post heritage fabric requires care and heritage review
- The interface of the 'Parcels Post' northern frontage with the proposed Central Plaza has sigificant barrier due to existing access road
- The interface of the 'Parcel's Post' northern frontage should be considered for future connection and integration with Sydney's future Third Square

Building height and envelope (based on F1 chamfered envelope)

- Offset floor plate to free up the building corners and preserve the civic prominence of the north western corner
- Partial extension of this envelope into Henry Deane
 Plaza on the South
- Slender envelope recommended as contrast to other large floor plates at Western Gateway
- Height defined by CSPS solar plane
- Maximum 1300sqm GBA floorplate. This achieves the desired slender envelope, and allows for a >1000sqm NLA floorplate which is the minimum for an A Grade PCA building with Premium Grade features. This allows for potential voids, atriums and interconnecting floorplates for potential innovation precinct tenants
- Total GFA to 41,000sqm (within the tower and Parcels Post excluding Henry Deane Plaza)
- The chamfered setback from the North Western corner to retain primacy of Parcels Post and for mitigating microclimatic conditions. Setback defined by a set from the North East and South Western corner of Parcels Post
- A 5-10m minimum setback on the Northern and Western sides. This setback should be calculated from the average of the substantive façade face, excluding projections and cornices
- The principal support for the new tower is to the South of the Parcels Post with secondary support to the east
- A clear vertical articulation is maintained between the restored Parcels Post roof (top of ridge) and the bulk of the tower envelope - (minimum 12.6m)
- 12m minimum setback from the Atlassian reference scheme, for the full height of both buildings
- The tower floor plate extension into Henry Deane Plaza should not exceed 16m. Potentially half of this depth is envisaged for the tower core, with the remaining space is clear for lobby / circulation

Microclimate

- Several massing options were presented, with the Option F1 envelope proposed for the reference scheme
- Comfort conditions on the ground plane at Lee Street and Henry Deane Plaza are expected to range from 'strolling to slightly above walking criteria', however there are public safety concerns due to wind flow and speed to the north-east, west and south. Of particular concern are dangerous wind conditions caused by the overall precinct development to the south.
- Inclusion of adequate facade articulation in the detailed design and future urban landscape in Lee Street will be required to create safe conditions and acceptable comfort levels for the desired uses, including outdoor seating / stationary pursuits
- Wind mitigation measures through building envelope and surrounding public realm design is a key design criteria.
- Maintain minimum separation between envelopes open to the sky
- A wind tunnel analysis has been completed including the current proposals for adjacent sites.
 See separate wind report

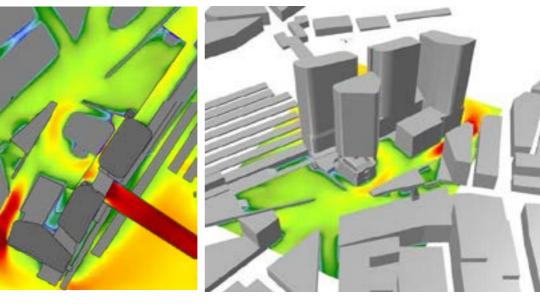
A pedestrian wind assessment of the Proposed Adina Central development was conducted by RWDI to assess the pedestrian wind environment in and around the development site.

There were a series of massing concept studies reviewed.

The reference scheme massing F1 was found to be a more favourable built form in terms of ground level wind conditions. While there are some noted areas of increased wind conditions, these are expected to be able to be addressed through horizontal built form articulation and awning elements as part of the Detailed Design process. The wind condiitions can be made to achieve the City of Sydney's wind comfort criterior (RWDI).

Refer to the separate Wind Assessment report completed by RWDI for further details

Design competition winning scheme massing shown for Block A Reference scheme massing for Block B



/ North West Aerial View

/ Wind Tunnel Test

/ Massing concept studies reviewed



/ Option D1A









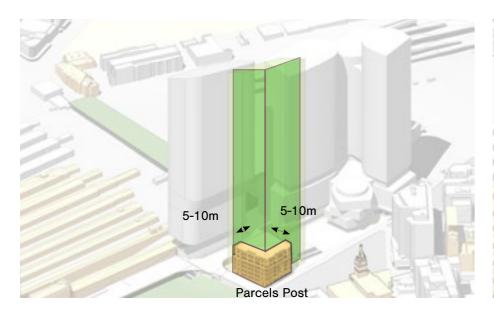




Proposed Planning Framework

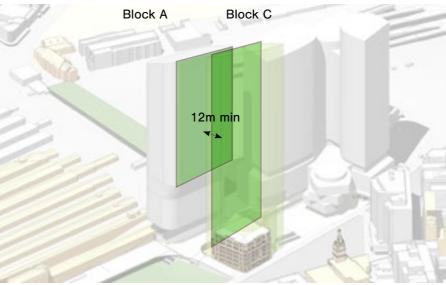
Proposed Envelope Principles

Design competition winning scheme massing shown for Block A and B



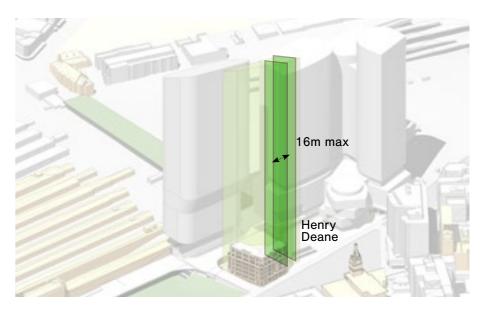
Primacy of Parcels Post

5-10m minimum setbacks on North and Western sides



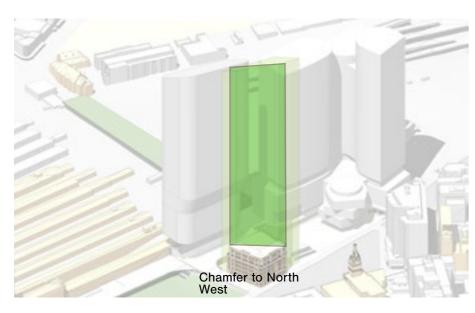
Tower separation

12m minimum separation on the Eastern side towards Block A (Atlassian)



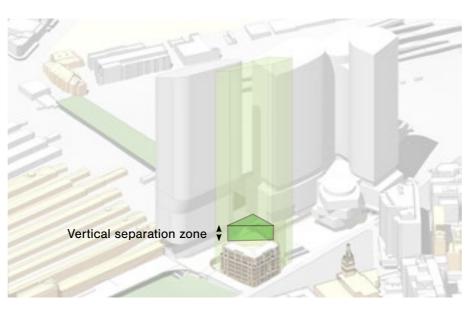
Henry Deane Plaza

Maximium extension of building footprint into Henry Deane Plaza by 16m.



Microclimatic

Following wind and heritage analysis, a chamfer setback is required on the North Western corner to ameliorate windy conditions. This chamfer is set by a line from the NE and SW corner of Parcels Post



Vertical separation zone

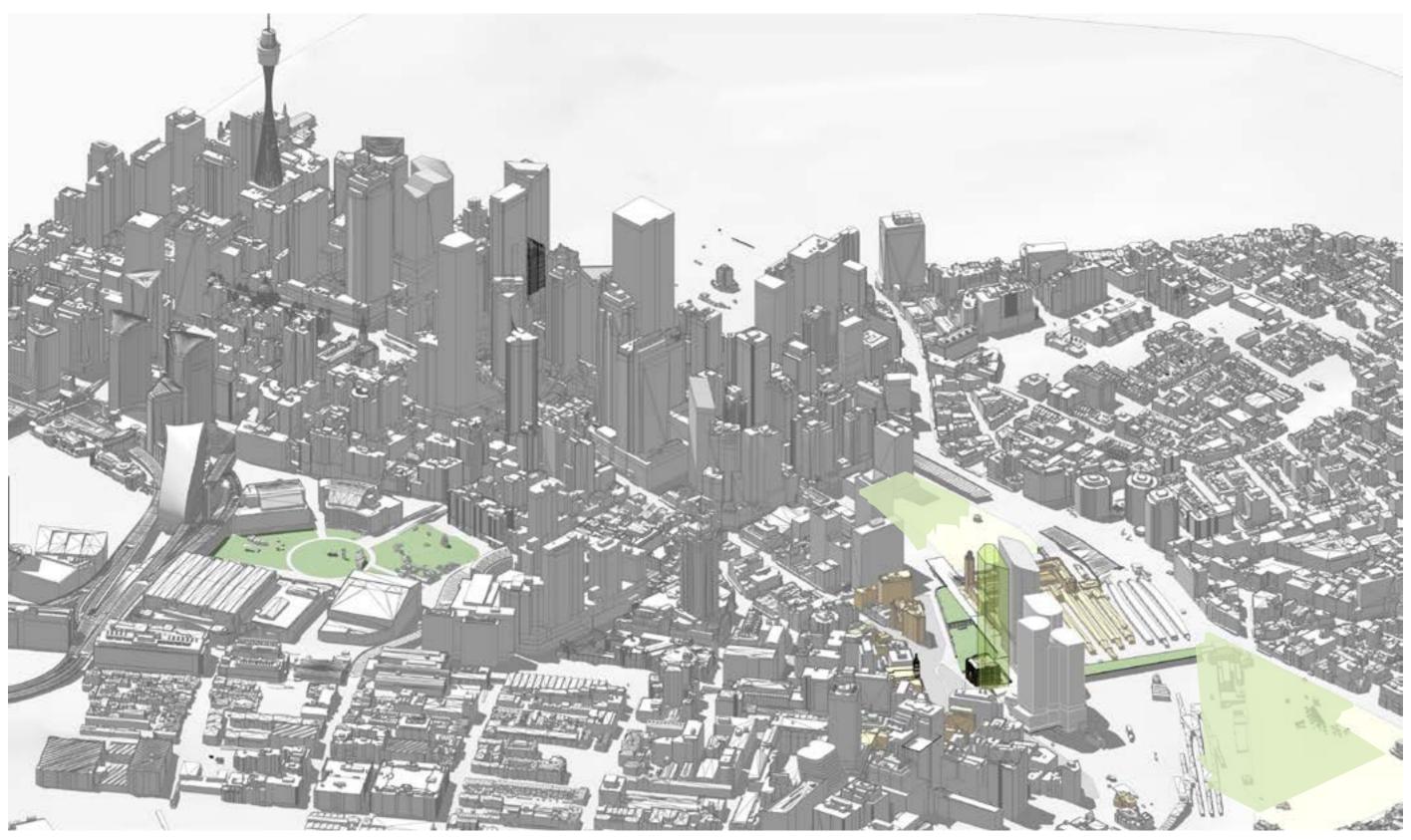
A vertical separation zone (height of 3 commercial floors, minimum 12.6m) between the new tower element and the ridgeline of the restored roof of Parcels Post.

This zone is to be clearly articulated such that it provides clear visual break between the tower and heritage building below. Structural encroachment in this zone is to be minimised



Solar Plane

The proposed Tower Envelope is extended in the site area above the other constraints to the maximum height defined by Prince Alfred Park Sun Access Plane.



Design competition winning scheme massing shown for Block A and B

Proposed Envelope

Proposed Envelope

Tower Height: 211m max(defined by SAP)

Tower Setbacks

Northern Facade 5-10m min
Western Facade 5-10m min

Atlassian 12m min

(Design Competition winning scheme)

Extension into Henry Deane 16m max

Maximum GBA floorplate 1300sqm

The proposed built form has been developed consistent with the Central Sydney Planning Strategy (CSPS), State Design Review recommendations and the supporting urban design and technical studies including heritage and wind analysis for the additional massing proposed.

The proposed envelope preserves the 'Parcels Post' prominence fronting Sydney's Third Square, and amongst the tower cluster at the Western Gateway Precinct.

This is expressed by the 5-10m minimum setback from the Parcels Post on the Northern and Western sides calculated as an average of the substantive facade face excluding projections and cornices.

The difference between the maximum floorplate and prescribed envelope provide flexibility for design excellence during a design competition. This provides greater opportunity for architectural articulation,

facade depth, external sunshading and design excellence.

On the North Western frontage, a chamfer is identified in the envelope assisting microclimatic conditions.

12m minimum Tower separation (from approved Atlassian Design Competition winning scheme scheme) is identified to the East. No building elements are to intrude on this setback.

16m maximum extension of the tower floorplate is proposed Southwards into Henry Deane Plaza - from the existing building footprint.

The resulting setbacks facilitates the ability to achieve a slender tower form and an area sufficient for the delivery of a Global Office Tower and Property Council of Australia (PCA) Premium Grade floor plate, as well as contemporary hotel floorplates. It also preserves the amenity outcomes for the site, providing a considered relationship to the neighbouring towers (both existing and proposed).

CSPS consistency

Consistent with CSPS overarching objectives to incentivise employment generating floor space.

The height is within the CSPS solar plane achieving no additional overshadowing to Prince Alfred Park (all year 10am-2pm.)

Urban Design Considerations

The 'Parcels Post' street wall definition is achieved through the Lee Street and Ambulance Avenue setbacks and can be further developed through the design

excellence process.

Appropriate building separation is maintained.

Contribution to the City

More efficient use of city space while still being CSPS compliant.

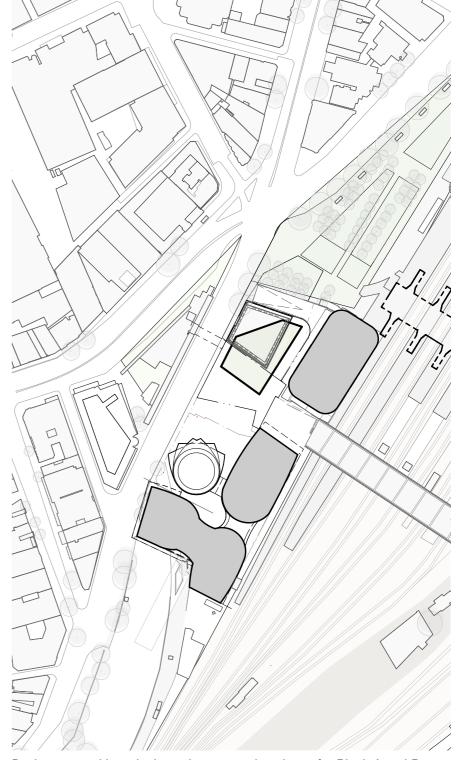
The resulting floor plate provides opportunities to meet the needs of contemporary business and future workplaces.

Opportunity for greater vertical connectivity both between floors and with the public domain through voids and atria.

The opportunity to achieve 1,000m2 NLA floor plates to meet contemporary workplace requirements.

Opportunity for increased floor/floor heights to allow increased natural light penetration.

Regular and high value hotel and commercial floor plate in terms of size and depth, also regular and efficient tower form is able to be achieved within the proposed envelope.

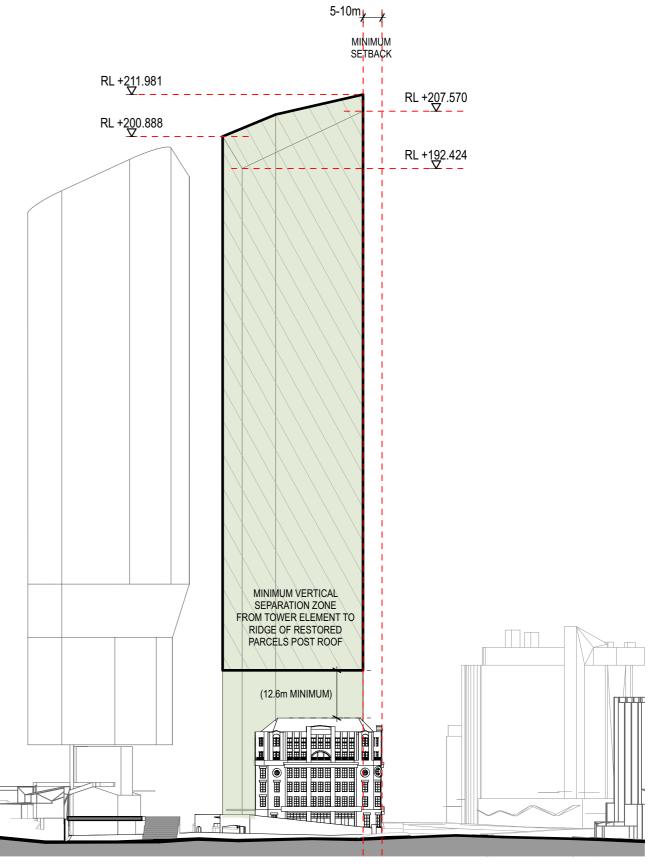


Design competition winning scheme massing shown for Block A and B

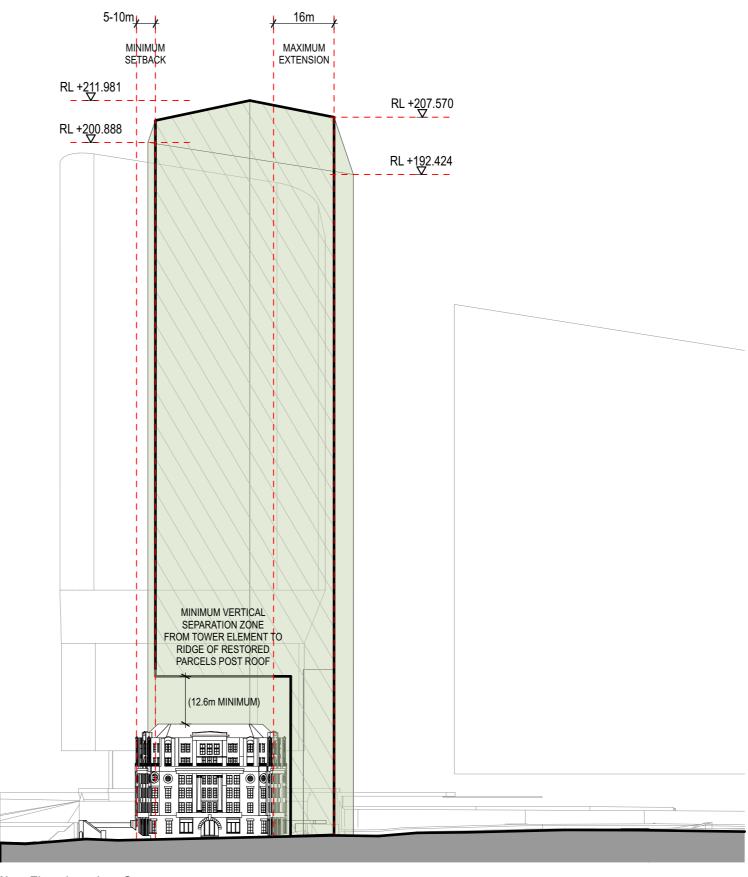


Design competition winning scheme massing shown for Block A reference scheme massing for Block B $\,$





North Elevation - Ambulance Avenue Design competition winning scheme massing shown for Block A





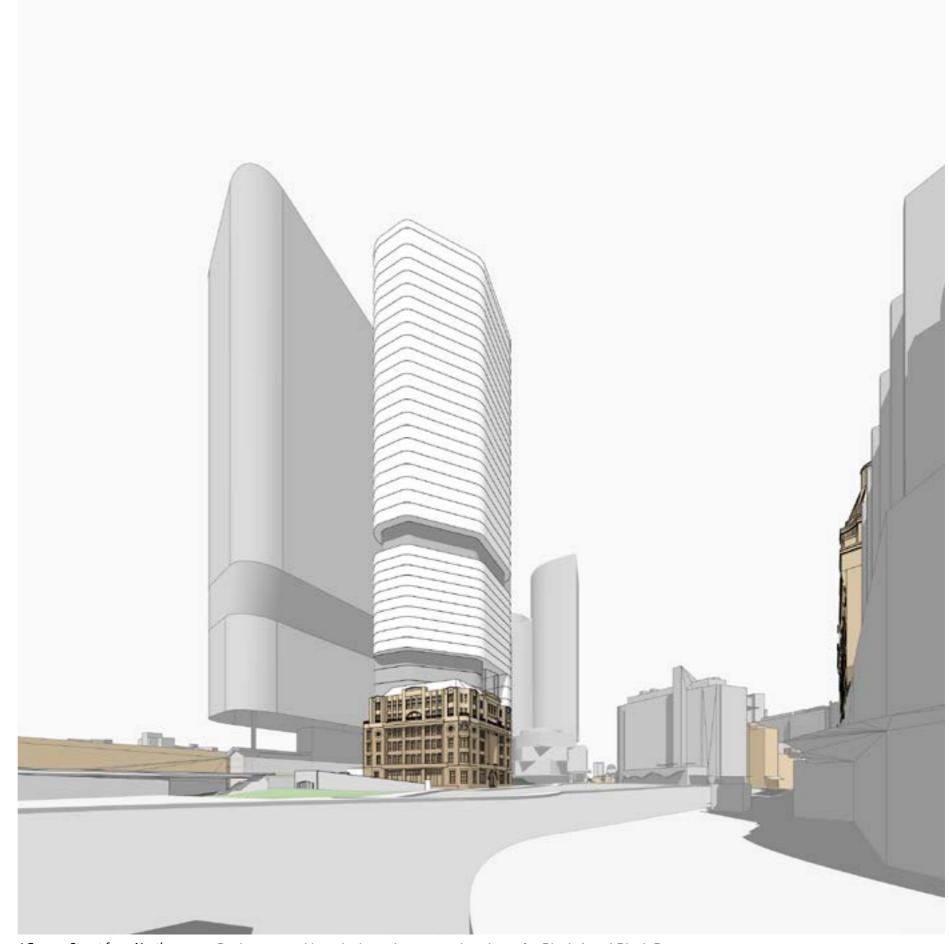
West Elevation - Lee Street
Design competition winning scheme massing shown for Block A

Reference Scheme

Tower Form

The reference scheme has been included in the Urban Design Report as a indicative application of the setbacks and height plane defined by the proposed envelope.

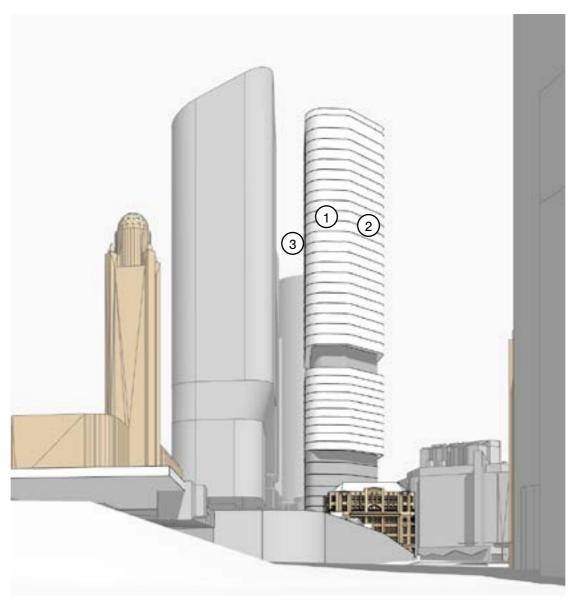
- A 5-10m minimum setback is to be achieved from the building line
 of Parcels Post (North and West), to ensure that the prominence
 of the former Parcels Post building is maintained, calculated from
 the average of substantive facade face excluding projections and
 cornices.
- A chamfered setback is to be achieved at the north western corner
 of the tower to enhance the prominence of the former Parcels Post
 building as viewed from the north west. (This setback aligns with a
 diagonal set from NE corner to the SW corner of the heritage building)
- 3. 12m minimum setback from Block A
- 4. 16m maximum extension south of Parcels Post existing building footprint
- 5. Core element on Eastern side, interfacing and replacing portion of Parcels Post elevation
- 6. Maximum 1300sqm GBA floorplate

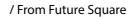


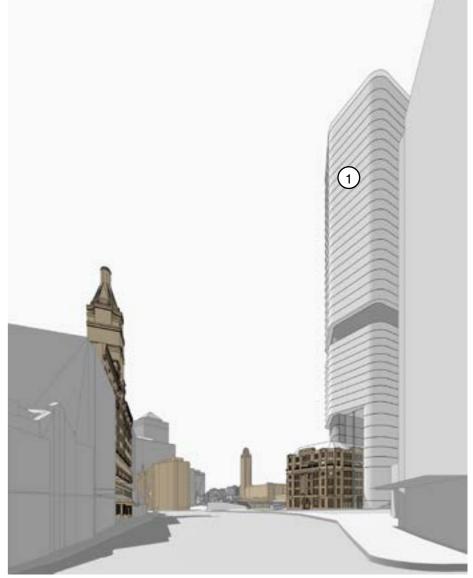
/ George Street from North

Design competition winning scheme massing shown for Block A and Block B

- A 5-10m minimum setback is to be achieved from the building line of Parcels Post (North and West), to ensure that the prominence of the former Parcels Post building is maintained, calculated from the average of substantive facade face excluding projections and cornices.
- 2. A chamfered setback is to be achieved at the north western corner of the tower to enhance the prominence of the former Parcels Post building as viewed from the north west. (This setback aligns with a diagonal set from NE corner to the SW corner of the heritage building)
- 3. 12m minimum setback from Block A
- 4. 16m maximum extension south of Parcels Post existing building footprint
- 5. Core element on Eastern side, interfacing and replacing portion of Parcels Post elevation
- 6. Maximum 1300sqm GBA floorplate



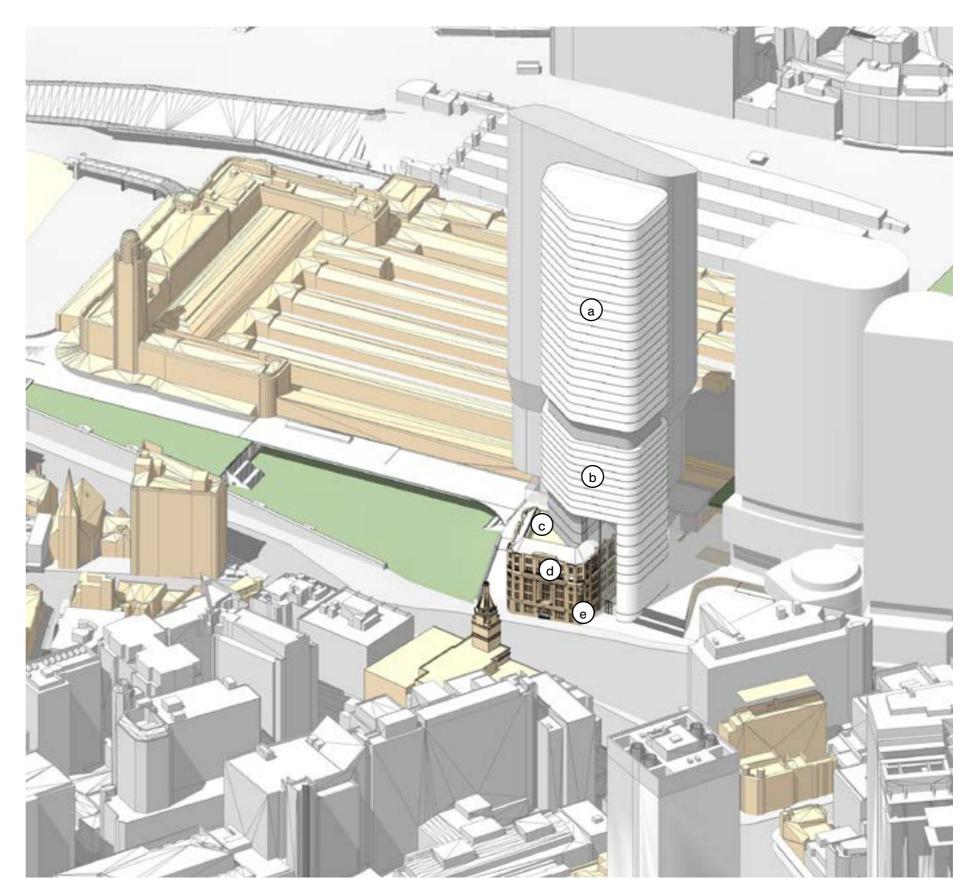




/ From Broadway South

Vision / Uses

The Vision for the project is to create a vibrant and exciting place that unites a world-class transport interchange with innovative businesses and public spaces. It will connect with the city at its boundaries, celebrates its heritage and become a centre for jobs of the future and economic growth. The Parcels Post will be targeted to be both a social and commercial hub for Sydney's most productive and energetic neighbourhood.



- a. Commercial
- b. Hotel
- c. Rooftop bar
- d. Co working
- e. Retail

Design competition winning scheme massing shown for Block A and Block B

(a) Commercial

High quality, highly flexible office spaces supporting emerging businesses and startups within Sydney's new Technology zone.













b Hotel

Providing accommodation, conference rooms for meetings, function halls for gatherings and catering for the innovation precinct













© Rooftop bar

At the juxtaposition between heritage fabric and modern additions will be a new hospitality venue overlooking Sydney's 3rd square

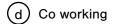












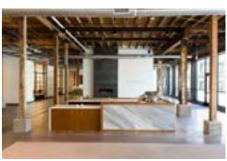
Adaptive reusing the Parcels Post building as contemporary commercial space













High quality retail precinct that caters to building tenants, precinct workers, daily commuters and visitors.











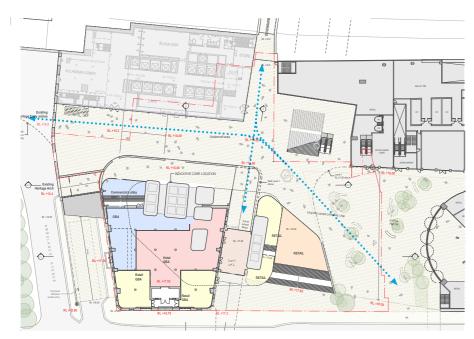




Groundplane

Block C sits at the nexus of predominant north south and east west pedestrian connections. The groundplane will be developed to integrate heritage and new built forms, various entries and lobbies to multi use functions and resolve connections and address to Henry Deane Plaza and the future Sydney Third Square.

Design competition winning for Block A Design competition winning scheme shown for Block B



Lower Ground Level RL 16

Circulation

12m setback to Atlassian preserves North South pedestrian connection on Level RL 16 and RL 21

East West connection at RL 16 ramped down to Devonshire tunnel, and ramped to Lee street



Upper Ground Level RL 21

East West connection at RL 21 connecting North South Spine to Lee Street, Block A and B lobbies and Future Over Station Development



Upper Ground Level RL 21

Open Space

12m setback to east preserves open space between Block A and C

16m maximum building footprint to Henry Deane Plaza preserves open space at Henry Deane and associated pedestrian connections







Entries

Hotel entry from Lee Street via internal steps, and ramped entry from glazed atrium to the south of 'Parcels Post'

Commercial entry from Railway Colonnade ramp at RL 17.5, from Ambulance avenue at RL 15.4 via heritage tunnel entry

Tower Cores

Hotel core guest lifts on south side of Parcels Post building via glazed atrium. Back of House core positioned centrally in plan.

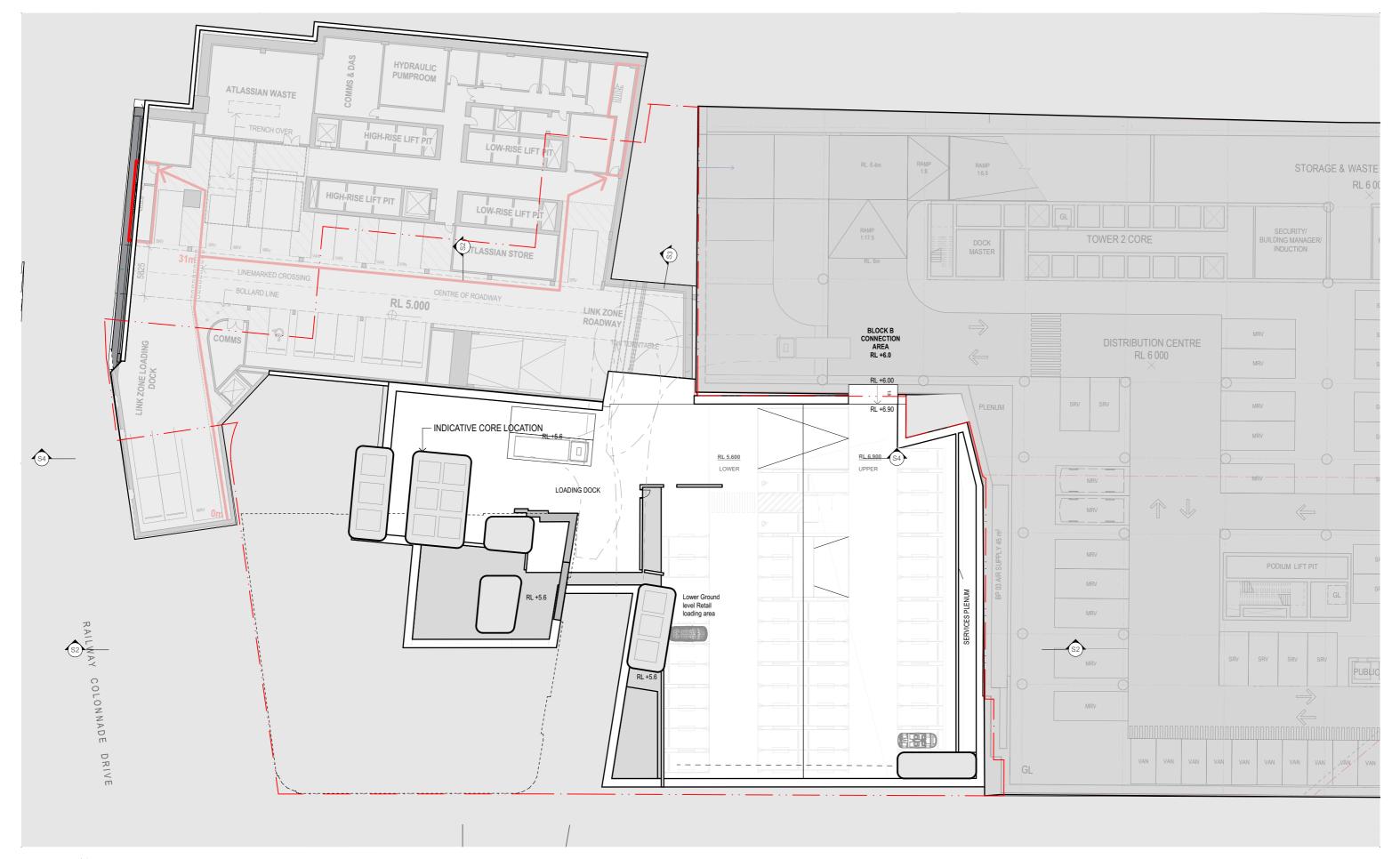
Commercial core to East of Parcels Post. The eastern elevation of Parcels Post is recognised as non original. The core position involves replacement of of this facade

Retail and lobbies

Lower ground retail accessed from Devonshire tunnel

The Ground level is envisaged as a rich blend of connected public facing functions that are permeable across the floor and accessed from Lee Street, from Ambulance Avenue and from the North South Spine. This area will include a mix of retail food and beverage and lobbies.

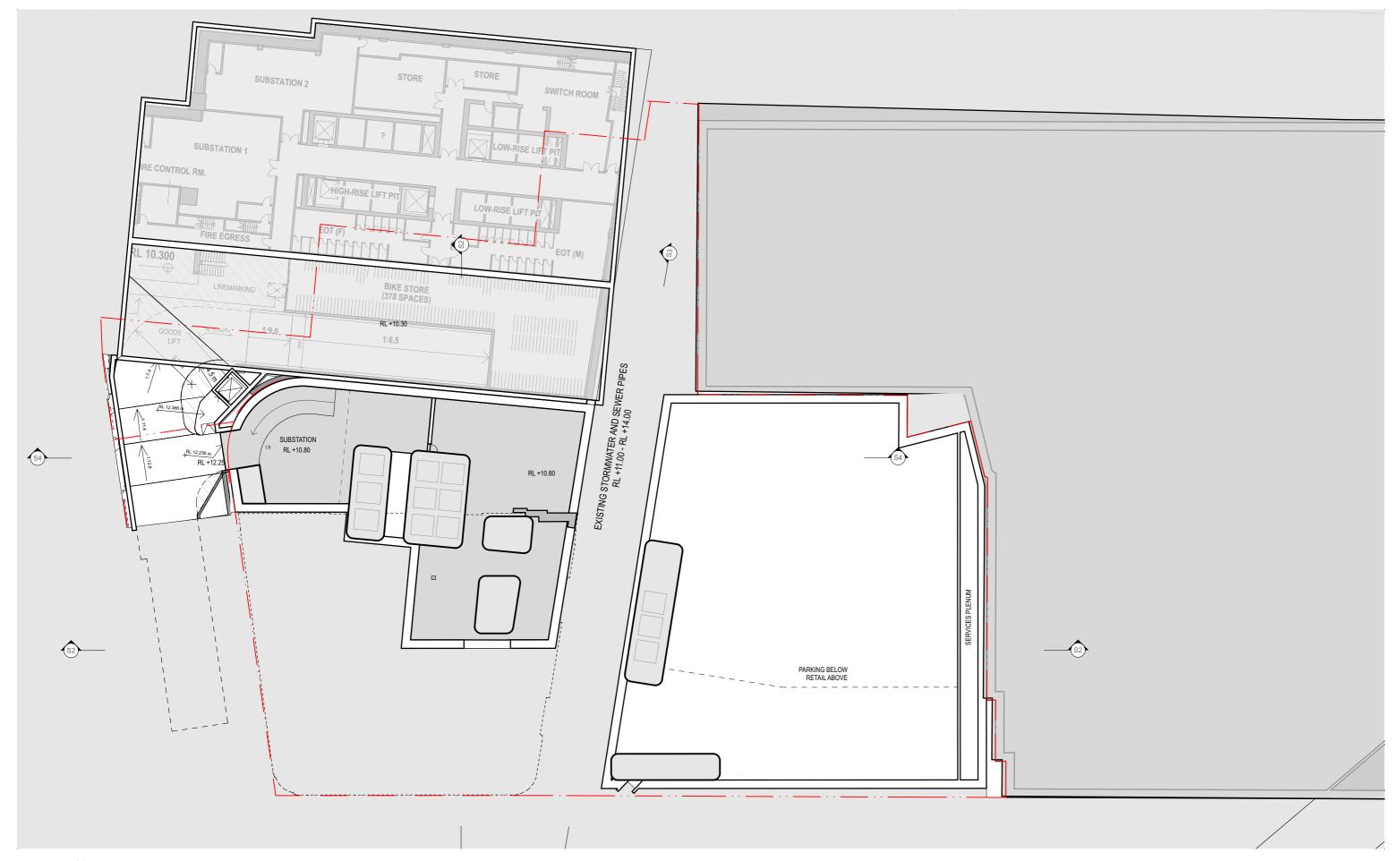
Level 1 retail addressing RL 21 groundplane and Henry Deane Plaza



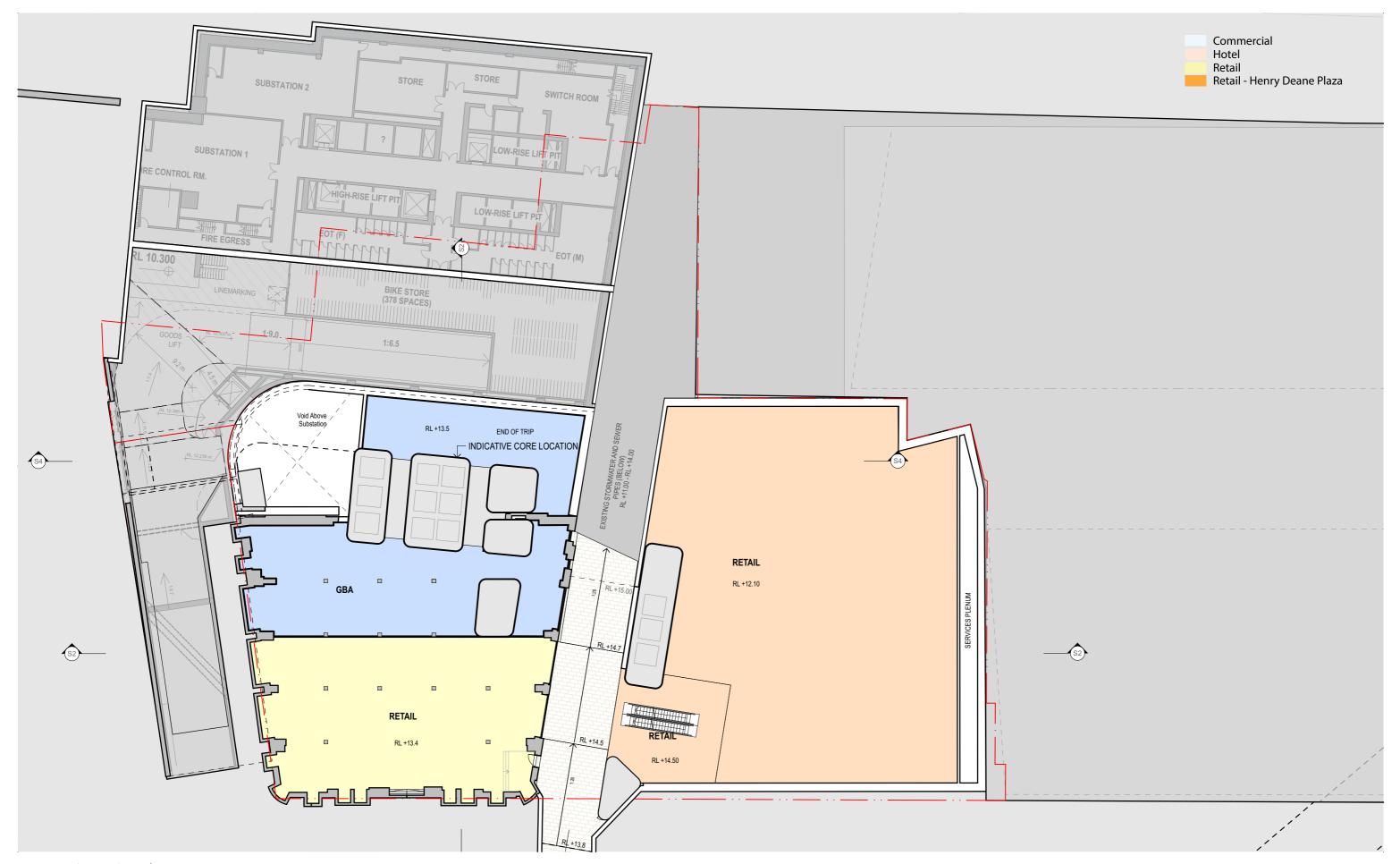
/ Basement 3



/ Basement 2



/ Basement 1

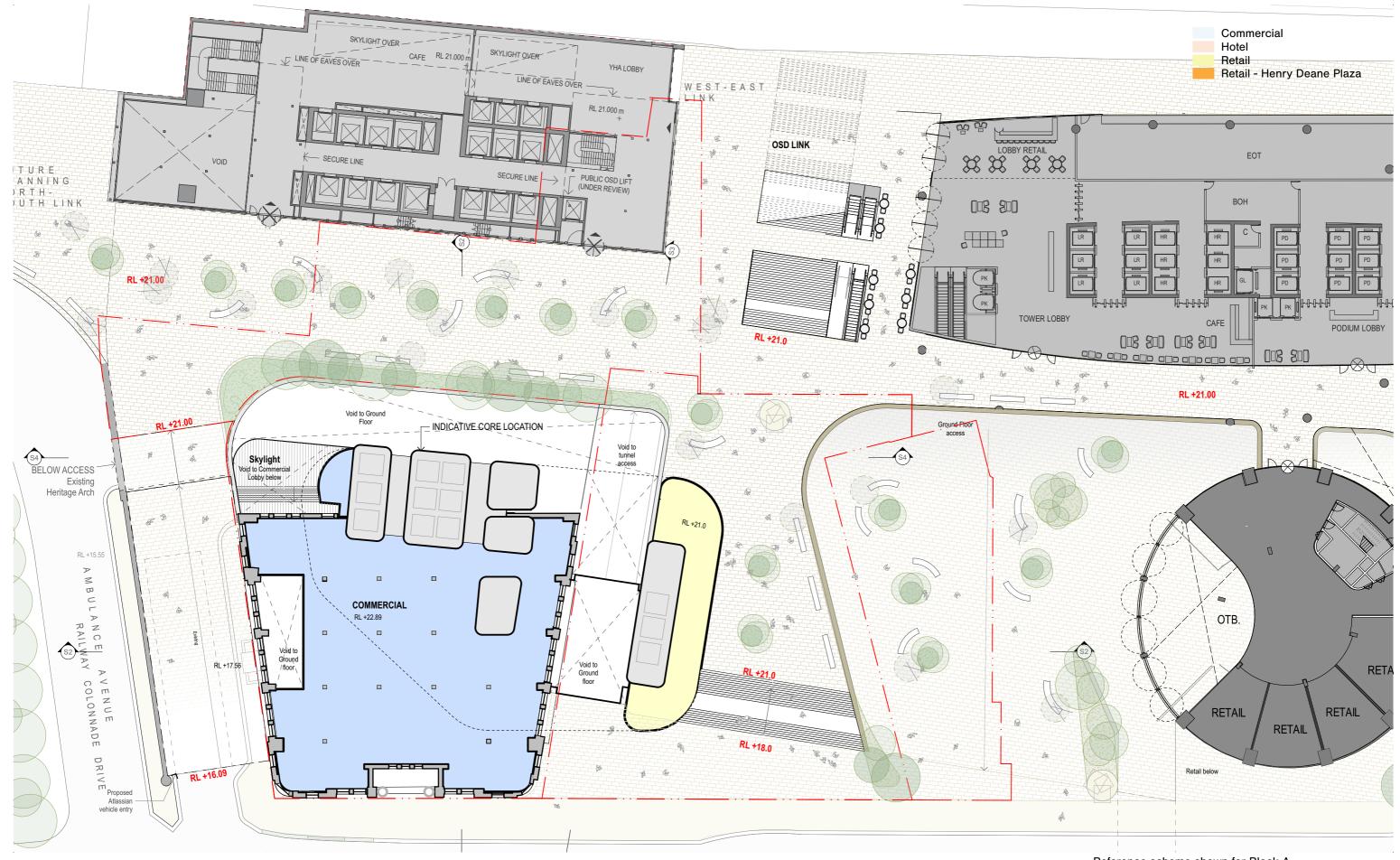


/ Lower Ground



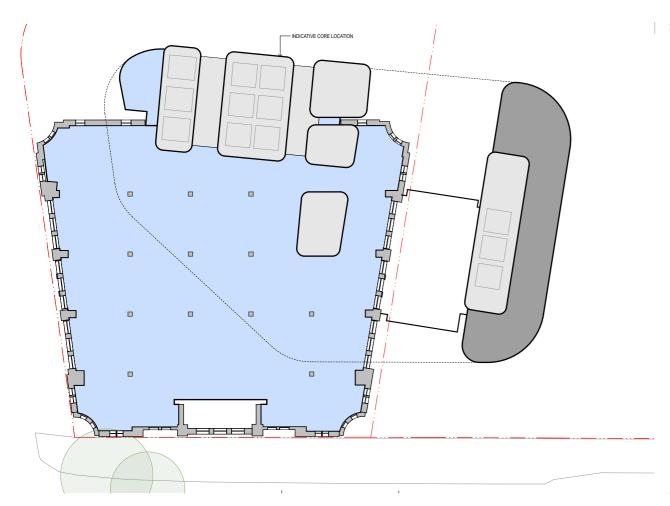
/ Ground Level

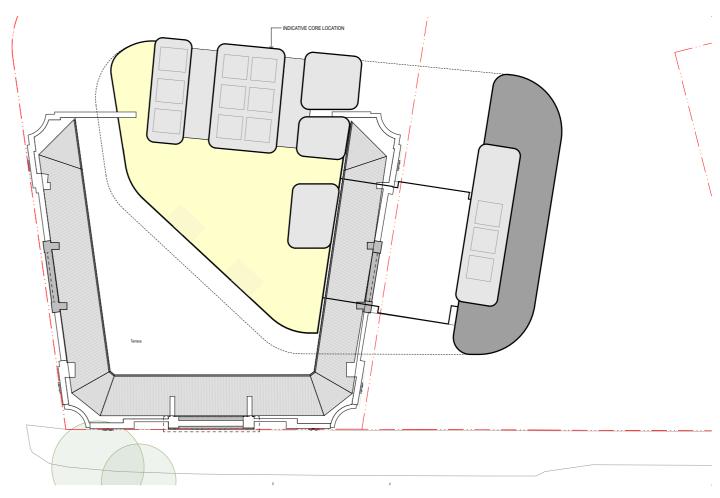
Reference scheme shown for Block A Design competition winning scheme shown for Block B



/ Level 1

Reference scheme shown for Block A Design competition winning scheme shown for Block B



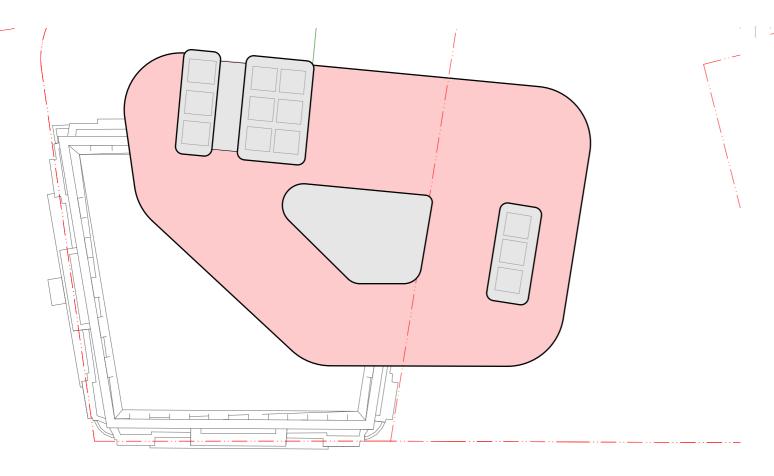


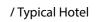
/ Level 5

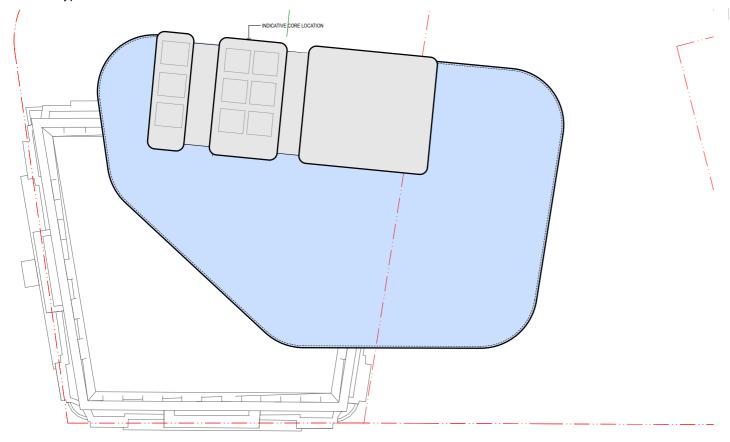
/ Level 7

Commercial
Hotel
Retail

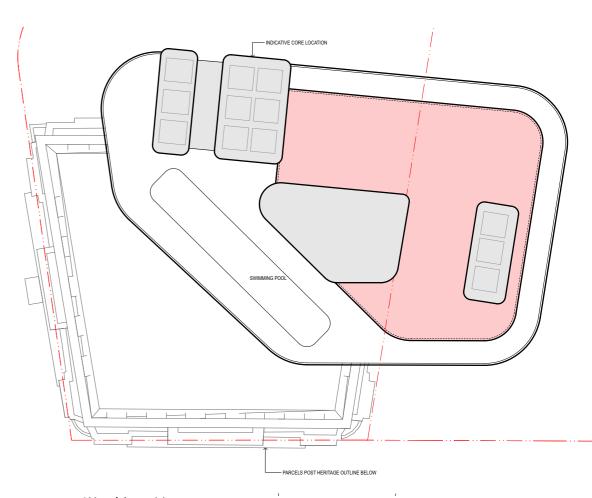
Retail
Retail - Henry Deane Plaza





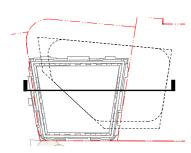


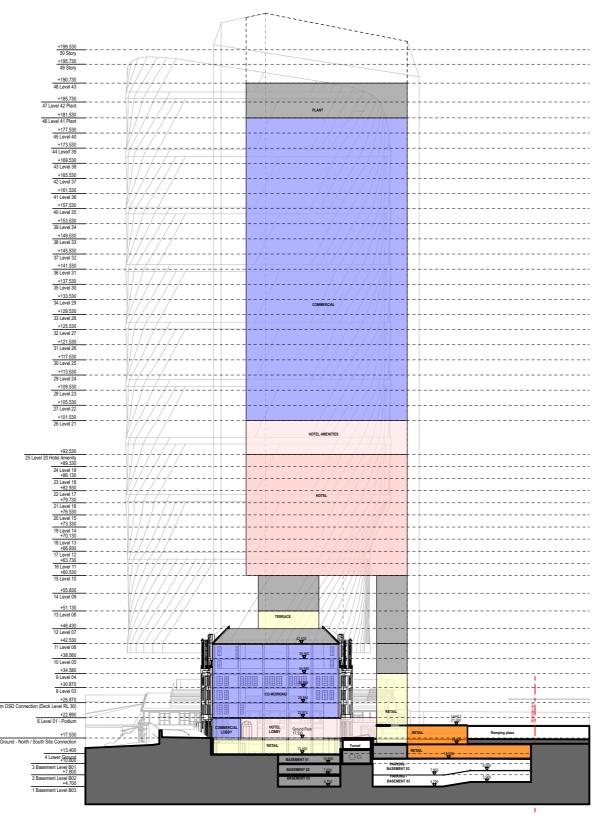
/ Typical Commercial Floor



/ Hotel Amenities

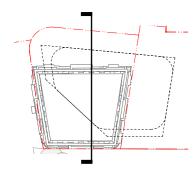


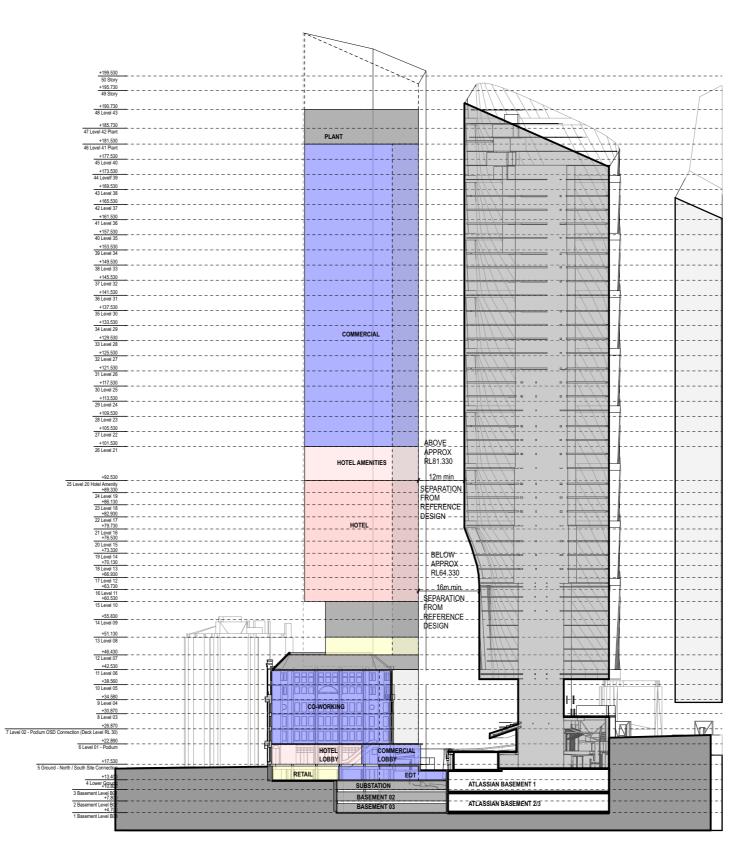




/ Section - North South Design competition scheme massing shown for Block A







/ Section East West 01

Design competition scheme massing shown for Block A

Indicative Reference Scheme Area Schedule

GFA Plaza The following is an indicative area schedule for the reference scheme COMMERCIAL It shows 41000sqm GFA in the new tower envelope and Parcels Post level 43 190.73 level 42 185.73 5.00 level 41 181.53 4.20 172.33 1300 1300 Commercial level 40 177.53 4.00 168.13 1300 1300 1133 1000 173.53 1000 level 39 4.00 164.13 1300 1300 1133 1000 level 38 169.53 4.00 160 13 1300 1300 1133 165.53 1000 161.53 level 36 4.00 1300 1300 1133 1000 152.13 level 35 157.53 4.00 148.13 1300 1300 1133 1000 along with 2000sqm GFA on the Henry Deane Plaza portion 144.13 1133 level 34 153.53 4.00 1300 1300 1000 level 33 149.53 4.00 140 13 1300 1300 1133 1000 Retail - Henry Deane Plaza level 32 145.53 4.00 136.13 1300 1133 1000 1000 level 31 141.53 4.00 139 13 1300 1300 1133 137.53 1300 1133 1000 133.53 4.00 1300 1133 1000 level 29 124.13 1300 level 28 129.53 4.00 120.13 1300 1300 1133 1000 level 27 125.53 4.00 116.13 1300 1133 1000 level 26 121.53 4.00 119 13 1300 1300 1133 1000 level 25 117.53 4.00 1300 1133 1000 level 24 113.53 4.00 104.13 1300 1300 1133 1000 level 23 109.53 4.00 100.13 1300 1300 1133 1000 level 22 105.53 4.00 1300 1300 1133 1000 96.13 HOTEL level 21 101.53 4.00 92.13 1100 1100 1133 1000 level 20 92.53 9.00 88.13 1300 level 19 89.33 3.20 79.13 1300 1060 965 714 level 18 86.13 3.20 75.93 1300 1300 22 965 714 level 17 82.93 3.20 72.73 1300 1300 22 965 714 79.73 3.20 69.53 1300 1300 714 level 15 76.53 3.20 66.33 1300 1300 22 965 714 level 14 73.33 3.20 63.13 1300 1300 22 965 714 714 714 level 12 22 965 66.93 3 20 5673 1300 1300 63.73 3.20 53.53 1300 1300 22 965 714 60.53 level 10 3.20 50.33 1300 1300 Level 9 55.83 4.70 47.13 517 517 level 8 51.13 4.70 42.43 517 517 level 7 4643 470 3773 517 level 6 HERITAGE 964 1060 level 5 38.56 3.97 29.13 1225 1225 34.58 3.98 25.16 1225 1225 1060 964 level 3 30.87 3.71 21.18 1225 1225 1060 964 level 2 26.87 4.00 17.47 1225 1225 1060 964 22.89 3.98 13.47 1225 1225 1000 740 level 1 17.53 430 400 600 455 200 300 ground 5.36 9.49 1928 1928 3650 3650 670 640 360 lower ground 2,000 subtotal 55,171 54.931 28,000 24,896 12.000 8,891 1,000 BASEMENT 2494 10.80 2.60 8.20 basement 2 2.60 2878 2878 8,250 subtotal 63,421 54,931

Hotel

Retail

Overshadowing and Solar Access

Solar Study

The proposed envelope has been developed to prevent additional overshadowing to the public domain in particular the relevant public places as outlined in the clause 6.19 of the Sydney Local Environmental Plan (LEP) 2012.

The public places and times identified in the LEP within the sites proximity that could potentially be impacted upon include;

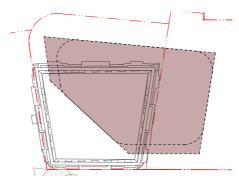
Prince Alfred Park 10:00-14:00

The following shadow impact analysis has been undertaken on Summer Solstice Dec 21, Autumn Equinox Mar 21, Winter Solstice Jun 21 and Spring Equinox Sep 21 during the specified times to demonstrate the achievement of this clause.

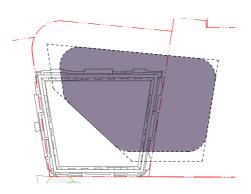
The shadow analysis has been tested relative to existing shadows:

- Existing shadows showing adjacent Block A and B reference schemes
- The proposed Block C planning envelope (5-10m minimum setbacks on North and Western sides and 12m minimum setback from Block A)
- 3. The Block C indicative reference scheme

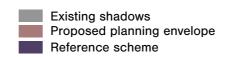
The results illustrate that all the above conditions will not result in additional overshadowing of Prince Alfred Park



The proposed planning envelope



The indicative reference scheme





Design competition scheme massing shown for Block A reference scheme massing shown for Block B

Summer Solstice - December 21

Existing shadows
Proposed planning envelope
Reference scheme



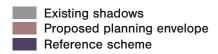


9am 10am





11am 12pm







2pm

1pm



3pm

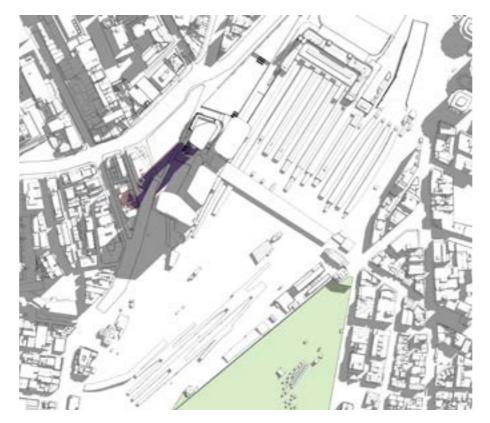
Autumn Equinox - March 21

Existing shadows
Proposed planning envelope
Reference scheme





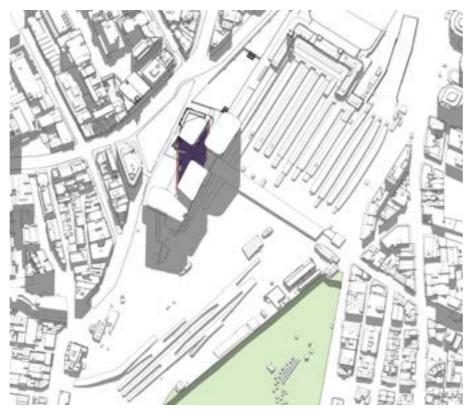
am 10am





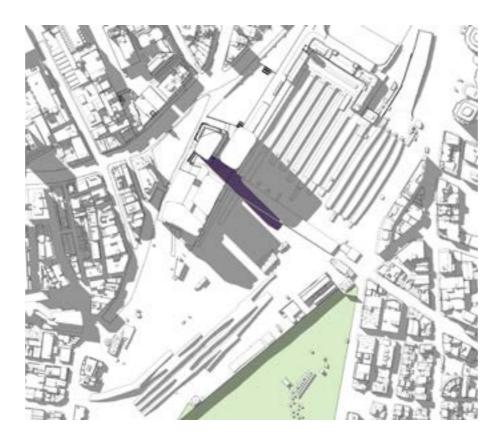
11am 12pm

Existing shadows
Proposed planning envelope
Reference scheme





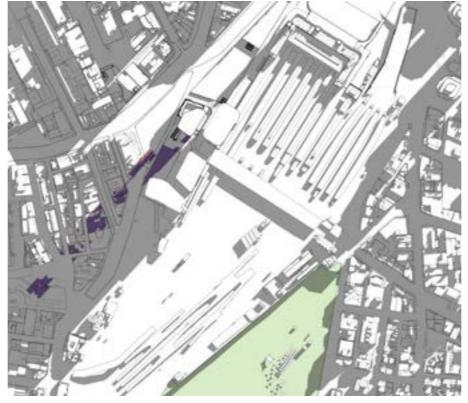
1pm 2pm



3pm

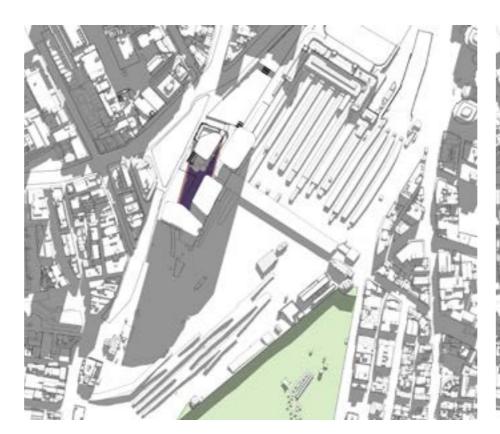
Winter Solstice - June 21

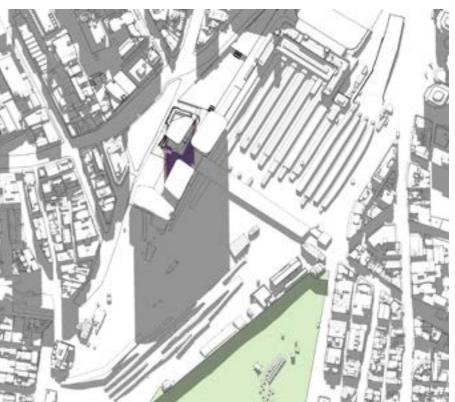
Existing shadows
Proposed planning envelope
Reference scheme





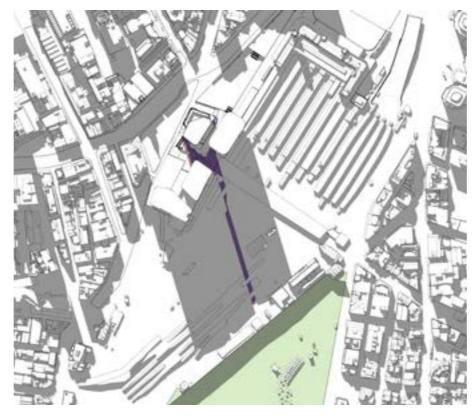
am 10am

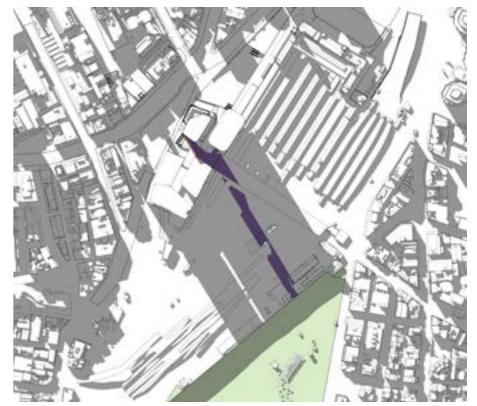




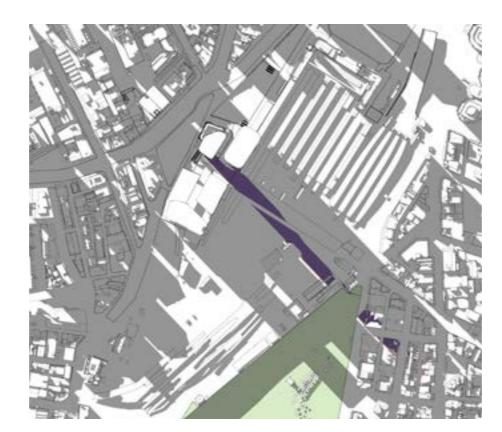
11am 12pm

Existing shadows
Proposed planning envelope
Reference scheme



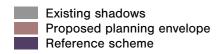


1pm 2pm



3pm

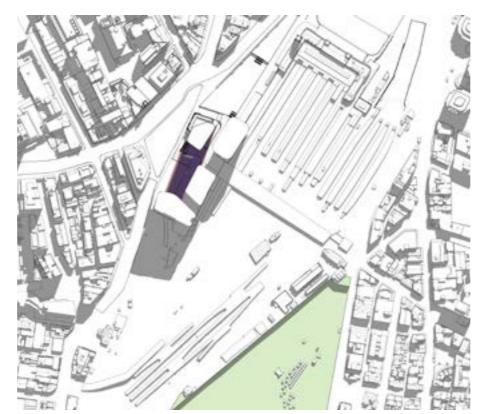
Spring Equinox - September 21

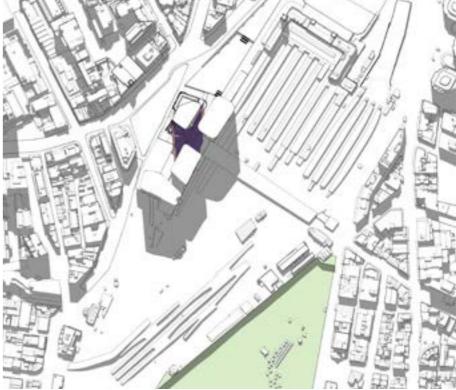






am 10am

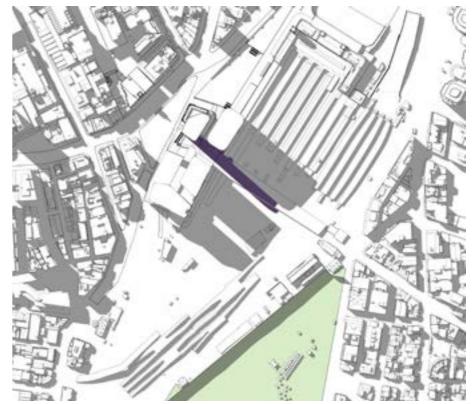




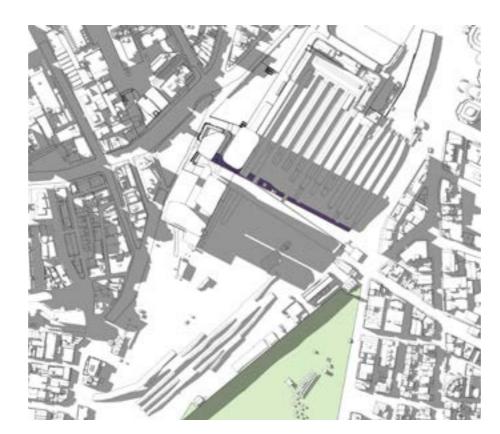
11am 12pm

Existing shadows
Proposed planning envelope
Reference scheme





1pm 2pm



3pm

fjmtstudio

Sydney

Level 5, 70 King Street, Sydney NSW 2000 Australia t+61 2 9251 7077

Melbourne

Level 2, 56 Hardware Lane, Melbourne Vic 3000 Australia

t+61 3 9604 2500

<u>UK</u>

Level 1, 8 St Aldates United Kingdom OX 1 1BS t+44 1865520 420

Find us on Instagram and LinkedIn: @fjmtstudio

www.fjmtstudio.com

Francis-Jones Morehen Thorp Pty Ltd ABN 28 101 197 219
Nominated architect Richard Francis-Jones ARBNSW 5301
Registered architect Richard Francis-Jones
Francis-Jones Morehen Thorp Ltd Company no 7384142 ARB 078103G