

Western Gateway Sub Precinct: Block B

14-30 Lee Street, Haymarket

Urban Design Report

10 October 2019

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Introduction

1.1 Executive Summary

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1.1 Executive Summary

The New South Wales Government and Transport for New South Wales (TfNSW) have established an ambitious vision for Sydney's growing population which will see major transport and urban renewal projects take place in the future. A city of connected strategic centres and sub precincts served by a renewed Sydney Metro will enable an efficient and accessible 30 minute city as part of Greater Sydney Commission's 'Metropolis of Three Cities', and TfNSW's Future Transport 2056 initiative.

A revitalised Central Station Precinct will encompass the proposed rezoning of the Western Gateway, along with the future Over Station Development (OSD). This development will facilitate the expansion of the CBD to the south and form a focal point of an emerging Innovation and Technology Precinct which will provide increased commercial space suited to the needs of the sector. The growth of the Precinct will require proposed new height and density controls, which form part of a State Significant Planning proposal for the Western Gateway.

Dexus Funds Management Limited (Dexus) and Frasers Property Australia (Frasers) propose to redevelop 14-30 Lee Street (Block B) which sits at the intersection of the Western Gateway and other TfNSW assets. Accommodated across two towers and a common podium the proposal will provide adaptable commercial floor plate offerings sized to suit the needs of the Technology and Innovation sector. Critically the project will also deliver a below ground Integrated Distribution Facility (IDF) and associated parking for the wider Precinct. The IDF will enable the OSD site and adjoining proponents to overcome logistical issues in achieving their development potential.

The podium and lower levels of the proposal will provide contiguous office floorspace as well as retail activation to Lee Street, Henry Deane Plaza and the future OSD interface. The podium will be the public face for the development and will be vibrant place for interaction, collaboration and events as envisaged by the Innovation and Technology Precinct vision statement.

The proposal responds to the unique ground plane challenges to deliver the connectivity required and support unified 'whole of precinct' outcomes. A collaborative approach between the proponents will improve the quality of the urban experience and create an attractive and vibrant part of Sydney which delivers the NSW Government's aspirations and objectives for the Precinct as a place where jobs, transport, tech and lifestyle converge.

This report sets out the Urban Design Framework for the redevelopment of Block B of the Western Gateway sub precinct, and contains Strategic and Technical urban design principles which cover four categories : Place, Public Realm, Urban Form and Futures.

The principles require future development to respect the heritage fabric of the setting while also delivering the required connectivity, development and infrastructure for the growing city. Design proposals which adhere to the framework will safeguard for an architectural response capable of achieving design excellence and delivering a clear, legible and activated public realm. The framework encourages sensitivity to place, height and scale, sustainability, heritage views and solar access.

The purpose of this framework is to ensure the principles put forward will meet TfNSW Requirements and guide the design of high quality urban outcomes for the renewal of the Central precinct, the interface with a proposed OSD, and the provide suitable commercial space for the Technology and Innovation precinct. The proposed principles have informed the Indicative Scheme put forward in the accompanying Concept Design Report prepared by Woods Bagot, SOM and Hassell, which aim to demonstrate how the application of the principles can achieve design excellence in the Western Gateway.

1.1.2 14-30 Lee Street (Block B) Project Objectives

The project objectives for Block B are to:

- High tech jobs – Deliver creative workspace that builds the Sydney Innovation and Technology Precinct and will underpin Sydney's enduring global competitiveness.
- Transport connectivity – Redefine the experience of over 20 million pedestrians who walk through Henry Deane Plaza every year with world class public realm and connectivity.
- A revitalised precinct – Transform Central into an exciting place with lively retail and dining options, supporting Sydney's day and night time economy.
- Infrastructure for the future – Enable wider renewal of Central by delivering underground smart building services, waste and utility infrastructure necessary for an integrated and sustainable precinct.

1.2 How to use this document

1.2.1 Structure

The framework has been structured into 3 parts.

Part 1- Introduction

Part 2- Provides an urban context and site analysis. Summarises the existing zoning and key development controls.

Part 3- Urban Design Strategic Principles identifies the urban ideals of future development in Sydney, with particular emphasis on place making as recommended by key strategic documents by the Greater Sydney Commission, TfNSW as well as best practice urban design, with an emphasis on place making and city identity. A set of Urban Design Principles has been formed to guide successful future design outcomes.

The report concludes with Part 3.2 Urban Design Technical Guidelines which explain in diagrammatic form the proposed controls of bulk/ scale, in particular solar height plane controls for the site defined by City of Sydney Council as viewed from critical vantage points.

Note that the lower ground plane ownership plan shown in Section 2.3 of this report is to be used in applying any amendment in LEP controls for Block B.

1.2.2 The team

The Framework has been prepared by a team of consultants whose advice and has informed the information presented.

Client/Owner: Dexus and Frasers Property

Heritage Consultant: GML Heritage

Traffic, Transport, Pedestrian modelling, Contamination, Structural, Services, Wind, Flood and Stormwater: ARUP

Architects: Woods Bagot & Skidmore Owings Merrill (SOM)

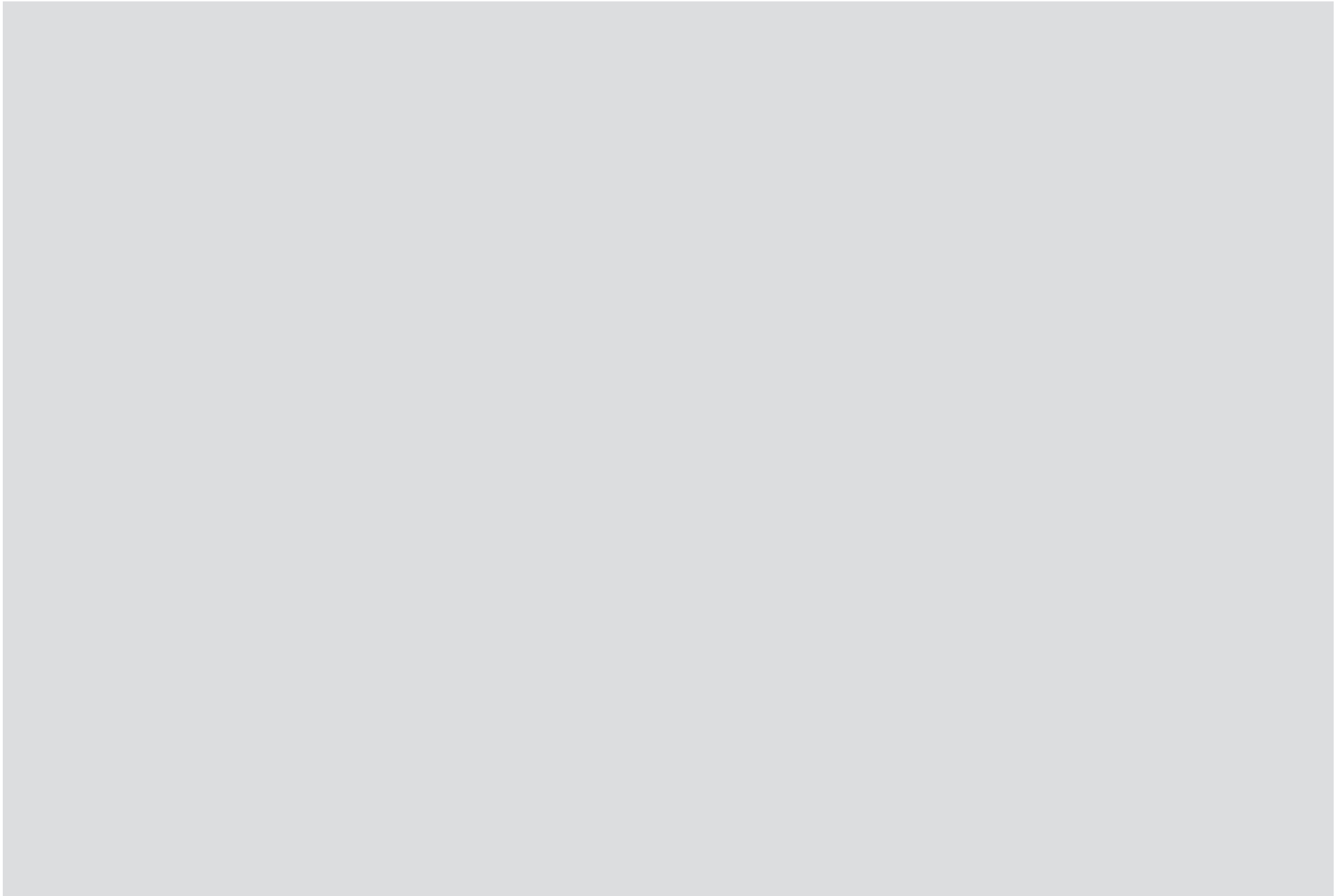
Planning Consultant: MG Planning/ Savills

Surveyor: LTS Lockley

Landscape Architect: Hassell

1.2.3 Acknowledgements

Various organisations have been consulted in preparing this Framework including TfNSW's, Atlassian Pty Ltd as developer of the YHA Site (Block A), Toga Group as owner and developer of the Adina Hotel (Block C) and their consultant teams. Reference material prepared by Bates Smart for Block A and JPW Architects (Block C) has been used to inform the understanding of preliminary massing for the site. Preliminary work by Architectus and Aspect commissioned by TfNSW has also been referenced.



02

Urban Context

2.1 Introduction

2.2 Site location

2.3 Site ownership

2.4 Existing site photos

2.5 Topography

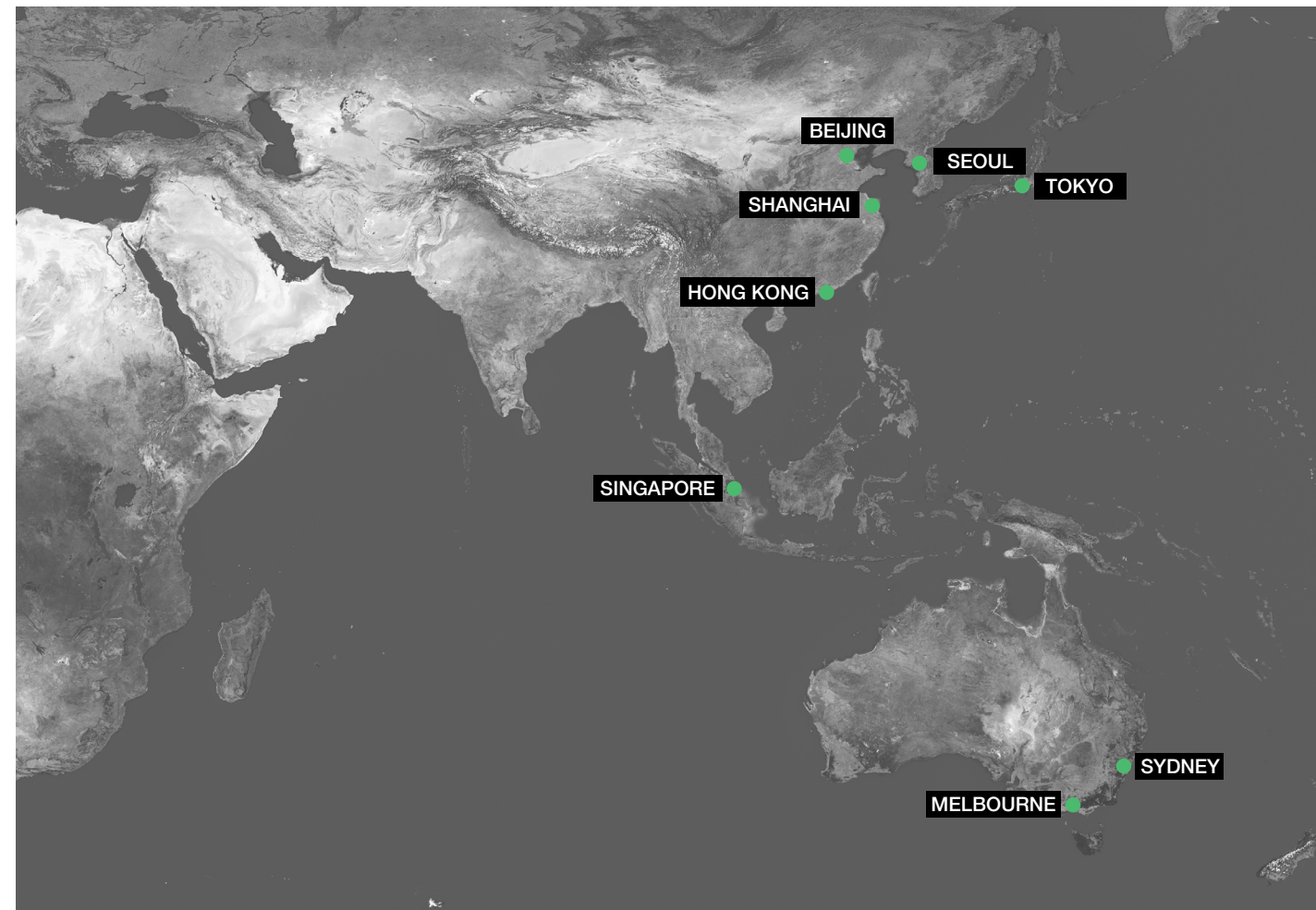
2.6 Site Analysis

2.7 Statutory Controls

2.1 Introduction : Macro to Micro

Macro to Micro: A Global City

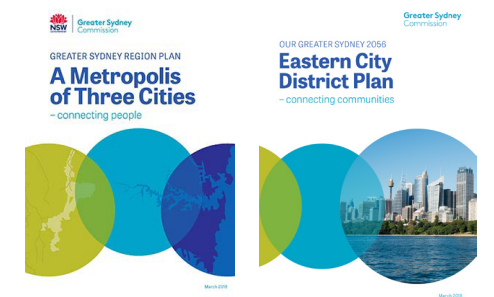
Sydney is Australia's leading global city and is competing with Australian and international cities for employment, investment, students, workers and residents. The vision for Greater Sydney as a metropolis of three cities – the Western Parkland City, the Central River City and the Eastern Harbour City as a 30 minute city – will see the Eastern Harbour City become more innovative and globally competitive, carving out a greater portion of knowledge intensive jobs from the Asia Pacific Region.



Macro to Micro: A Metropolis of Three Cities / The Eastern Harbour City

The Eastern Harbour City has Australia's global gateway and financial capital, the Harbour CBD, as its metropolitan centre. Well-established, well-serviced and highly accessible by its radial rail network, it has half a million jobs and the largest office market in the region.

The Eastern Harbour City will focus on innovation and global competitiveness to underpin its continued growth. It will extend its capabilities with an emerging Innovation Corridor on its western edge comprising universities and other tertiary institutions, a major teaching hospital, international innovation companies and fast growing start-ups.



Macro to Micro: A Network of Precincts

The Sydney CBD is at the centre of a network of strategic centres and distinct precincts, with a vision for growth and connectivity:

Central Business District:

Sydney’s CBD is the largest premium office sub-market in Australia and a location favoured by the financial, legal, property and technology sectors.

Camperdown-Ultimo Health and Education Precinct:

Camperdown-Ultimo is distinctive for work-leading education, health and training institutions which supply research, skills and talent for science-based deep-tech, medicine, business and creative sectors. The precinct contains Royal Prince Alfred Hospital, the University of Sydney, University of Technology Sydney, University of Notre Dame Sydney and TAFE Ultimo.

Central to Eveleigh Precinct:

The Central to Eveleigh Precinct is a CBD precinct -serving existing education, visitor accommodation and creative uses, as well as yet-unknown business models that are seeking a creative point of difference to the traditional office core.



Macro to Micro: Innovation Corridor

An Innovation Corridor is emerging along the western and southern fringes of the Harbour CBD. The Innovation Corridor contains knowledge intensive, creative and start-up industries, which require event spaces and affordable and scalable office spaces.

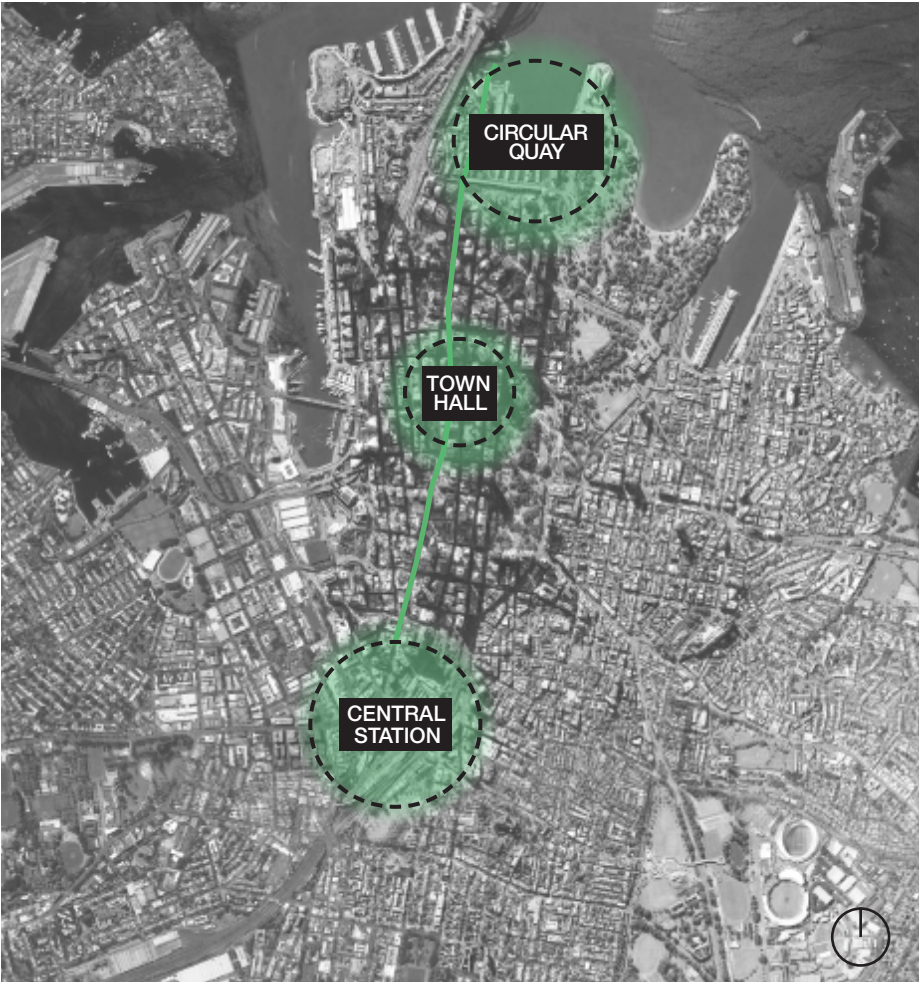
The Corridor will be a place to drive the creation of new technology and commercialisation of new products and services. The development will be based on global best-practice place making principles and focus on the sustainable, accessible and affordable creation of workplaces for the future.



Macro to Micro: Place Led and People Focused

The George Street public domain spine will connect 3 city squares - a square for celebration at Circular Quay, a civic square opposite Town Hall, and a village square at Central Station.

The proposed new Central Station metro station, the redevelopment of the space above the train lines at Central Station, and private site redevelopments, will all contribute to the growth of this area as a future extension of the CBD. The Draft Central Sydney Planning Strategy also supports the revitalisation of the Midtown and Southern precincts.

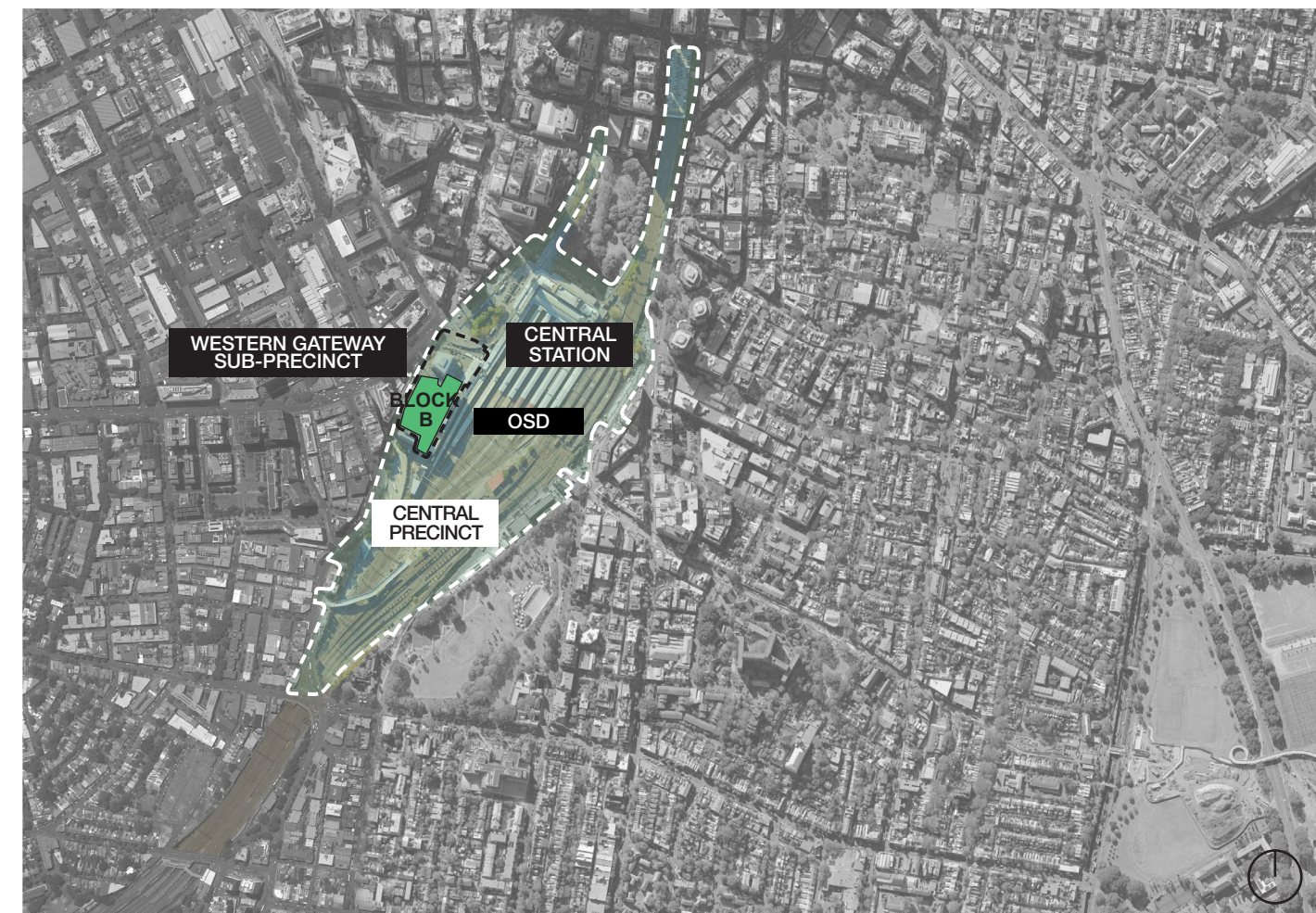


2.2 Site Location

2.2.1 Site Location

Central Precinct

Block B is part of the Central Precinct and is a key part of the Western Gateway. The Western Gateway comprises several sites along Lee Street to the west, each of which will play a critical role in redefining the Central Precinct from this side of the city. The Central Precinct extends to Lawson Street in the south and Goulburn Street in the north and is envisaged to become a revitalised gateway to the city, the region and the global community, creating new energy for the areas commercial, social and cultural engine. A world-class transport interchange will support the development of innovative businesses and diverse public spaces connecting eclectic pieces of the city and celebrating the Precinct's heritage and place.



Block B

Block B is located at 14-30 Lee St, Haymarket and in close proximity to the Devonshire Street Tunnel, Lee St Tunnel, the Goods Line and the proposed Central Walk West. The two immediate neighbours (shown in the diagram below) along with the Western Forecourt collectively form part of the Western Gateway. Block A currently houses the YHA and is a heritage listed original parcel shed. Block C is the the former Parcel Post Office building which is also heritage listed and currently houses the Adina Hotel. The three sites form the core of the Western Gateway.

Block B benefits from direct pedestrian access to Central Railway Station, including country and metropolitan rail services, and the future Central Metro Station.

It is also within close walking distance to existing and future light rail and interstate, regional and metropolitan bus services.



2.3 Site Ownership

2.3.1 Site Ownership

Site Ownership : Above Ground

The current site is a collection of parcels controlled by the Consortium. The arrangement of the lots varies in shape and area between the upper and lower levels.

The diagrams below illustrate the ownership structure and property boundaries.

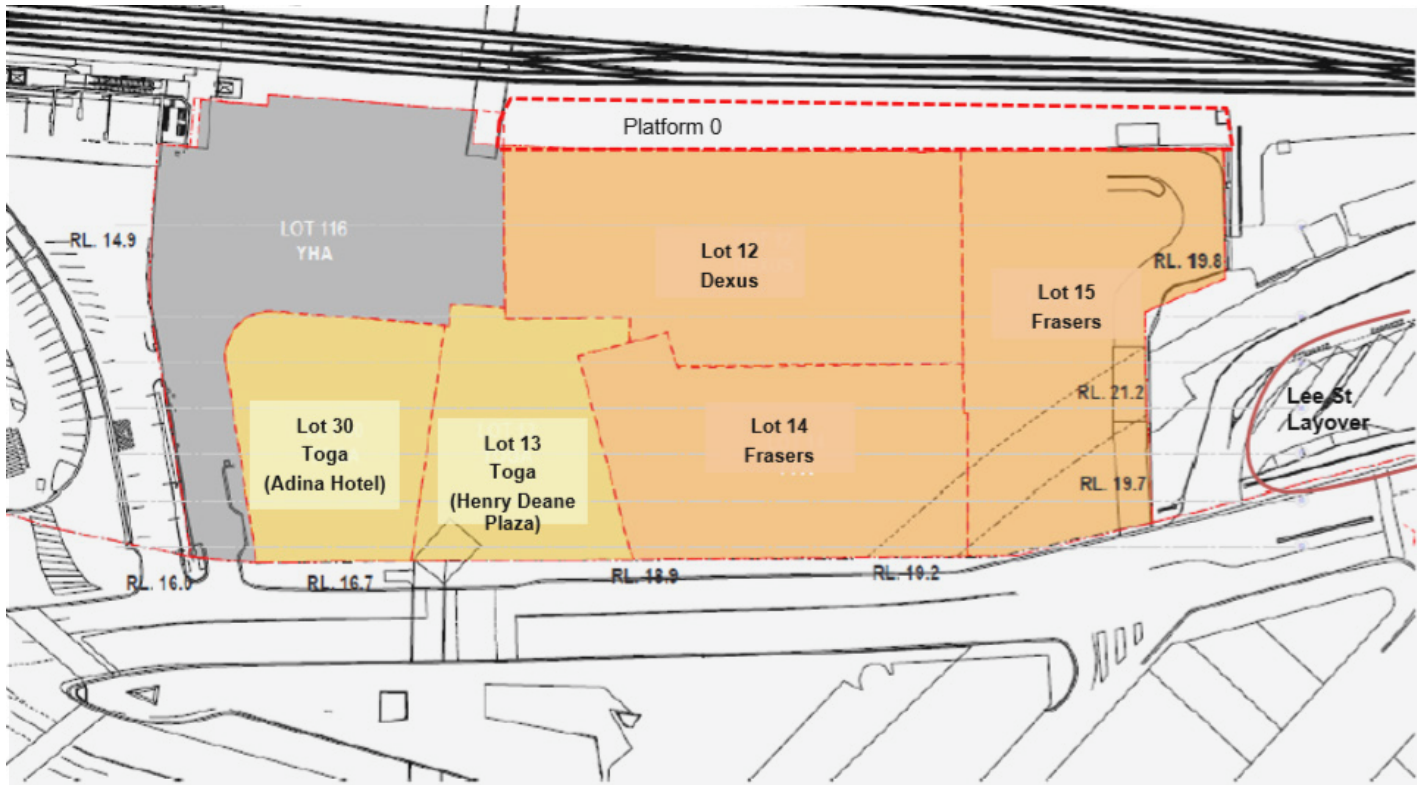


Figure 2.1 Ownership Above Ground

LOT 12	Dexus CPA Pty. Ltd.	
LOT 14	Henry Deane Building Nominees Pty. Ltd. (Fraser's Property)	
LOT 15	Gateway Building Pty. Ltd. (Fraser's Property)	

2.3.1 Site Ownership

Site Ownership : Lower Ground

The lower ground of the ownership parcel consists of a reduced footprint for the Lot 14 parcel. The site is dissected by the Goods Line at this level.

The diagrams below illustrate the ownership structure and property boundaries.

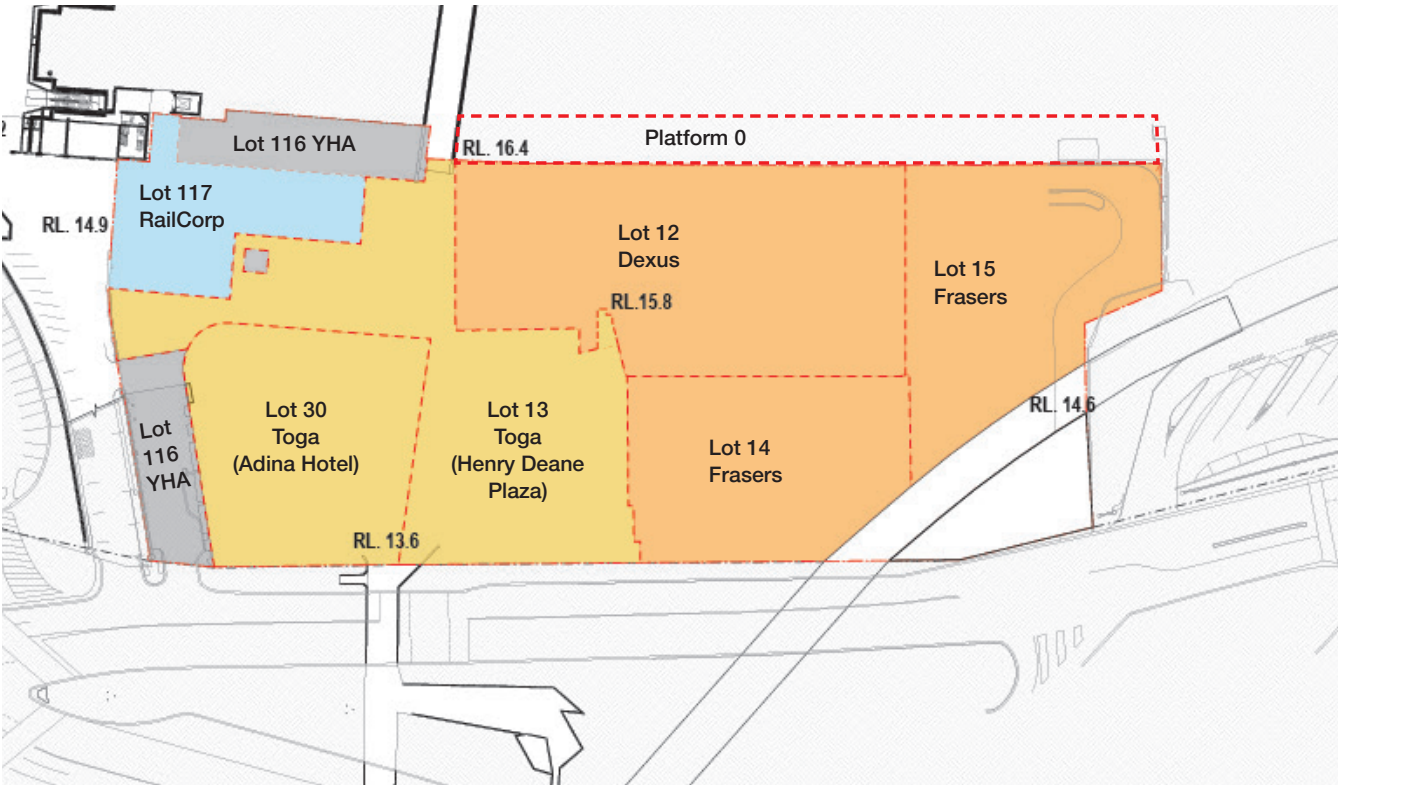
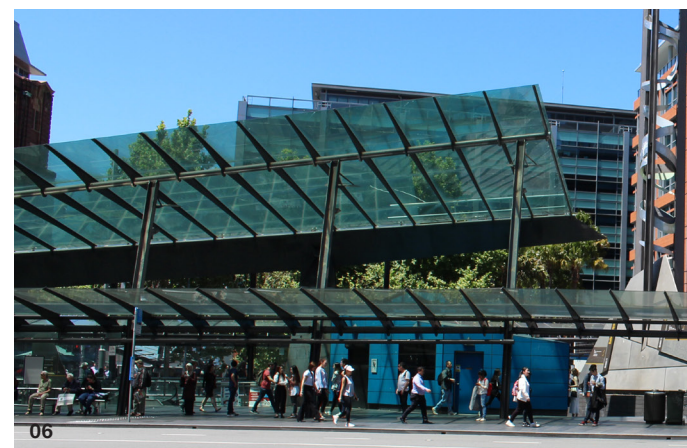


Figure 2.2 Ownership Below Ground

LOT 12	Dexus CPA Pty. Ltd.	
LOT 14	Henry Deane Building Nominees Pty. Ltd. (Fraser's Property)	
LOT 15	Gateway Building Pty. Ltd. (Fraser's Property)	

2.4 Existing site photos

2.4.1 Existing site imagery



- 01 Vehicle entry off Lee Street
- 02 Henry Deane Plaza view above from Lee Street
- 03 Henry Deane Plaza looking toward Devonshire Street Tunnel
- 04 Henry Deane Plaza stairs up to YHA
- 05 Lee Street Tunnel viewed from upper portion of Henry Deane Plaza
- 06 Existing buildings on site as viewed through Railway Square from Broadway across Railway Square
- 07 Level change between lower Henry Deane Plaza and Adina hotel above

2.5 Topography

2.5.1 Precinct Topography

Topography

Block B is located within a surrounding topography that generally slopes gradually from the south east, Prince Alfred Park and the southern portion of Surry Hills to the north west. The diagram beside shows levels relative to Block B as 0 metres.



Figure 2.3 Topography Context

2.5 Topography

2.5.1 Topography Elevation

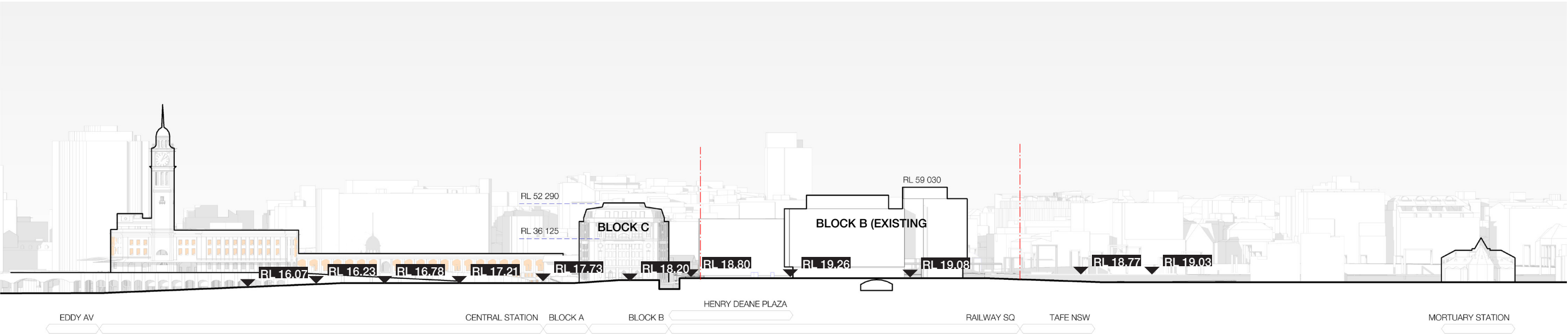


Figure 2.4 Topography - Elevation West

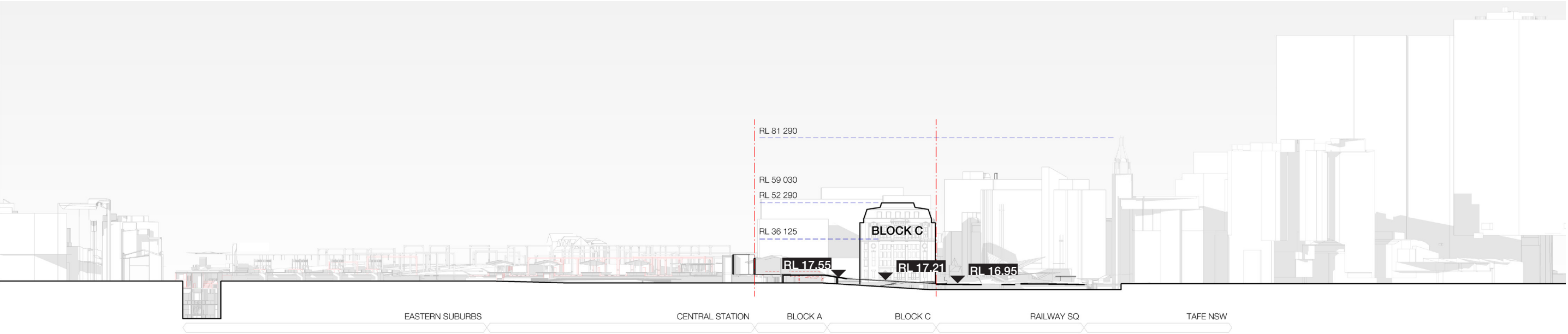


Figure 2.5 Topography - Elevation North

2.5 Topography

2.5.2 Block B Topography

Topography

Block B falls gradually from the South of Lee Street to RL 19.03 down to the north at Railway Square and George Street to RL 16.43.

The existing building ground level courtyard is located at RL 19.60 and is reached by a series of steps. Henry Deane Plaza falls away from Lee Street at RL 17.73 down to the Devonshire Street Tunnel connection at RL 16.35.

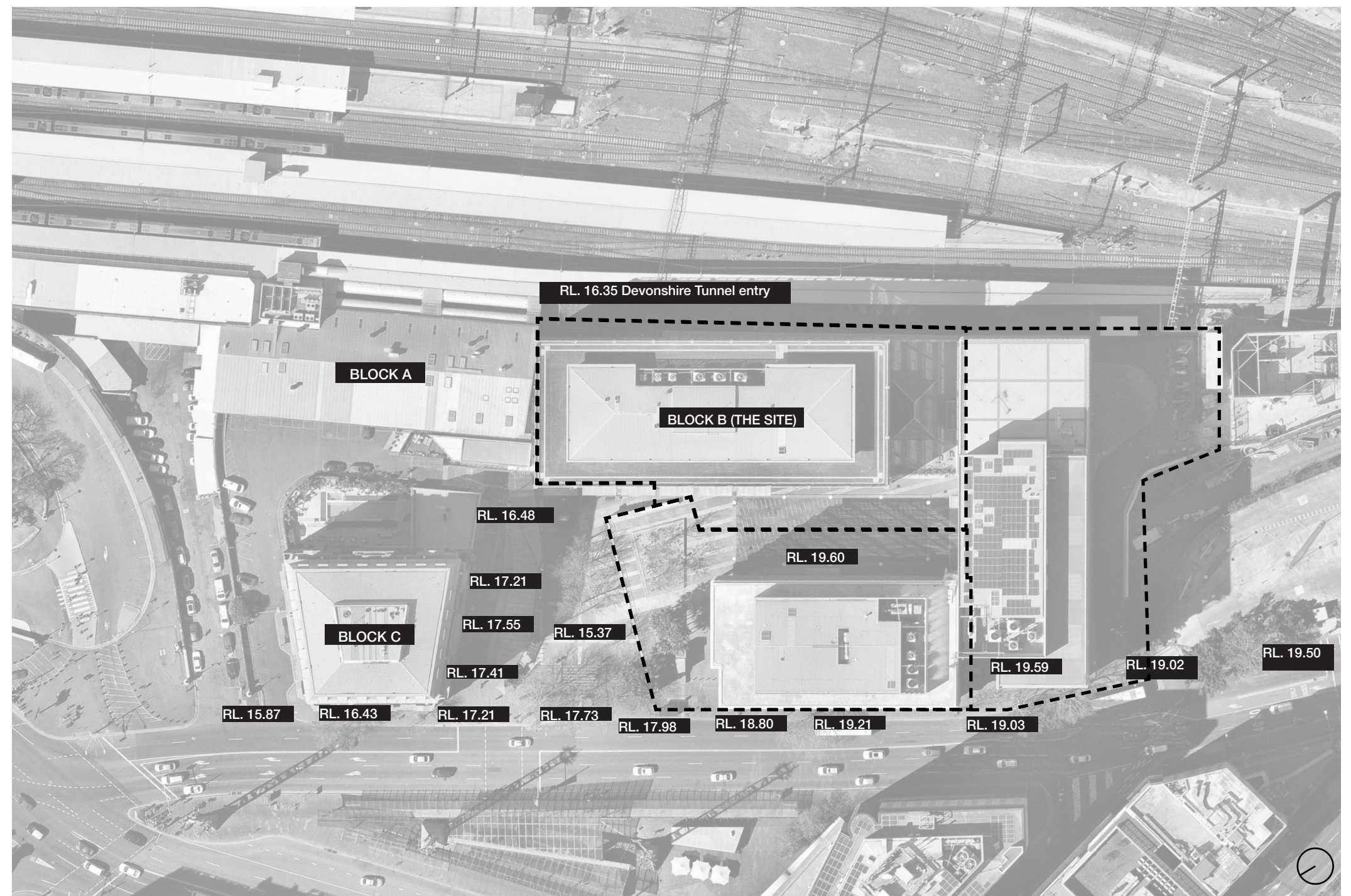


Figure 2.6 Topography existing levels

2.6 Site Analysis

2.6.1 Land Use Context

A number of hotels, including the Adina Hotel located in the restored former Post Office building are north of Block B. The area to the north-east and surrounding Belmore Park is predominantly commercial use. To the north-west is an education precinct, including Tafe NSW Ultimo and UTS. To the west is a mixed-use area, including Central Park and its combination of retail, commercial office, residential, and public open spaces. To the south-west is the Chippendale mixed-use precinct.

A future Tech Precinct is planned over the Western Gateway and area to its east over the existing rail corridor providing future potential links to the open space of Prince Alfred Park.

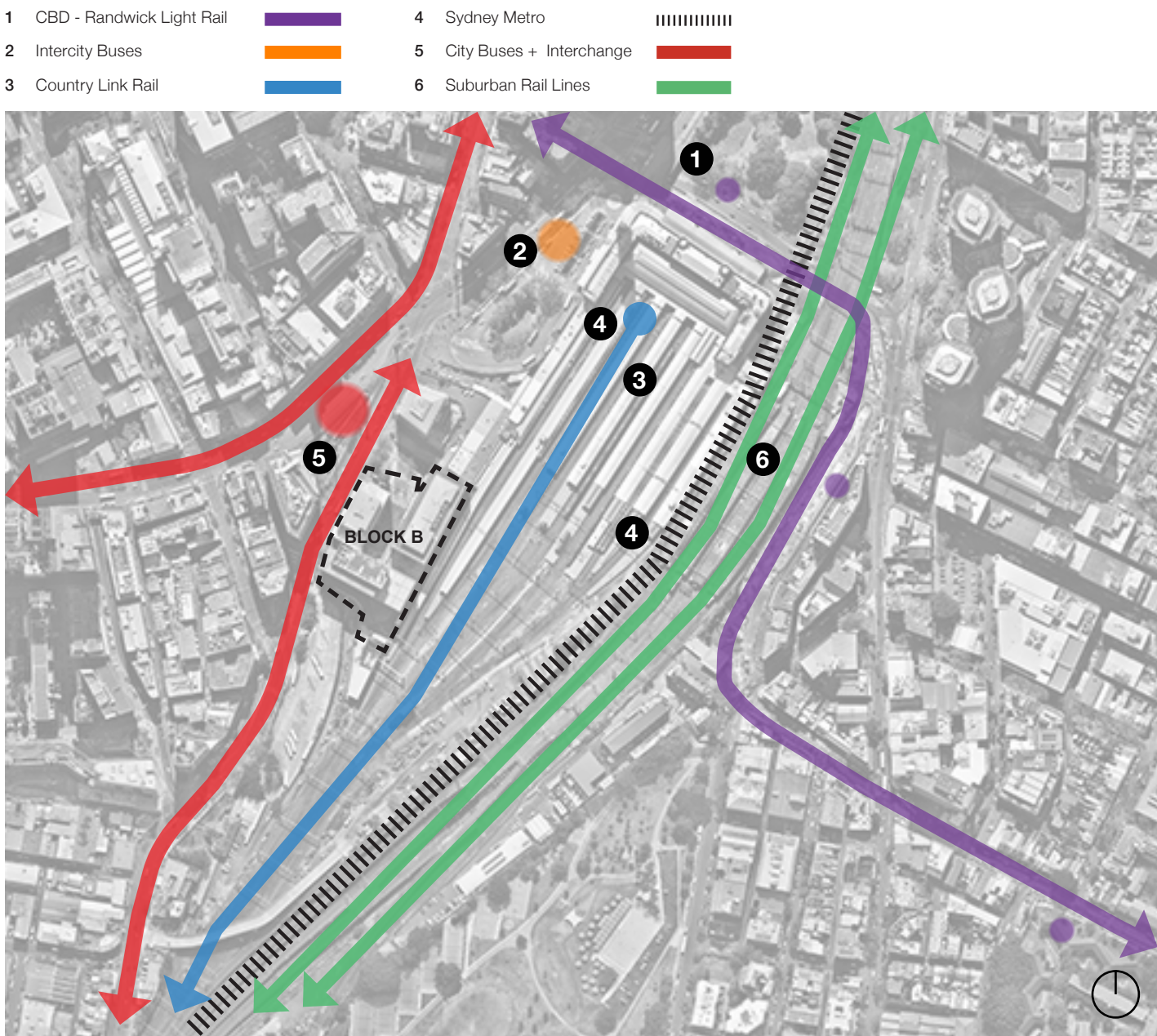


2.6 Site Analysis

2.6.2 Public Transit

Central Station connects Sydney through multiple modes of transportation, and is set to be further enhanced as a inter-modal hub with the Light-Rail and Sydney Metro currently under construction.

Block B is ideally positioned to enhance connectivity and amenity between Railway Square and Central Station.



2.6.3 Pedestrian Movement

An existing east-west link connects George Street with Devonshire Street via the Lee Street and Devonshire Street tunnels, with primary access points at Railway Square, Henry Deane Plaza, Central Station and Chalmers Street. Central Walk East (approved) and Central Walk West (proposed) will provide an additional below ground connection and access to Central Station Metro. Potential new links across the OSD site will enable open to sky connectivity between Surry Hills and Railway Square.



2.6 Site Analysis

2.6.4 Vehicular Movement

Vehicular access to Block B is currently provided via the southern end of Lee Street, which is a 5 lane two-way street.

Lee Street connects with George Street and Regent Street to provide access to the Vehicle Bypass Loop, as identified in the Draft Central Sydney Planning Strategy, providing access around the Sydney CBD. Primary vehicle routes near Block B are Cleveland Street to the south which links with Abercrombie in the west and forms part of a ring road around the CBD. Should Lee Street be closed for pedestrian priority in the future, access should be retained in the existing location.

- 1

Primary CBD ring road
- 2

Existing vehicle routes
- 3

Potential closure



2.6.5 Open Space

Block B sits adjacent to several existing open space areas. This area is earmarked for a future plaza. To the south is HDP which provides pedestrian access into Central Station and a connection to Devonshire St and Surry Hills to the east. Belmore Park is located north of Central Station and to the South is Prince Alfred Park which provide recreational amenities.

- 1

Belmore Park
- 2

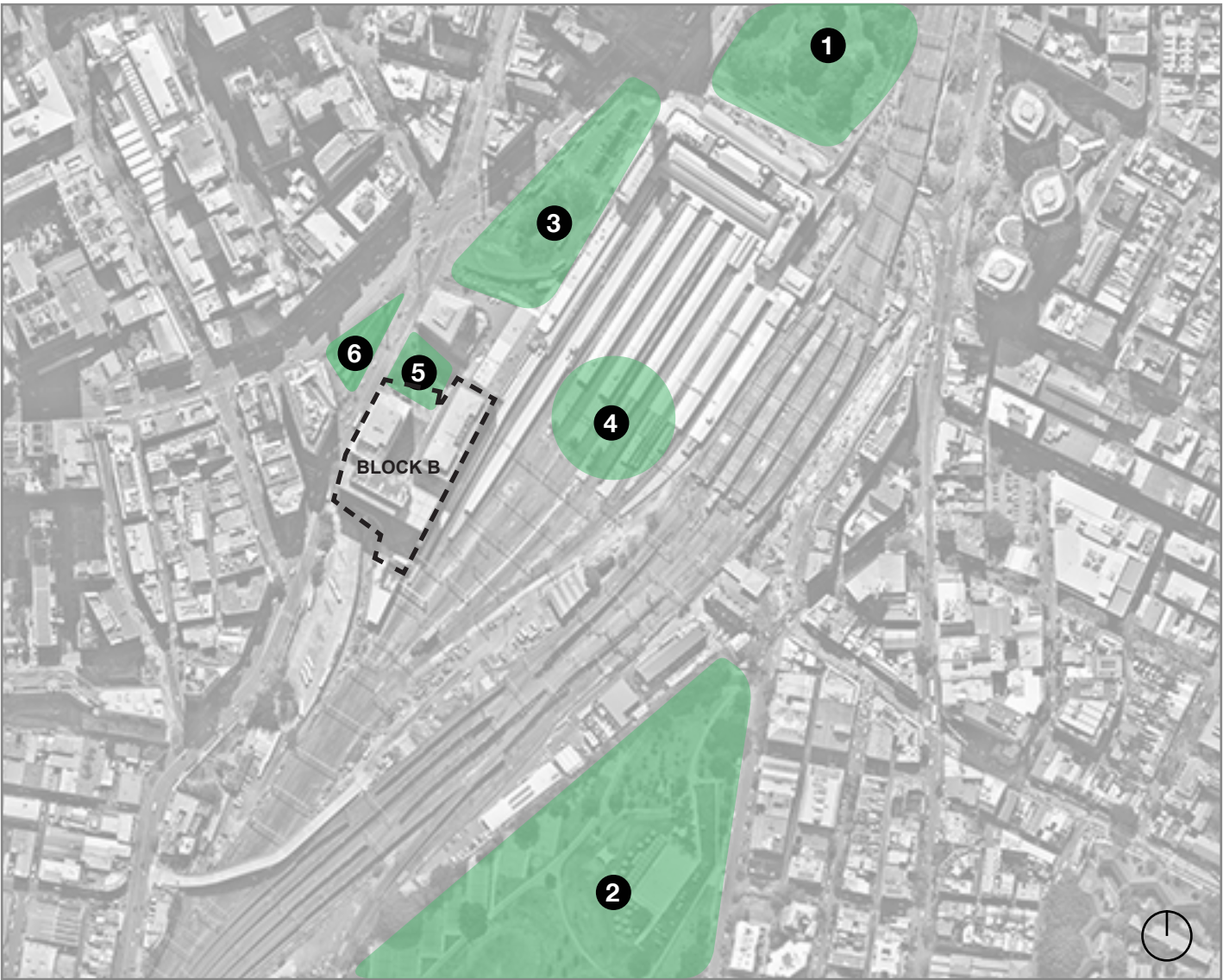
Prince Alfred Park
- 3

Western Forecourt
- 4

Future Open Space
- 5

Henry Deane Plaza
- 6

Railway Square



2.6 Site Analysis

2.6.6 Built Form Context & Tall Buildings

The Western Gateway and future OSD will act as a continuation of the Sydney CBD, and contribute to the growth of this area of the city.

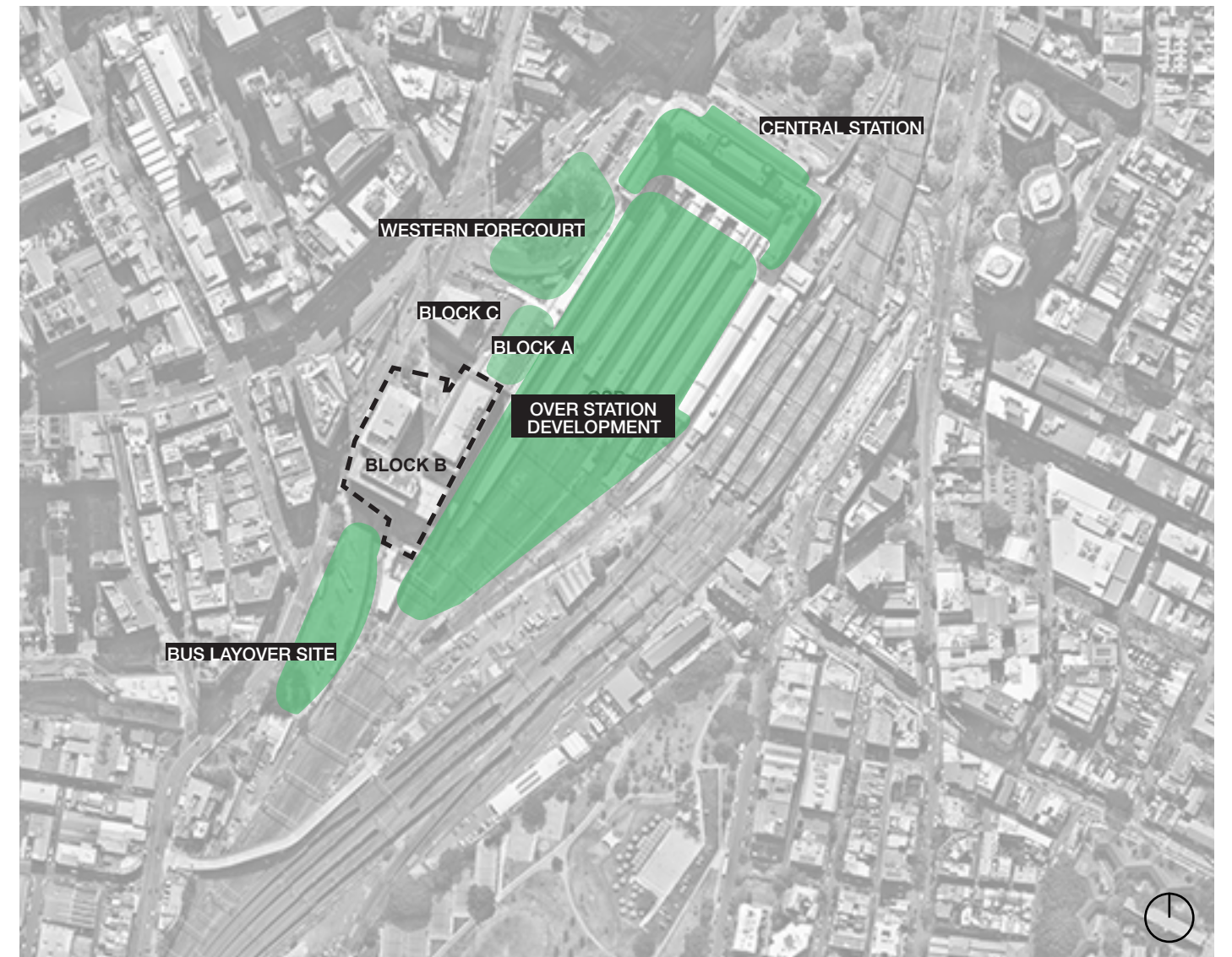
The built form around Block B to the north and west varies in scale, being predominantly high rise, typically 6 storeys and above (approximately 30m+).

The buildings surrounding Railway Square, including Block C, TAFE NSW and Mercure Hotel, define a height datum around the square of approximately +40m. Historic tower forms, most notably the Central Clock Tower (+65.7m), extend past that datum and create landmarks within the built fabric of the Precinct. A number of towers extend further in height, including One Central Park at +120m, and UTS Sydney at +117m.



2.6.7 Future Development

New developments are currently under construction at Darling Square and UTS, as well as a new vertical school at the south western edge of Prince Alfred Park. All of these new developments create a great opportunity for Henry Deane Plaza to clarify pedestrian movement to the evolving desire lines across the Western Gateway, as well as a major vertical connection to the OSD and connections across the station to the East. There is an emerging and inevitable change and transition in height as the new Southern CBD continues to take shape, in particular with the Western Gateway, Tech hub and the potential future OSD. The OSD to the east of Block B and the Western Gateway offers opportunity for links to the open space of Prince Alfred Park. The OSD site will allow for connectivity to be achieved between the surrounding areas, creating direct overland links to Surry Hills which are currently unavailable.



2.6 Site Analysis

2.6.8 Heritage Context

Block B falls within the Central Station heritage group, a local heritage item as defined by the Sydney Local Environmental Plan 2012, and is included within the boundary of the Section 170 Rail Corp Heritage and Conservation Register listing for Central Station. Block B is not State heritage listed.

Additionally, Block B is surrounded by heritage items of varying degrees of significance, highlighted below.

- 1 Dental Hospital

2 Central Station

3 Former Lottery Office
- 4 Former Orchards Chambers

5 Former Parcels Post Office

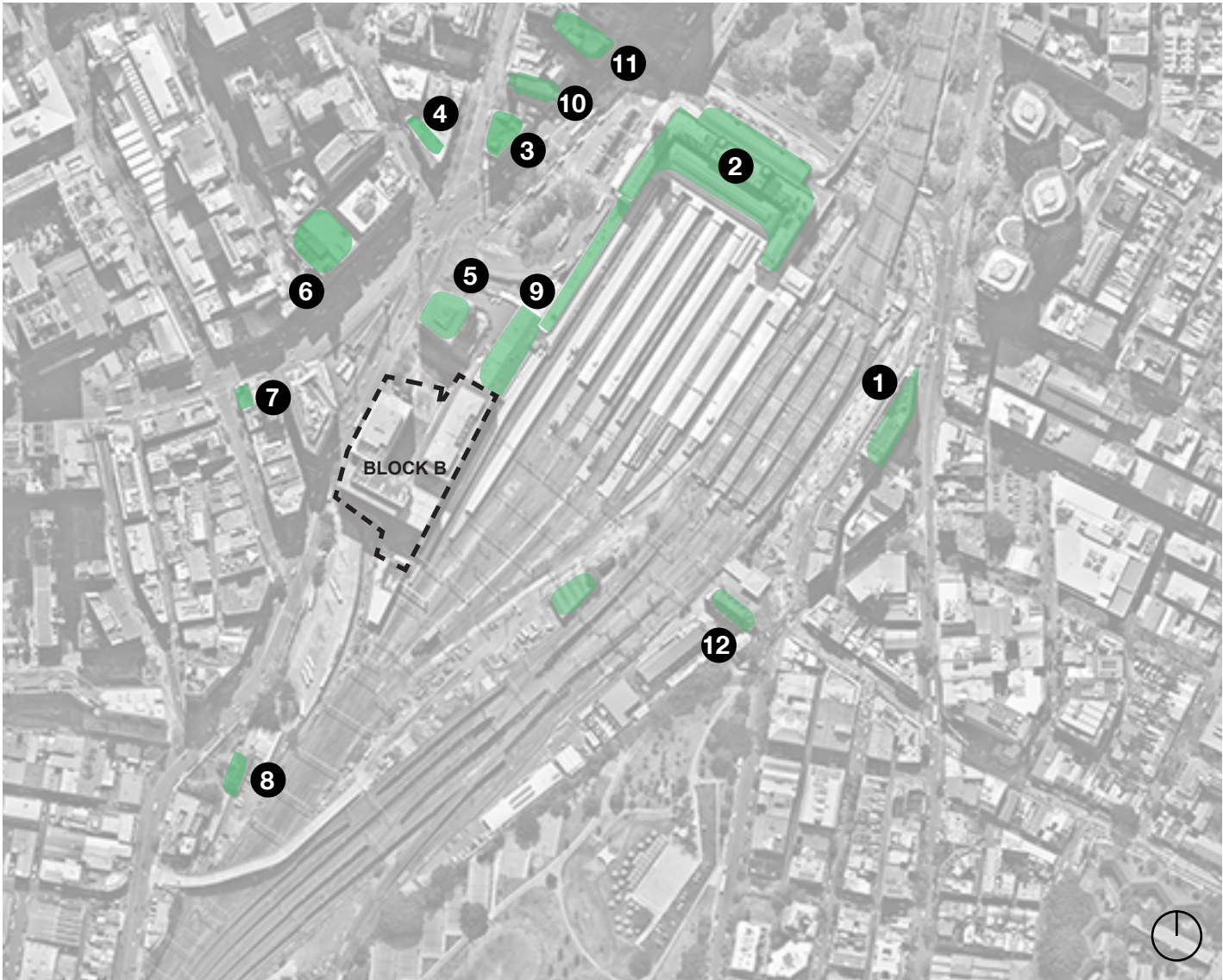
6 Marcus Clarke TAFE
- 7 Former Bank of NSW

8 Former Mortuary Station

9 Parcels Shed
- 10 Saint Laurents Church

11 Former Daking House

12 Former Railway Institute



2.7 Statutory Controls

2.7.1 Current Controls

Land Zoning: Metropolitan Centre (B8)

Floor Space Ratio: 3:1

Max Building Height: 33-35 m

No Additional Shadowing to Prince Alfred Park

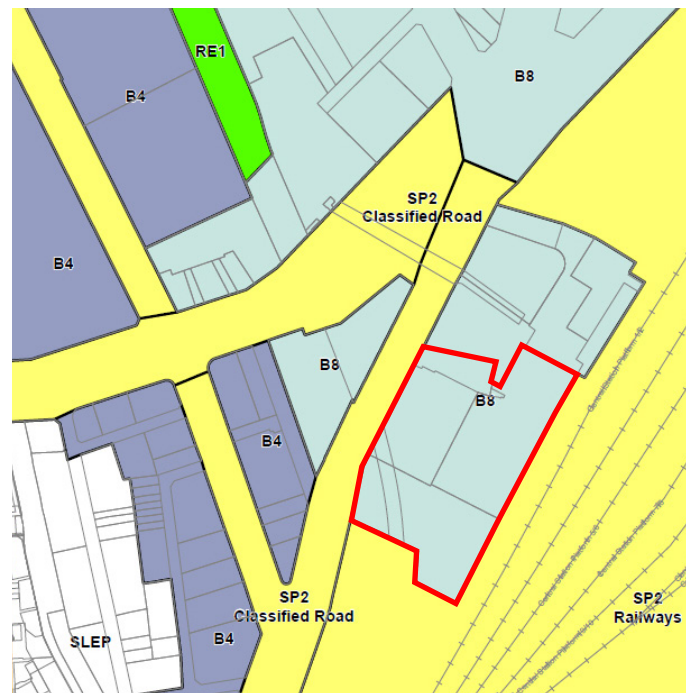


Figure 2.7 Land Zoning

2.7.2 Design Excellence

Competition design process required for:

- Buildings over 55 metres.
- Development with \$100m CIV or more;
- Where a DCP is required; or
- Where the applicant has chosen the process.

Buildings demonstrating design excellence are eligible for a bonus 10% height or FSR.

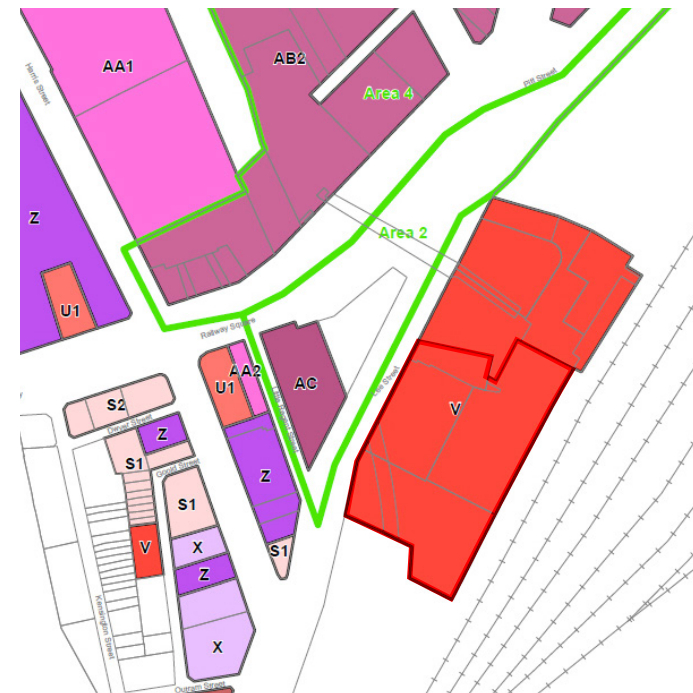


Figure 2.8 Floor Space Ratio

2.7.3 Building Height

The Maximum Building Height on the Site is 33-35m (U-V) as per the Sydney LEP 2012. It is adjacent to zones with a variety of maximum heights:

- Lot 30: 35 metres (V)
- Lot 117: 7.5 metres (H)
- Lot 13: NA (no control)
- Lot 12: 35 metres (V)
- Lots 14 and 15: 33 metres (U)



Figure 2.9 Building Heights

2.7.4 Sun Access Protection

No additional overshadowing to Prince Alfred Park from 14 April to 31 August (beyond shadow cast by 20 metre frontage height on boundary between park and railway land) between noon and 2pm as per the Sydney LEP 2012.

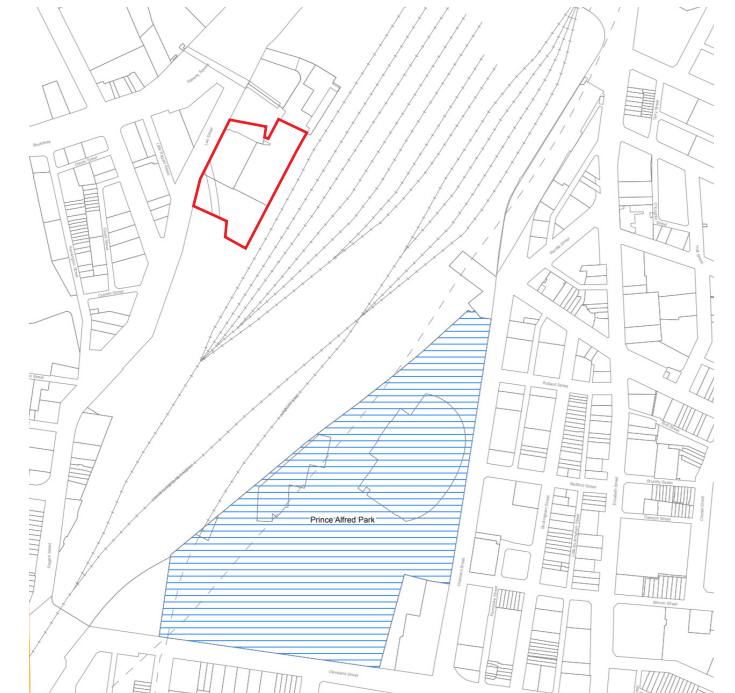


Figure 2.10 Overshadowing Control

2.7 Statutory Controls

2.7.5 Heritage

Block B is heritage listed within Schedule 4 of the Sydney LEP 1012 as local heritage item.

I824: Central Railway Station group including buildings, station yard, viaducts and building interiors.

Block B sits immediately adjacent to Railway Square and Central Station Special Character Area, as defined in Section 2 of the DCP.

Figure 2.11 shows Block B in relationship to the State Heritage Register with curtilage boundaries of the State Heritage Register listing of the Central Station group shown in yellow dashed line. Local Heritage items in buff and heritage conservation areas are shown red hatched.

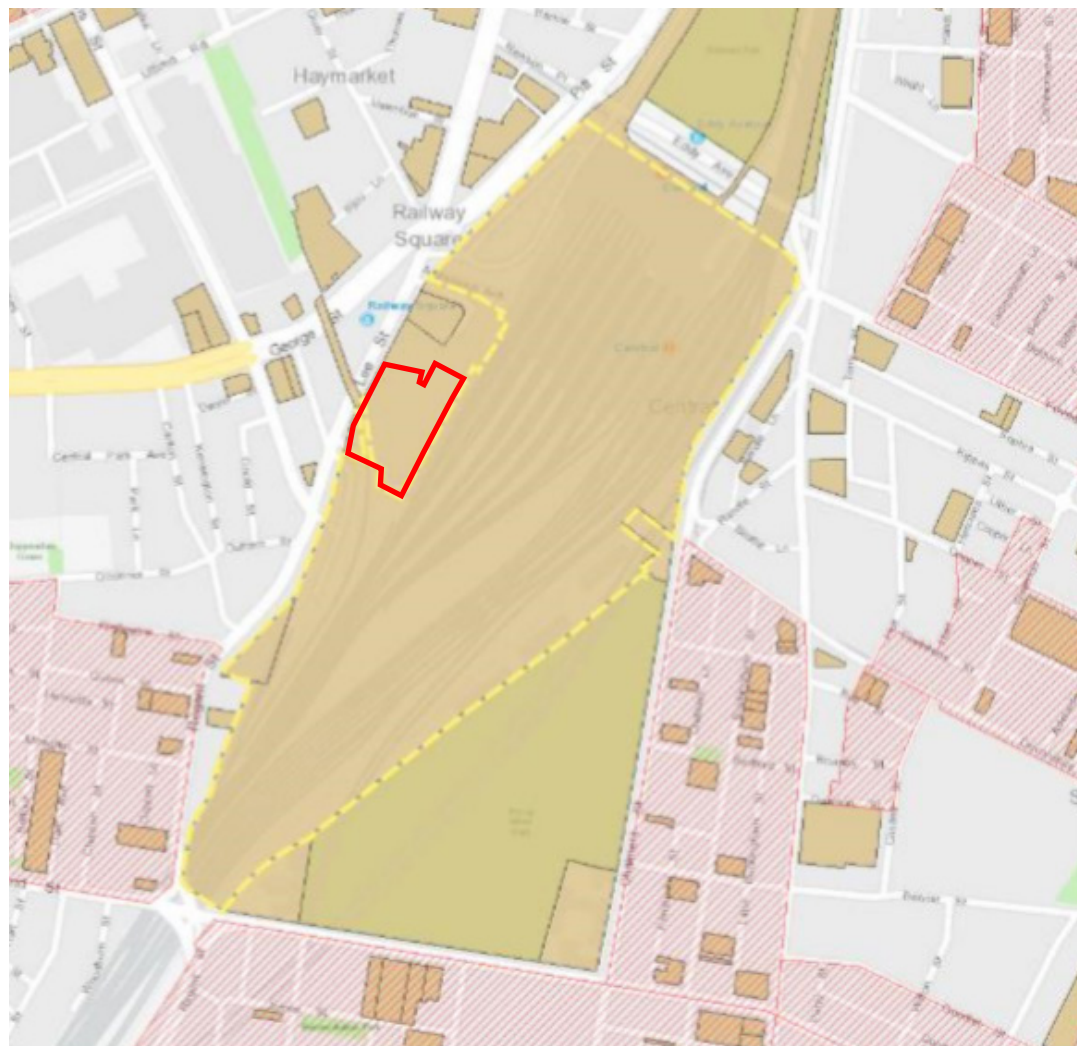


Figure 2.11 State Heritage Context. Source: NSW Planning ePlanning Spatial Viewer 2019



Figure 2.12 Heritage Control

2.7.6 Public Domain

Block B is affected by an existing underground through-site link (as marked in blue). A proposed through site link is shown in purple and an existing through site link shown in green affect the properties to the north.



Figure 2.13 Through Site Link

3.1

Urban Design Framework Strategic Principles

3.1.1 Summary

3.1.2 Place Principles

3.1.3 Public Realm Principles

3.1.4 Urban Form Principles

3.1.5 Futures Principles

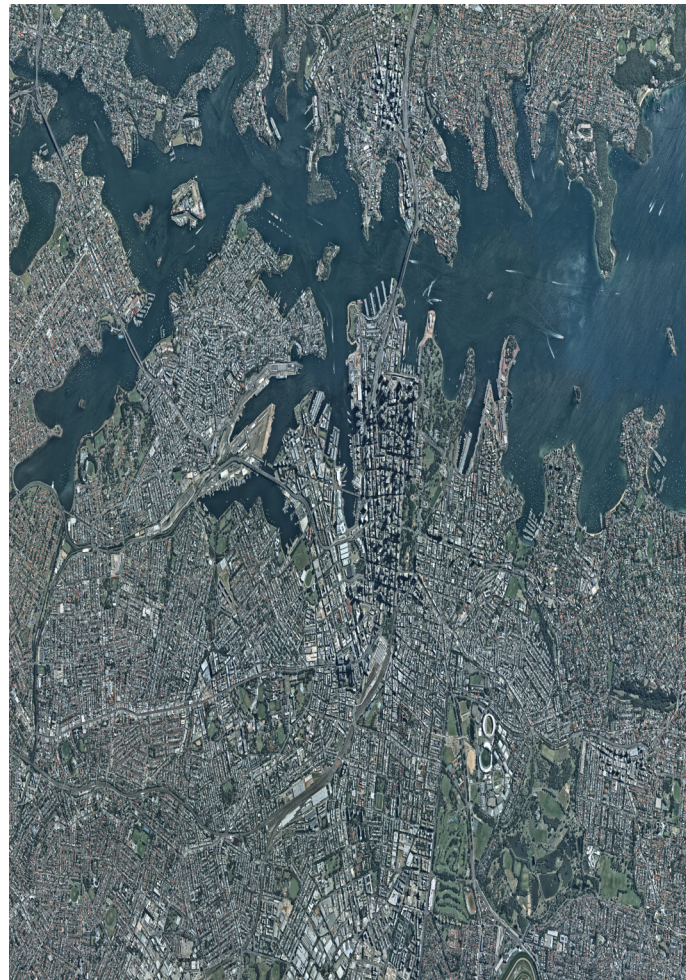
3.1 Strategic Principles

3.1.1 Introduction

Block B, is proposed to be redeveloped to support the NSW Government's aspirations for the Central Precinct and the Sydney Innovation and Technology Precinct. The proposal Urban Design Principles have been developed to enable best practice urban and built outcomes in line with the NSW Government strategic design related plans and policies such as 'Better Placed'. These Strategic Principles are the key moves required to successfully deliver a place based outcome. They are underpinned by TfNSW's Requirements (functional and operational specifications), as well as the commercial requirement of providing suitable tech workplace for an emerging Innovation Precinct. The Strategic Urban Design Principles are driven by city making and ensure proposals are thoughtfully linked to the spaces surrounding Block B and create sustainable and meaningful development for the future.

The Urban Design Principles cover four key drivers which relate to Place, Public Realm, Urban Form and Futures, which are outlined in this section of the report and which are to be used to ensure design excellence is achieved. Section 3.2 of this Framework identifies the Technical Principles, which are metric based controls proposed to apply to future development of Block B.

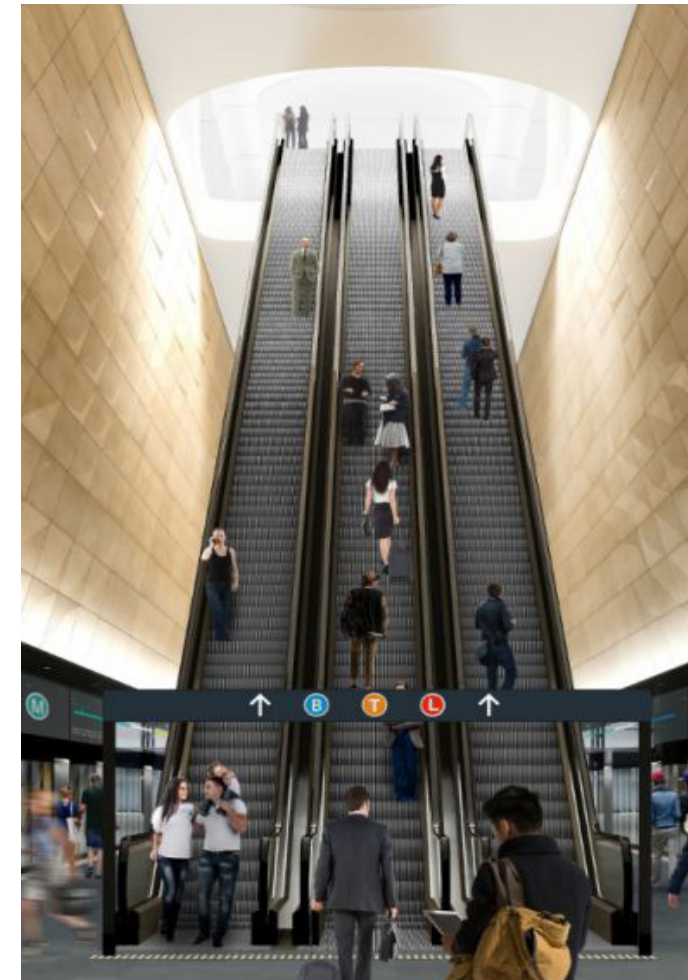
Place



Public Realm



Urban Form



Futures



3.1 Strategic Principles

3.1.2 Place

3.1.2.1 Macro to Micro

Principles

- Integrate into the Central Precinct both existing and proposed infrastructure and allow for pedestrian connections to nearby sites and transport networks. See Figure 1.1
- Demonstrate how the proposal supports the Sydney Innovation and Tech Hub through provision of a variety of workplace options (eg tech, office, retail). Include outdoor work spaces with access to nature where possible.
- Demonstrate the ability to facilitate connections to existing and future adjacent properties including Over Station Development, Block A, Block C and the Bus Layover site.

3.1.2.2 Ecology

Principles

- Include opportunities for landscaping in appropriate areas, considering availability of depth and drainage of soil.
- Use native and climate appropriate species and minimise water/ maintenance resources.

3.1.2.3 Climate

Principles

- Orient the functions, layout and design of the building envelope in response to the site's climatic conditions.

3.1.2.4 Culture

Principles

- Create a multi purpose place which can be used 24 hours, and which is equitable for all.



Figure 1.1

- 1 Integrate into existing and proposed transport precinct pedestrian connections by retaining connections to Lee Street Tunnel, Devonshire Street Tunnel and Central Walk West
- 2 Create a minimum 24-30 metre tower setback zone between Block A and Block B. Provide a clear zone to proposed OSD with stairs and escalators from RL 21.0 up to RL 30
- 3 Create secondary North/ South connection through the site to be accessible 24/7 and have minimum width of 6 metres. To be located at approx RL 21.0
- 4 Consolidate vehicular entry down to basement and Integrated distribution centre and ramp up to Over Station Development to be located at Southern end of Lee Street.
- 5 Maintain Goods Line in its current state and allow for enabling of future connection and use
- 6 Provide basement connection to Blocks A and Blocks B

- Below ground vehicle connection to Connect to Block A + C
- Underground Connection
- Aboveground Connection
- Vehicular Connection
- The Goods Line

3.1 Strategic Principles

3.1.2.5 Heritage

Principles

- Set back development on the Site from the former Parcels Post Office to respect the heritage item's setting, mindful that the former Parcels Post Office was designed to be viewed in the round.
- Reference the principal datums and relative scale of the former Parcels Post Office and its components and, particularly, its upper cornice. Development above the datum of the upper cornice should be either set back or have a vertical separation (for example through a recess). Built form on the Site from ground to this datum should be expressed solidly, with that above being more lightly expressed.
- Retain principal public domain views to the clock tower of Central Station from the south and west and consider longer views to the clock tower including those from Redfern Station on Lawson Street, from the intersection of Regent and Cleveland Streets, from Foveaux Street and from Wentworth Avenue.
- Use high-quality materials that are sympathetic to the historic setting of Railway Square Precinct should be used in new development of contemporary design. These may include materials characteristic of the Railway Square Precinct, such as brick and limited use of sandstone and steel, but may also include contemporary materials.
- Assess the additional shading on Mortuary Station and its gardens and Prince Alfred Park to ensure the heritage impacts upon them are minimised.
- Respect the continuity of the ground plane, existing levels and transitions of Lee St and connections to Railway Square and the Precinct.
- Protect the alignment and spatial envelope of the former Goods Line and explore opportunities to reintegrate it into movement networks.
- Develop an Interpretation Strategy and incorporate interpretation into redevelopment of the Site.
- Integrate these guidelines into site-specific development controls whether by amendment to the Sydney Development Control Plan 2012 or a discrete development control plan for the Site and/or its wider Railway Square/Central Station precinct.



3.1 Strategic Principles

3.1.3 Public Realm

3.1.3.1 Henry Deane Plaza

Principles

- Prioritise the needs of pedestrian and transport customers while considering the use and operation of the Plaza outside weekday peaks.
- Facilitate simple, direct movement between modes of transport and changes of level.
- Integrate with the proposed Central Walk West from Ambulance Avenue.
- Ensure space is capable of handling peak hour pedestrian movements.

3.1.3.2 Pedestrian

Principles

- Integrate with and deliver the pedestrian connections shown in Figure 1.1 and ensure links are comfortable and safe 24/7.
-

3.1.3.3 Cycles

Principles

- Provide sufficient cycle parking and End of Trip facilities to meet projected demands.

3.1.3.4 Service Vehicle/ Private car/ Motorcycle

Principles

- Consolidate service vehicles/ private cars and motorcycle access for Blocks A, B, C and future OSD to the southern end of Lee Street to minimise conflicts with pedestrian movements and allow for provision of an active street frontage to Lee Street.
- Comply with the ‘TfNSW Requirements’ to provide an integrated basement.

3.1.3.5 Lee Street Principles

Principles

- Maximise articulation and modulation of the Lee Street facade.
- Respond to any committed plans to pedestrianise Lee Street.

3.1.3.6 Devonshire Street Tunnel

Principles

- Retain the current levels, portal connection and operation of the Devonshire Street Tunnel.
- Comply with relevant TfNSW Requirements relating to the Devonshire Street Tunnel.
-

3.1.3.7 Lee Street Tunnel

Principles

- Retain the current levels, portal connection to and operation of the Lee Street Tunnel.

3.1.3.8 The Goods Line

Principles

- Maintain the Goods Line in its current form/ scale and enable future use/ activation.

3.1.3.9 Mortuary Station

Principles

- Demonstrate a future connection with Mortuary Station.
-

3.1.3.10 Central Walk West

Principles

- Consider direct pedestrian access between the Central Walk West and Block B.

3.1.3.11 Rail Track Infrastructure/ Over Station Development

Principles

- Enable appropriately sized and equitable vertical access to future Over Station Development from the north eastern edge of the Block B.
- Demonstrate future integration with Over Station Development.

3.1.3.12 Equitable and Inclusive

Principles

- Integrate the use of passive and active surveillance in the design.
- Provide full and equal access to all people of all levels of mobility.
- Work with TfNSW to develop a precinct wide strategy for social inclusion and equity.
- Attenuate noise impacts for different land uses consistent with statutory requirements.
- Development is to consider the provisions of the State Environmental Planning Policy (Infrastructure) 2007 and Development Near Rail Corridors and Busy Roads Interim Guidelines.

3.1 Strategic Principles

3.1.4 Urban Form

3.1.4.1 Height

Principles

- Comply with the Prince Alfred Park Solar Access requirements required in clause 6.19 of LEP 2012 as well as the City of Sydney’s Draft Central Sydney Planning Strategy: Solar Access Height Contours Map.
- Consider visual and overshadowing impacts to the adjoining sites.
- The proportion and articulation of tower forms are to be of appropriate scale and detail and respond to long city views.

3.1.4.2 Skyline

Principles

- Assess skyline impacts from the following viewpoints:

1. Prince Alfred Park
2. Circular Quay to Central
3. Anzac Bridge to Central

The assessment must consider:

1. Existing conditions.
2. Projects that are either under construction or have secured planning consent.
3. Future development plans, either submitted or in early conceptual phase for Blocks A,C and OSD.

Demonstrate the relationship to Central Station and the Clock Tower in both wider urban panoramas, as well as at street level or from public spaces.

3.1.4.3 Scale

Principles

- Demonstrate an appropriate change of scale, articulation and detail with particular attention at the base and roof levels.
- New structures are to demonstrate how the design responds to the proportions, forms, details of the historic buildings and surrounding context.

3.1.4.4 Materiality and Character

Principles

- Articulate a visually solid podium base which compliments existing structures and clearly defines Lee Street and Railway Square.

3.1.4.5 Lee Street and Railway Square

Principles

- Any podium form is to be built out to the plot boundary to define the street edge.
- Setbacks are to demonstrate a positive relationship to Railway Square.

3.1.4.6 General Principles

Principles

- Define Henry Deane Plaza through a podium edge which provides for permeability and activation.
- Design ground floor levels to provide activation.

3.1.4.7 Tower Principles

Principles

- Towers may be at zero setback from the podium if it can be shown that there are no public realm benefits from setting the tower(s) back from the podium.
- If the tower extends the full or majority of length of podium, then significant articulation is required.
- Demonstrate relationship to future OSD.

3.1 Strategic Principles

3.1.4.8 George Street Views

Principles

- The design needs to terminate the view with a visual point of interest. Views matching those shown in the Technical Principles section of the Urban Design Report are to be provided.

3.1.4.9 The Goods Line Views

Principles

- Tower form and articulation to positively address this viewpoint.

3.1.4.10 Broadway Views

Principles

- Demonstrate complimentary eastern foreground definition of Railway Square to strengthen the view of the clock tower

3.1.4.11 Lee Street Views

Principles

- Retain and enhance the current qualities of Lee Street, by building to the street edge boundary.

3.1.4.12 Devonshire Street/ Chalmers Street Views

Principles

- Tower form and articulation to positively address and anchor the viewpoint from Surry Hills.

3.1.4.13 Prince Alfred Park Views

Principles

- Develop tower forms and articulation that break down the scale when seen from Prince Alfred Park.
- Where possible, maximise the view of the sky below the Solar Access Plane.

3.1.4.14 Wentworth and Goulburn Views

Principles

- Develop tower forms and articulation that break down the scale when viewed from Wentworth Avenue and Goulburn Street.
- Where possible, maximise the view of the sky below the Solar Access Plane.



Figure 3.7 Street View Locations

- 1 George Street View vantage point
- 2 Pitt Street View vantage point
- 3 The Goods Line View vantage point
- 4 Broadway View vantage point
- 5 Lee Street View vantage point
- 6 Devonshire Street/ Chalmers Street vantage point
- 7 Prince Alfred Park vantage point
- 8 Wentworth and Goulburn Street View vantage point

3.1 Strategic Principles

3.1.5 Futures

3.1.5.1 Sustainability

Principles

- Target material selection and design responses that mitigate the impacts of the urban heat island.
- Create a welcoming and equitable public realm which encourages health and wellbeing outcomes.
- Targeting:
 - 5 star NABERS energy rating for commercial uses
 - 4.5 star NABERS energy rating for hotel uses
 - 4 star NABERS water rating for commercial and hotel uses
 - Silver core and shell WELL rating (version 2) for commercial uses
 - 5 star Green Star Design and As-Built rating (Version 1)
- Consider water sensitive design principles including detention and retention of storm water having regard to present and future climatic conditions.

3.1.5.2 Building Form and Workplace

Principles

- Consider form and function of floor plates suitable to the requirements of the modern workplace and which are able to provide adaptable space desired by technology and innovation services providers.

3.2

Technical Principles

3.2.1 Envelope Principles

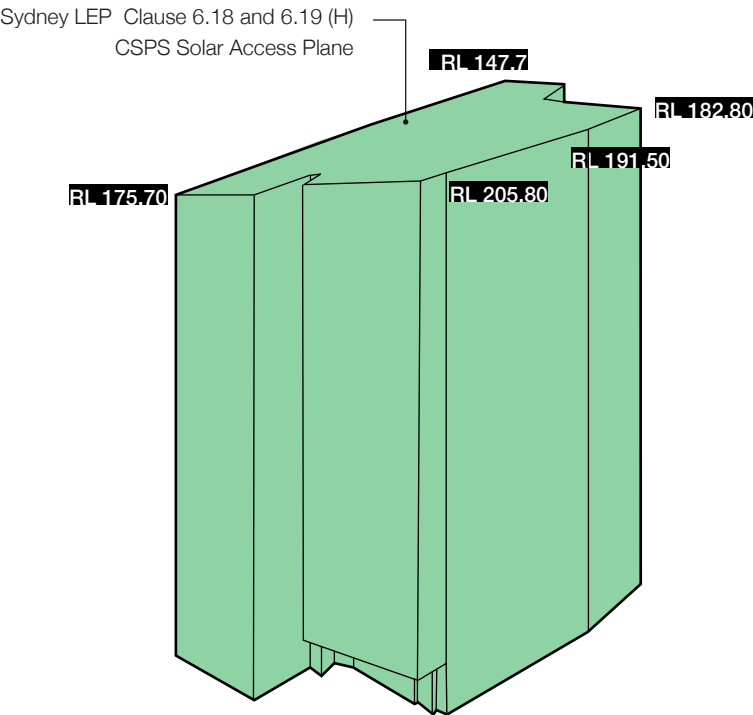
3.2.2 Principle Diagrams

3.2 Technical Principles

3.2.1 Envelope Principles

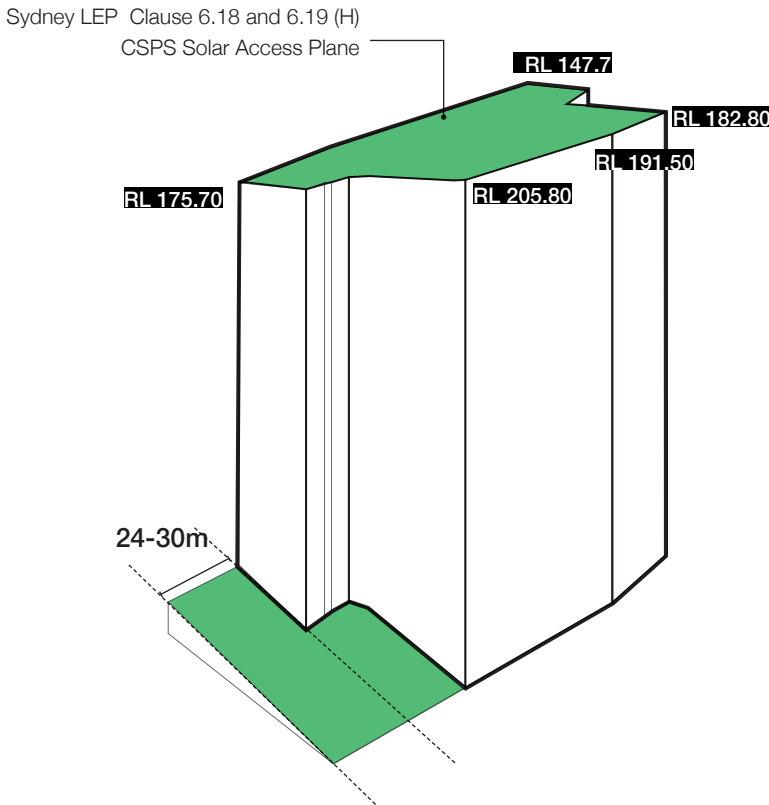
3.2.1.1 Site Planning Envelope

The building envelope is extruded from the site boundary at grade to a maximum height which shall conform to the Sydney LEP (2012) Clause 6.18 and 6.19 (h) and the City of Sydney's Draft Central Sydney Planning Strategy: Solar Access Height Contours Map.



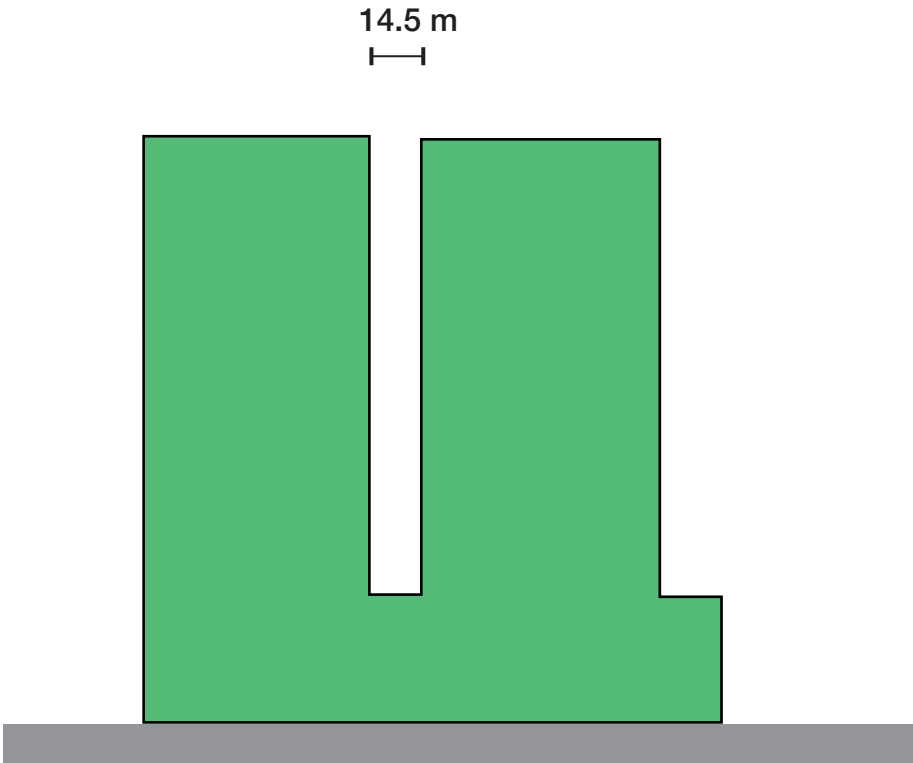
3.2.1.2 Planning Envelope Massing

The planning envelope sets back Block B (the sum of 24m + 1m + measurement of Block A setback) from the line of the Block C façade when extended to the east to achieve a minimum 24-30 metre tower separation between Block A and Block B built form. The line of the setback is then extruded to a maximum height to comply with Sydney LEP (2012) Clause 6.18 and 6.19 (h) and the City of Sydney's Draft Central Sydney Planning Strategy: Solar Access Height Contours Map. The envelope is further reduced to align to the lower ground boundary of Lot 14.



3.2.1.3 Tower Separation

If more than one tower is proposed within the site a 14.5 metre setback is required between towers.

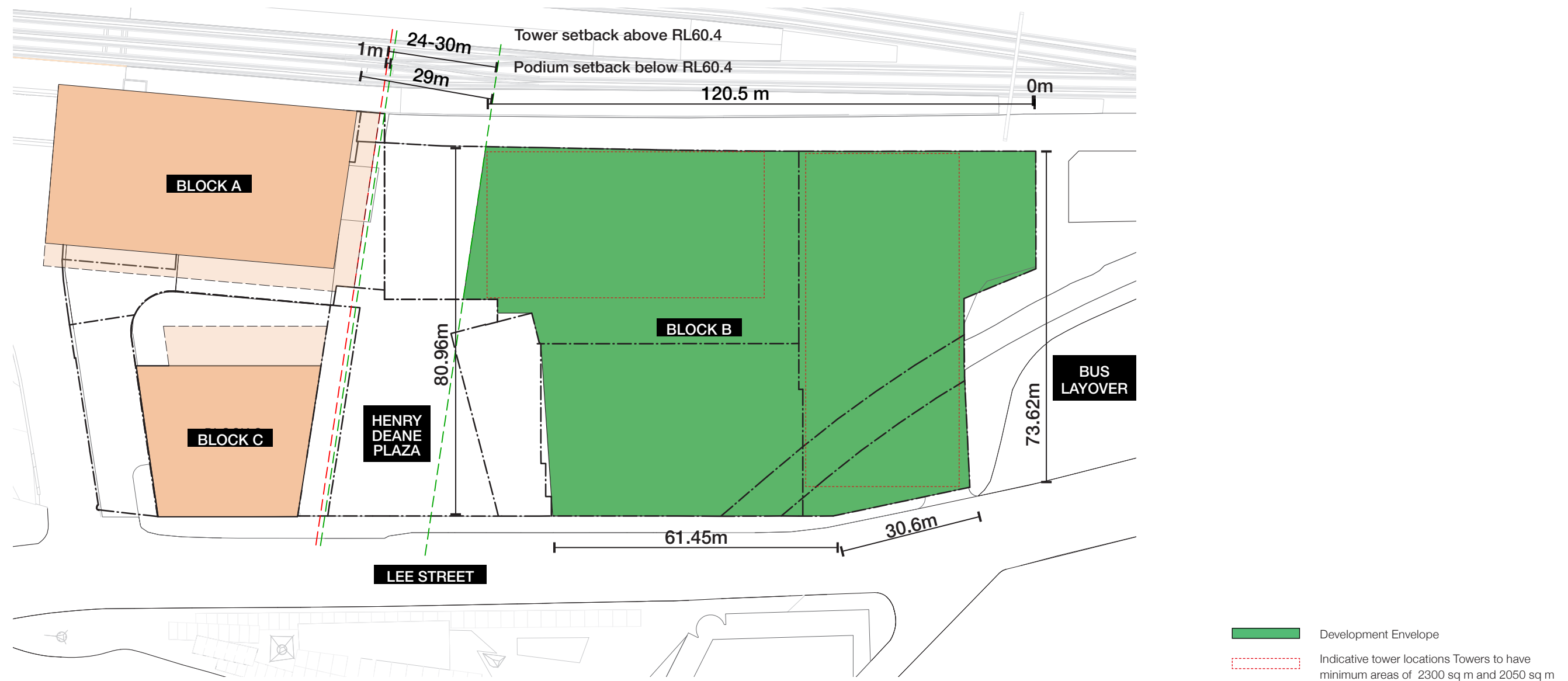


Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.

3.2 Technical Principles

3.2.1.4 Strategic Principles Plan

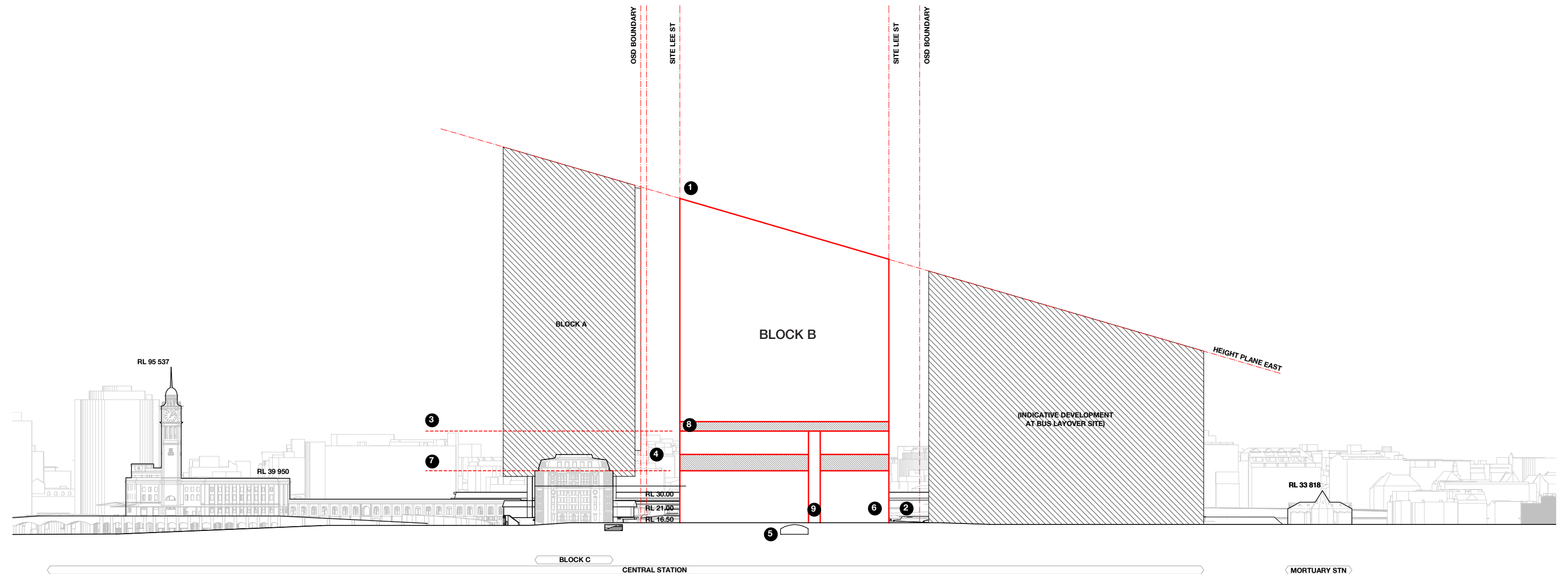
The strategic principles require a setback of 24-30 metre to the north / eastern boundary between Block A and Block B towers. Block B is to be set back (sum of 24m + 1m + measurement of Block A setback) from the line of the Block C façade when extended to the east to achieve a minimum 24-30 metre tower separation between Block A and Block B built form. Any podium to Lee Street to have a maximum RL of 63.80 above which a setback is required.



Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.

3.2 Technical Principles

3.2.2 Principle Diagrams



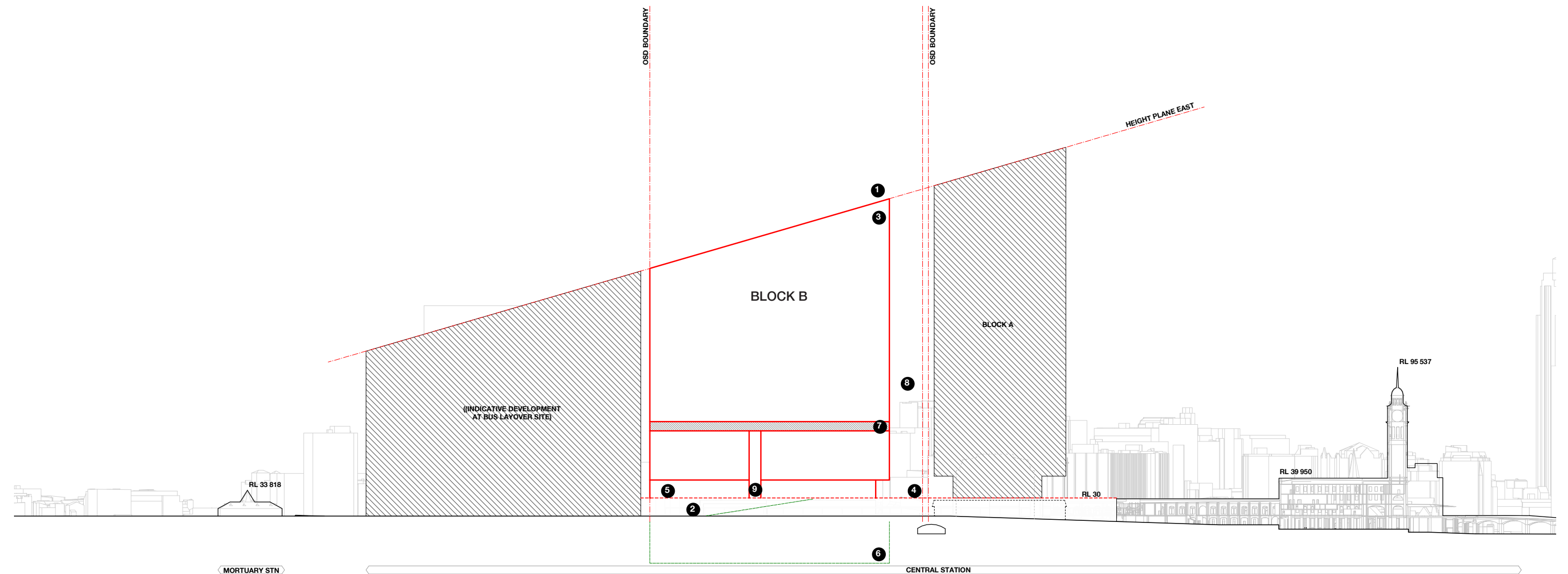
3.2.2.1 Diagram - Western Elevation/ Lee Street Principles

- 1 Building envelope height to comply with Sydney (2012) LEP clause 6.18 and 6.19 (h) and the City of Sydney's Draft Central Sydney Planning Strategy: Solar Access Height Contours Map
- 2 Provide vehicle access to southern end of Lee Street to below ground distribution centre and up to OSD at RL 30.0
- 3 Zero setback to Lee Street and Henry Deane Plaza to a maximum podium RL of 63.80
- 4 Block B is to be set back (sum of 24m + 1m + measurement of Block A setback) from the line of the Block C façade when extended to the east to achieve a minimum 24-30 metre tower separation between Block A and Block B built form
- 5 Maintain access to the Goods Line
- 6 Provide an activated frontage to Lee Street
- 7 Lee Street podium facade to align to Masonry parapet of Block C with separation and articulation above. Refer to Heritage Principles
- 8 Provide clear definition between Podium and Tower
- 9 Provide east/ west connection through the site to future OSD

Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.

3.2 Technical Principles

3.2.2 Principle Diagrams



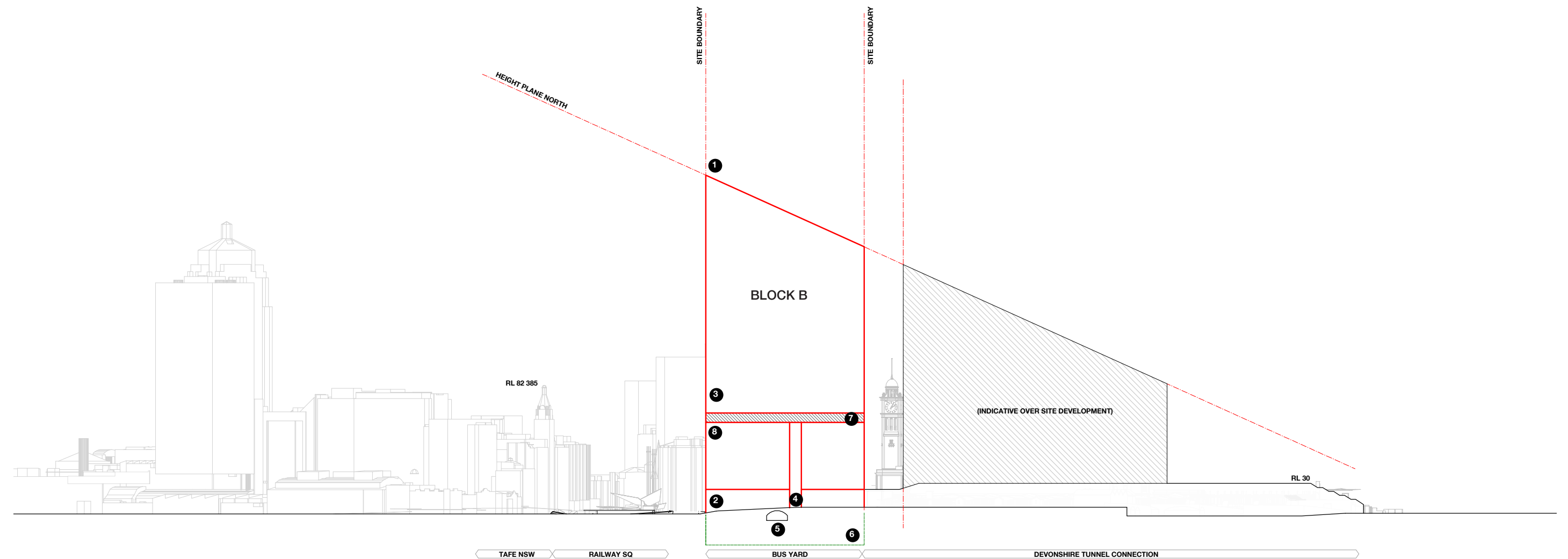
3.2.2.2 Diagram - Eastern Elevation/ OSD interface Principles

- | | |
|---|---|
| <ol style="list-style-type: none"> 1 Building envelope height to comply with Sydney (2012) LEP clause 6.18 and 6.19 (h) and the City of Sydney's Draft Central Sydney Planning Strategy: Solar Access Height Contours Map 2 Provide vehicle access up to OSD deck to RL 30.00 3 Zero setback to eastern elevation 4 Provide pedestrian connection to Henry Deane Plaza 5 Provide activated frontage to OSD where no conflict to ramp exists 6 Provide below ground Integrated Distribution Facilities and associated vehicle parking and End of Trip facilities | <ol style="list-style-type: none"> 7 Provide clear definition between Podium and Tower 8 Block B is to be set back (sum of 24m + 1m + measurement of Block A setback) from the line of the Block C façade when extended to the east to achieve a minimum 24-30 metre tower separation between Block A and Block B built form. 9 Provide a secondary east/ west connection through the site to Lee Street |
|---|---|

Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.

3.2 Technical Principles

3.2.2 Principle Diagrams



3.2.2.4 Diagram - Southern interface Principles

- 1 Building envelope height to comply with Sydney (2012) LEP clause 6.18 and 6.19 (h) and the City of Sydney's Draft Central Sydney Planning Strategy: Solar Access Height Contours Map.
- 2 Provide vehicle access up to OSD deck to RL 30.00 and down to Integrated Distribution Facility and associated vehicle parking and End of Trip facilities
- 3 Zero setback to southern elevation
- 4 Provide connection through Block B to enable north/ south connection to bus layover site
- 5 Maintain access to the Goods Line
- 6 Provide below ground Integrated Distribution Facilities and associated vehicle parking and End of Trip facilities

- 7 Provide clear definition between Podium and Tower
- 8 Podium to respond to RL of 63.80 metres to reinforce character of Railway Square.

3.3

Building Envelope

3.3.1 Building Envelope

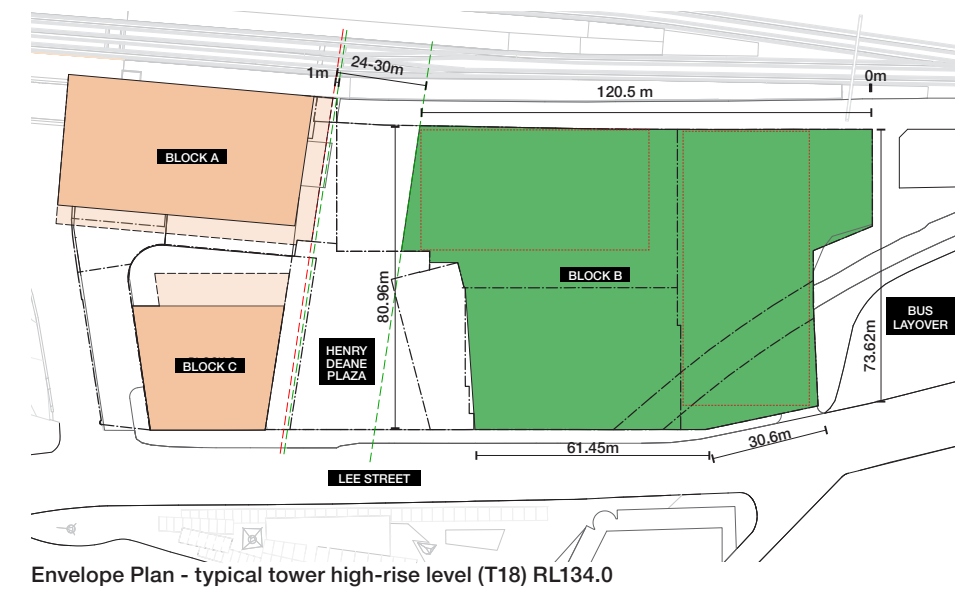
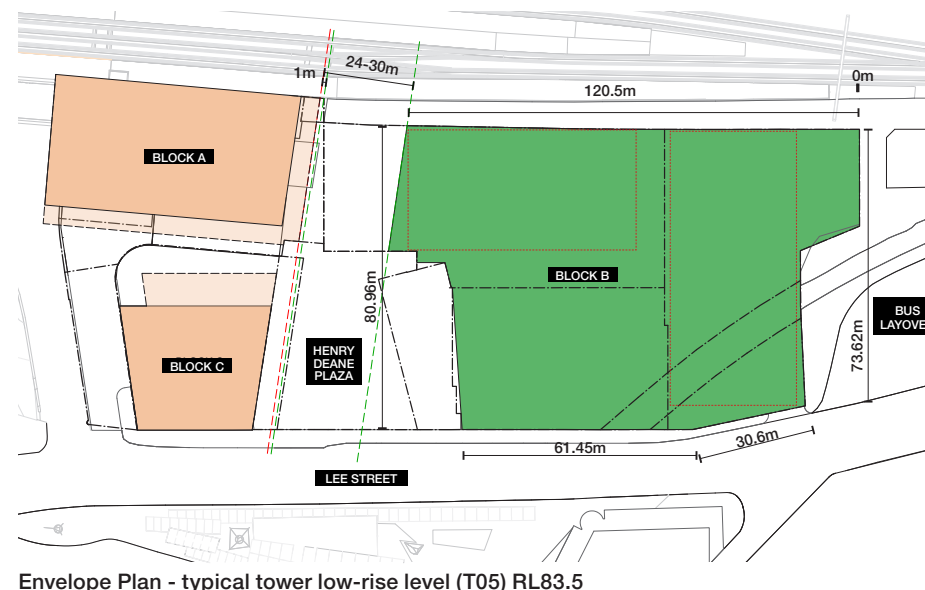
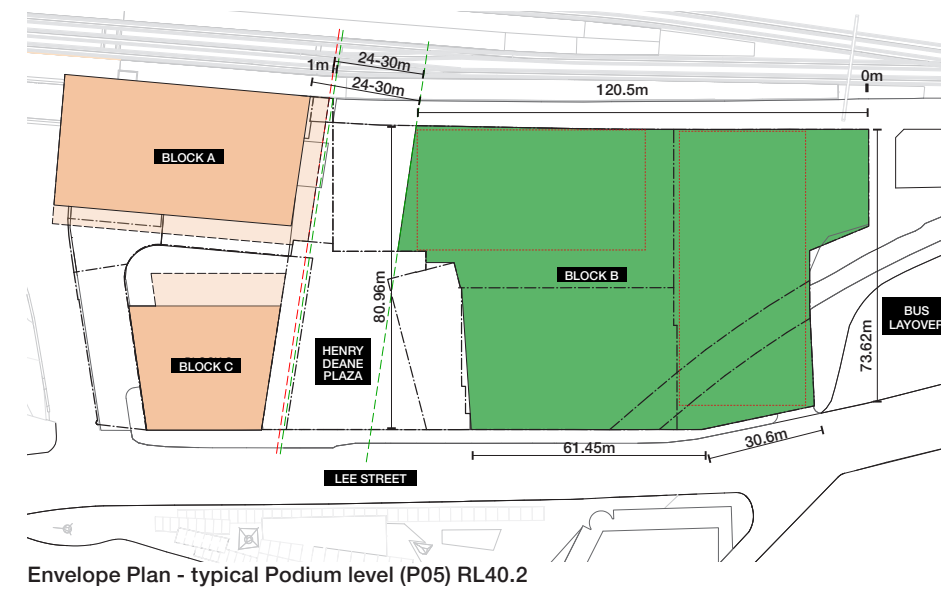
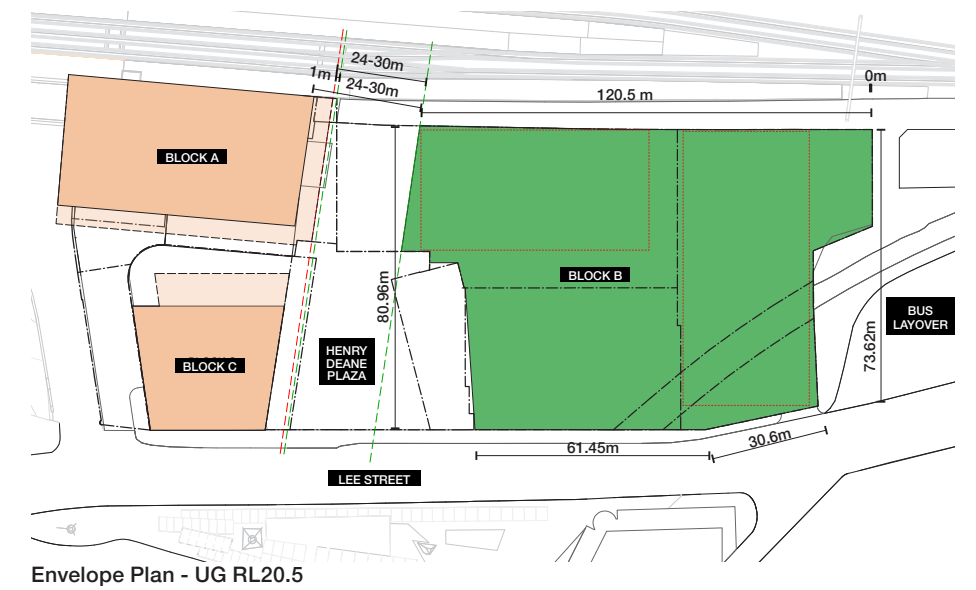
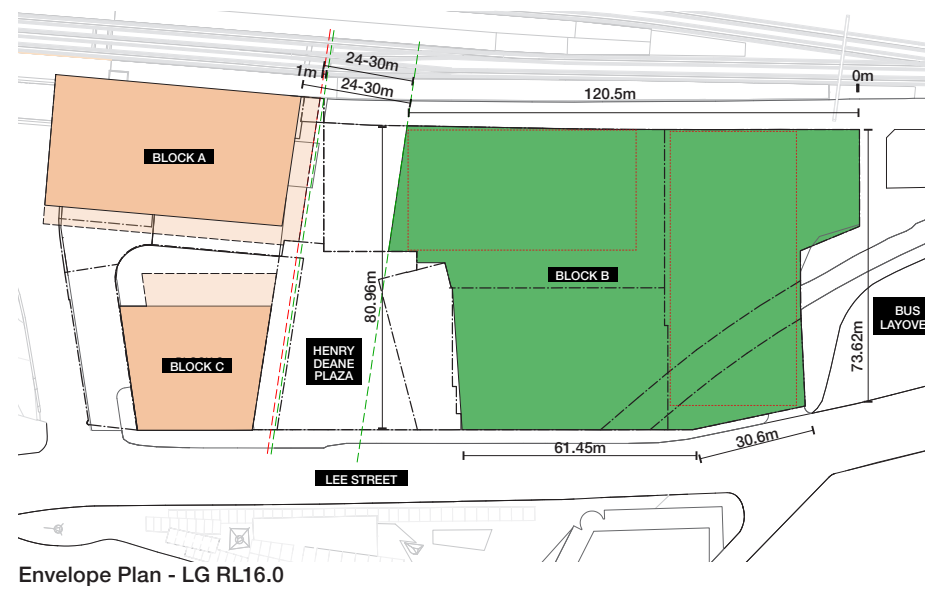
3.3.2 Envelope Impact Analysis

3.3 Building Envelope

3.3.1 Building Envelope

3.3.1.1 Building Envelope Plan

- Development Envelope
- Indicative tower locations Towers to have minimum areas of 2300 sq m and 2050 sq m

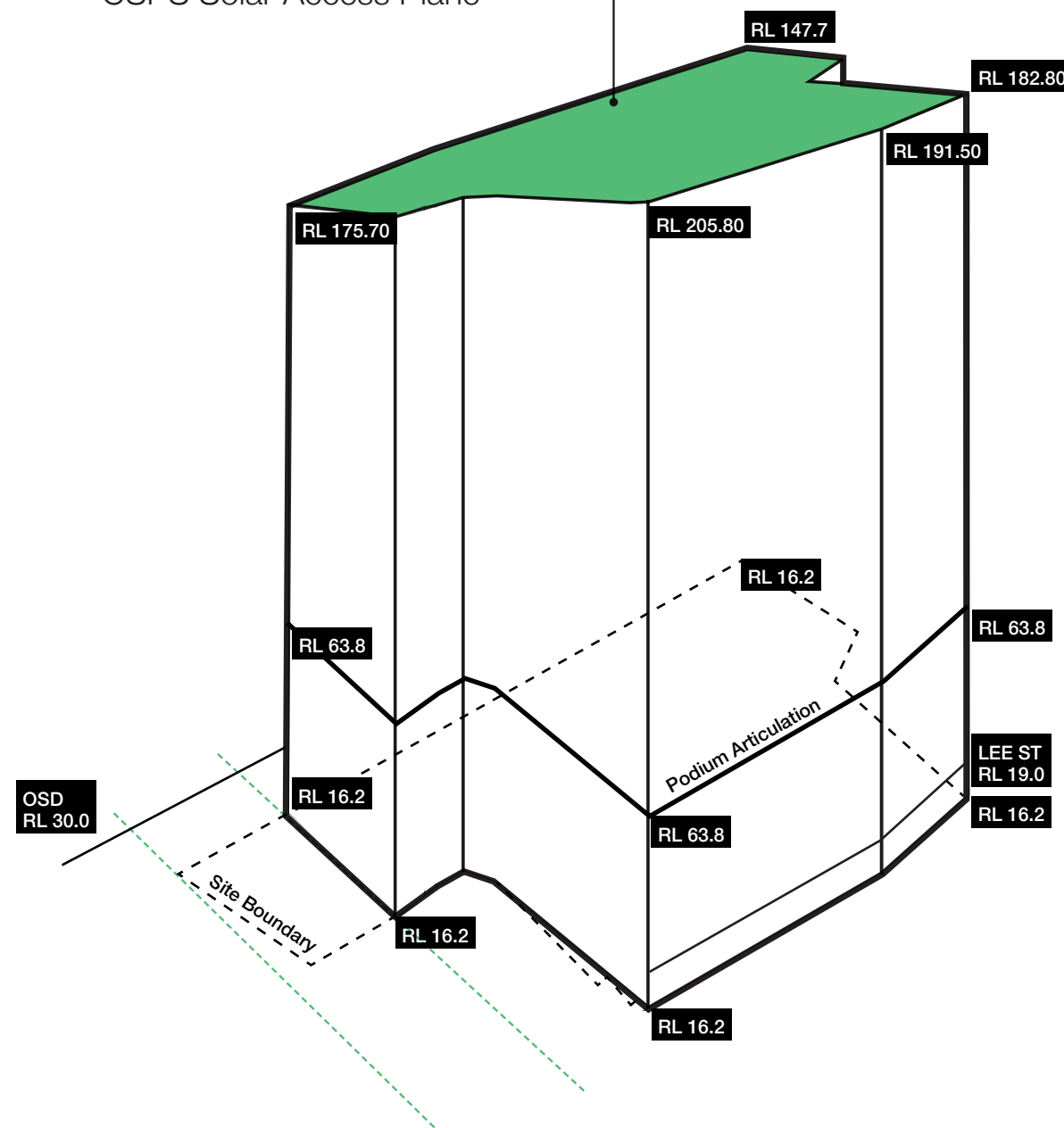


Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.

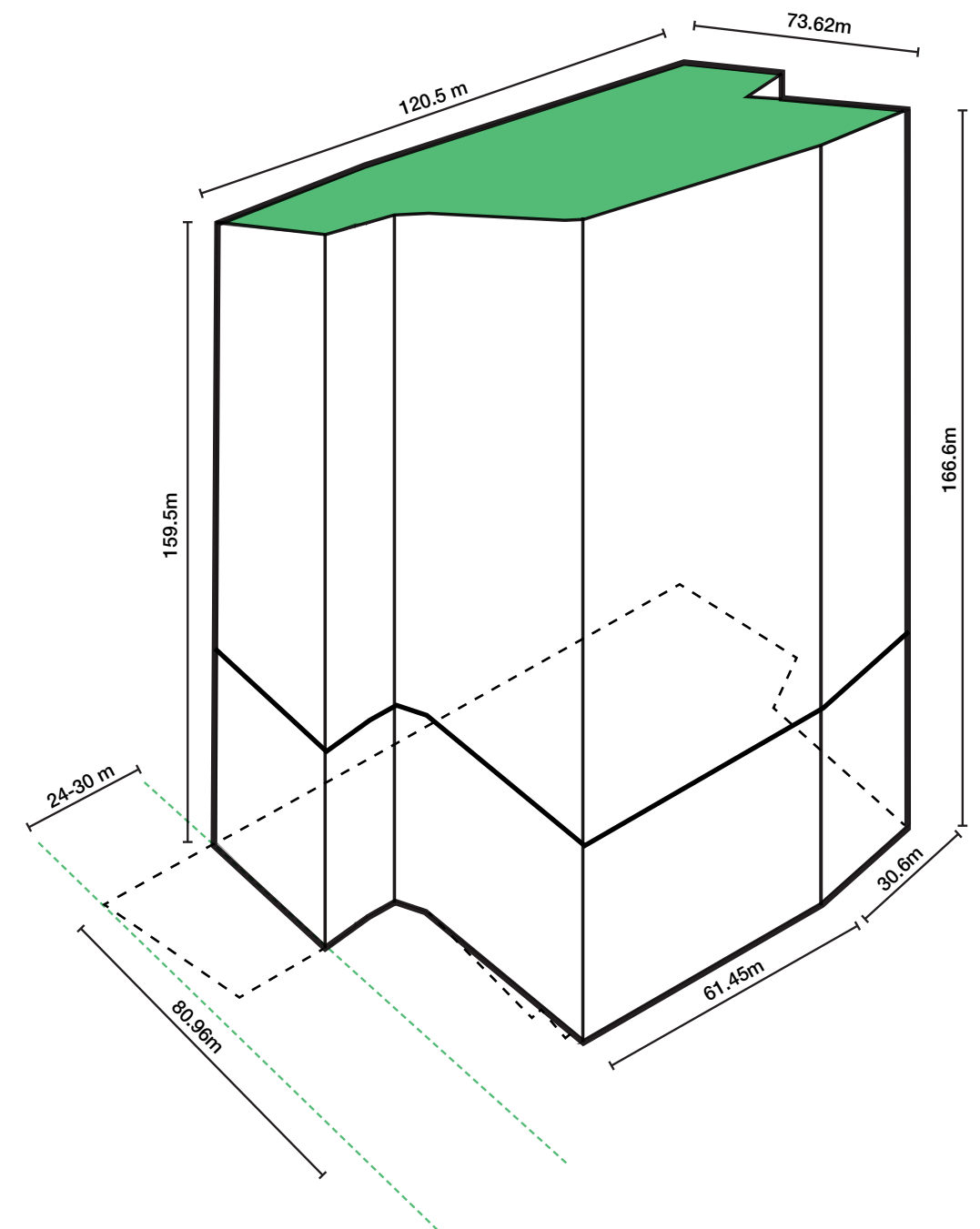
3.3 Building Envelope

3.3.1 Building Envelope

Sydney LEP Clause 6.18 and 6.19 (H)
CSPS Solar Access Plane

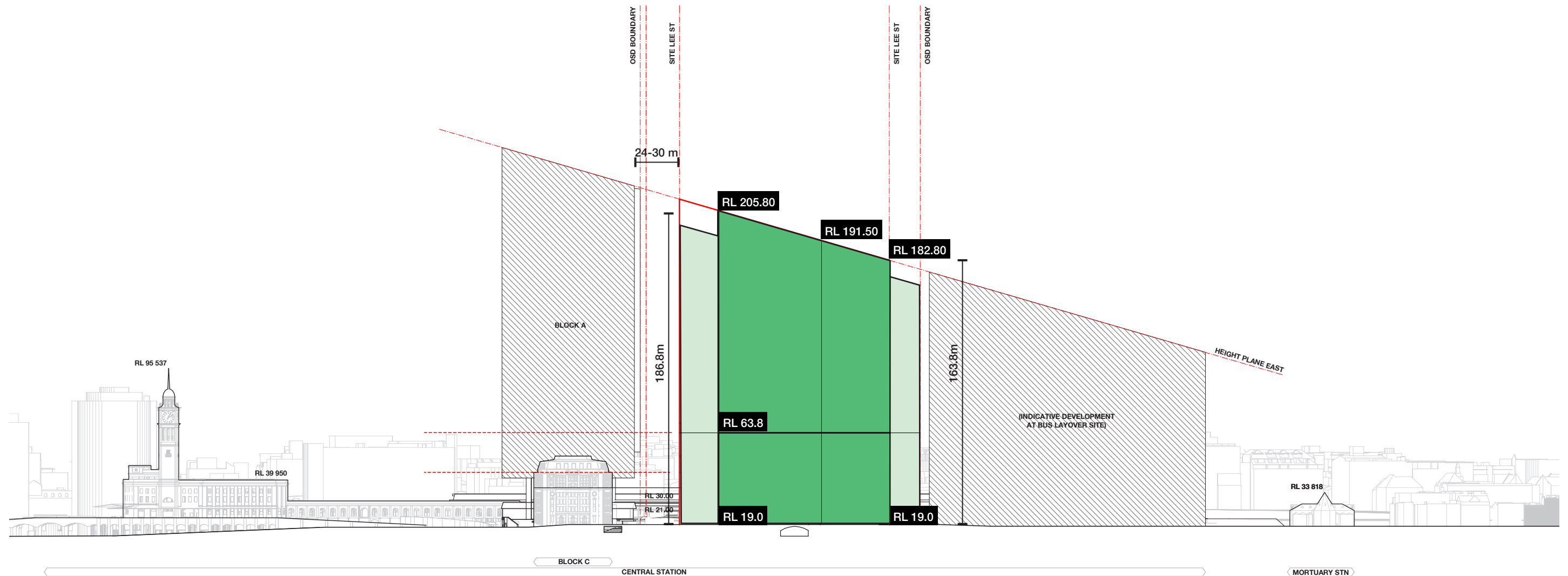


Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.



3.3 Building Envelope

3.3.1 Building Envelope

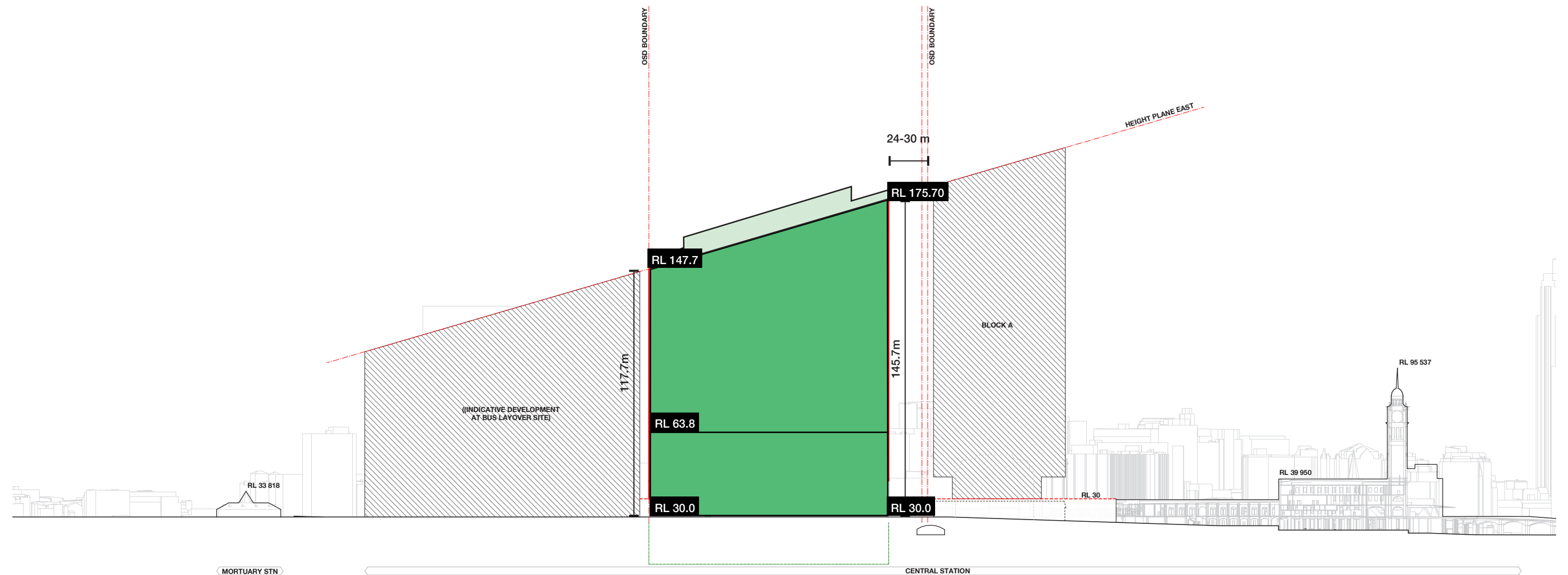


3.2.3.1 Diagram - Elevation West

Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.

3.3 Building Envelope

3.3.1 Building Envelope

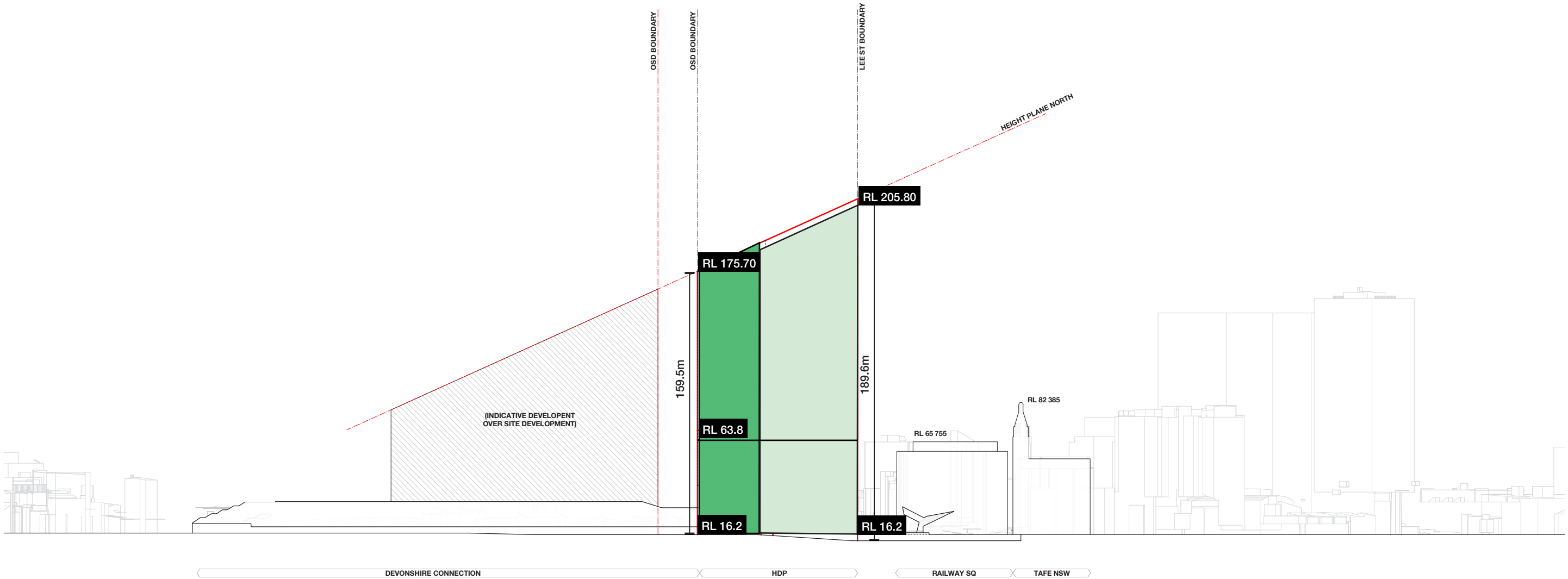


3.2.3.2 Diagram - Elevation East

Refer to Western Gateway Design Guide for minimum east-west building separation requirements and matters to be considered.

3.3 Building Envelope

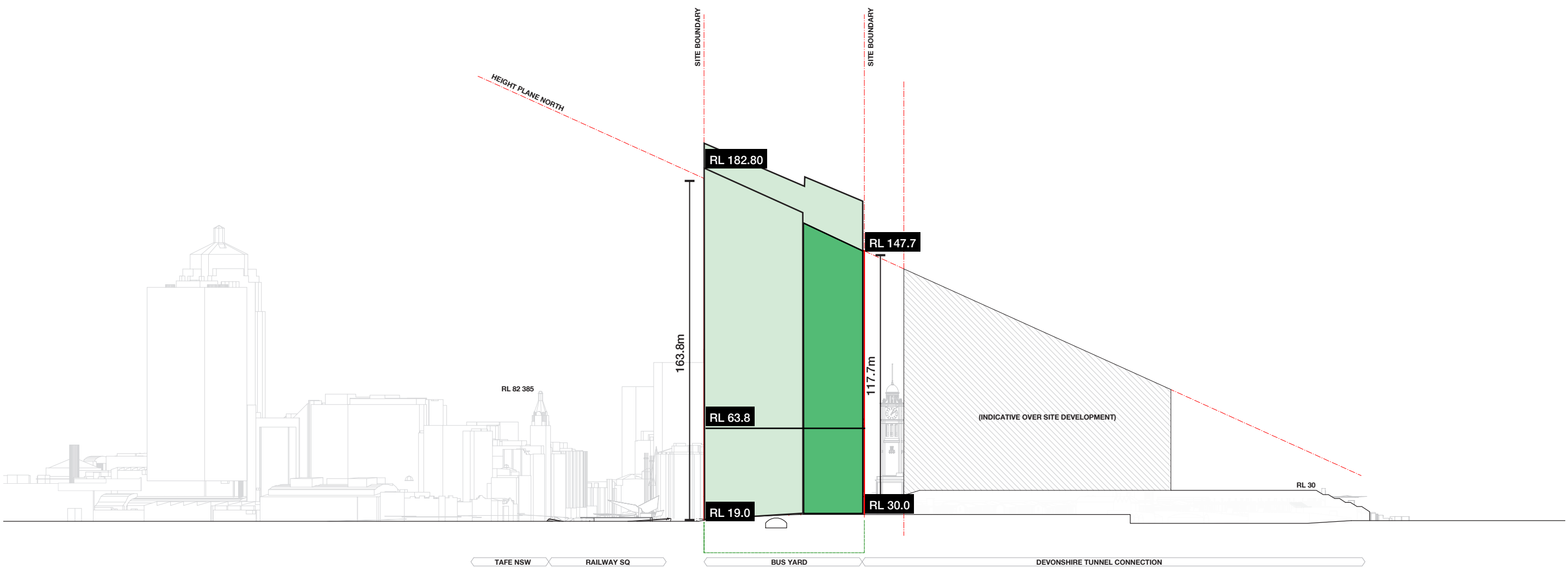
3.3.1 Building Envelope



3.2.3.3 Diagram - Elevation North

3.3 Building Envelope

3.3.1 Building Envelope



3.2.3.4 Diagram - Elevation South

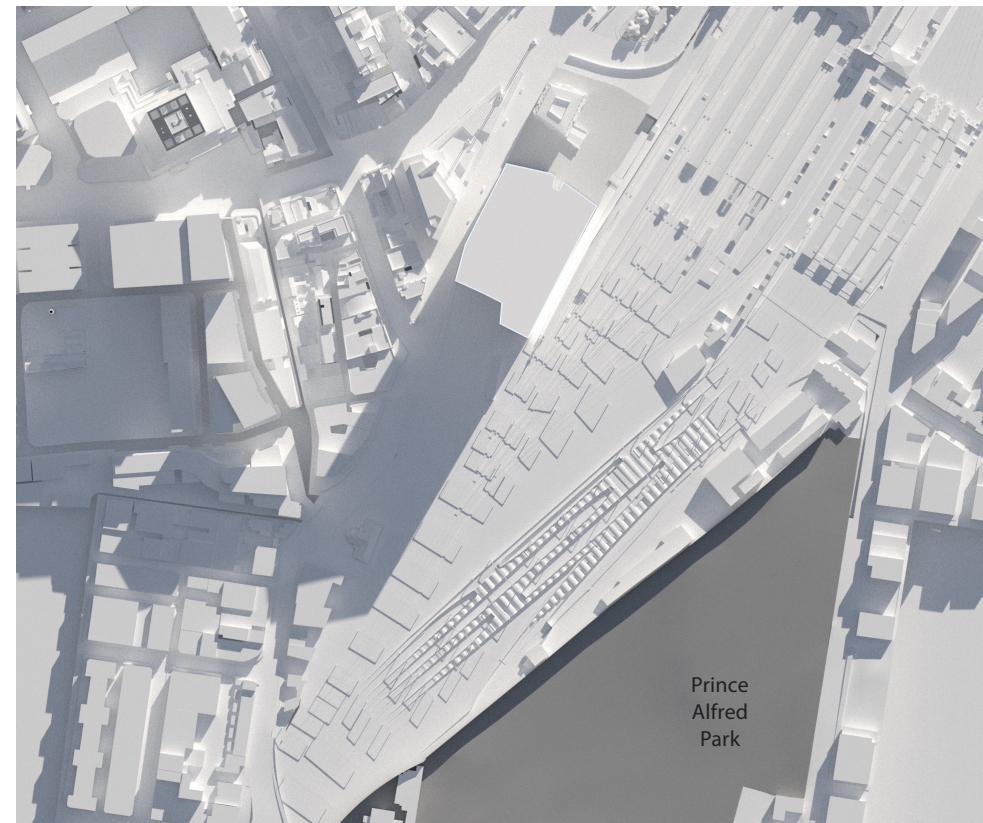
3.3 Building Envelope

3.3.2 Envelope Impact Analysis

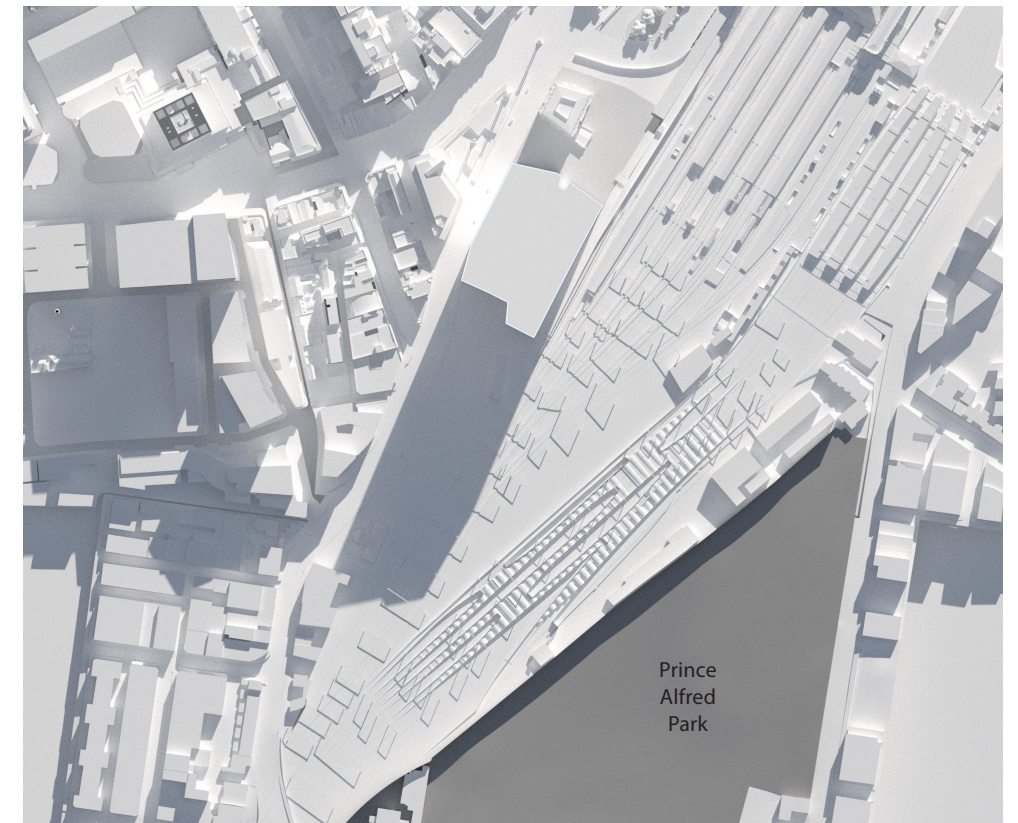
3.3.2.1 Solar Analysis: Winter

Solar Analysis: June 21st (Winter Solstice)

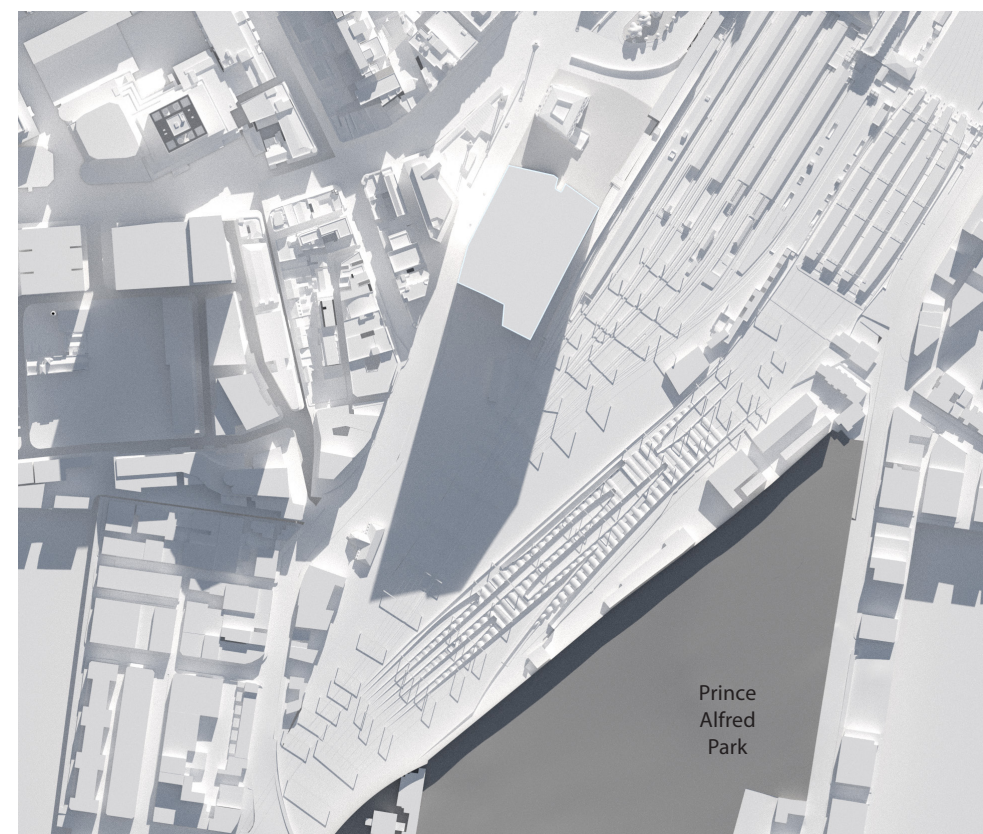
The proposed envelope complies with Building envelope height to comply with Sydney (2012) LEP clause 6.18 and 6.19 (h) and the City of Sydney's Draft Central Sydney Planning Strategy: Solar Access Height Contours Map.



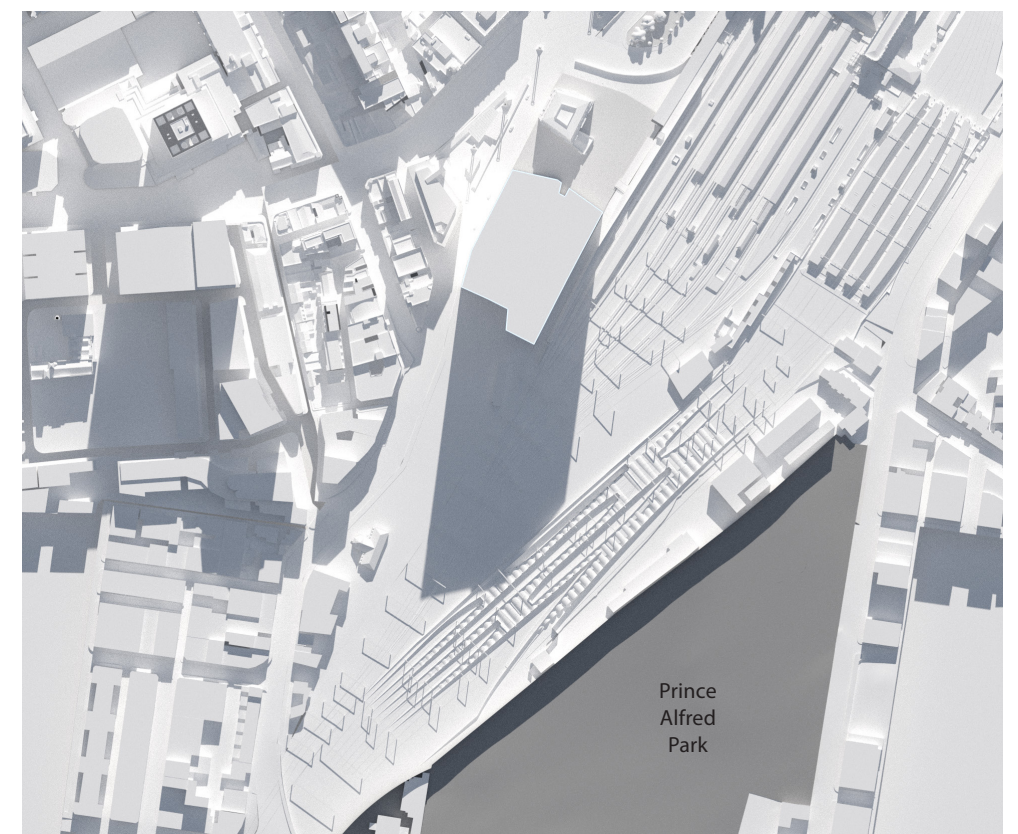
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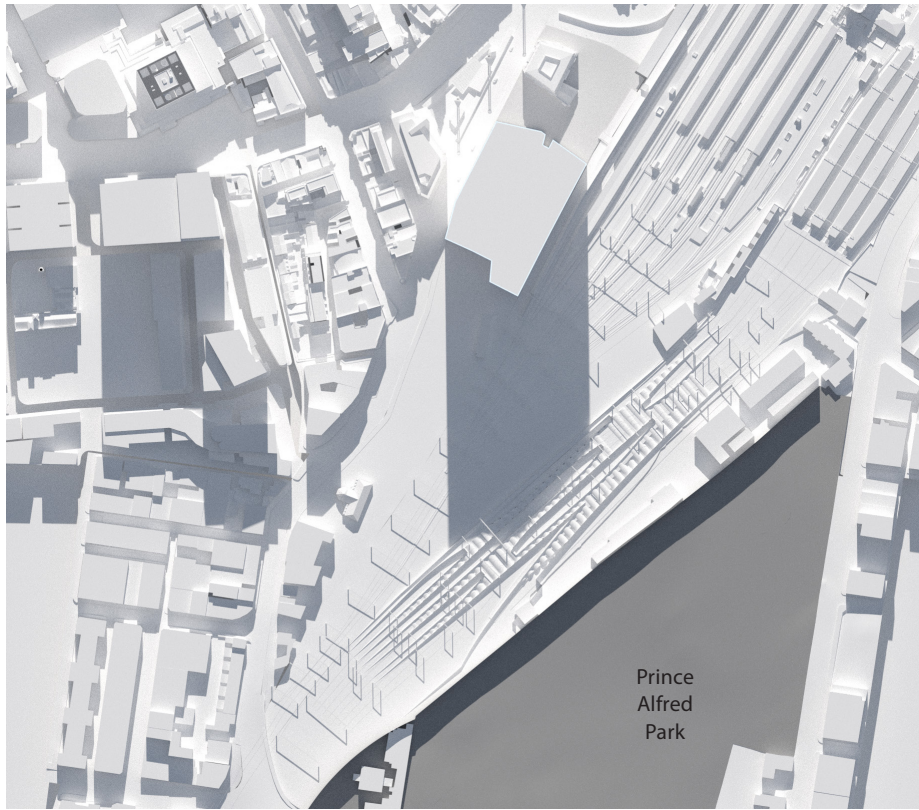
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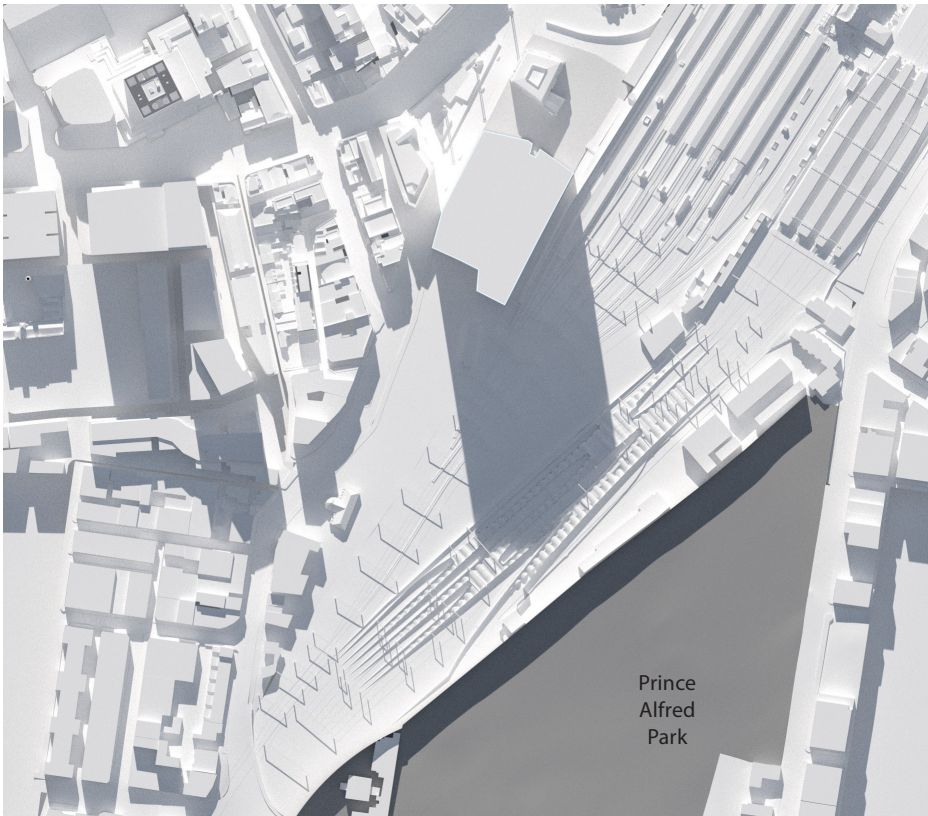
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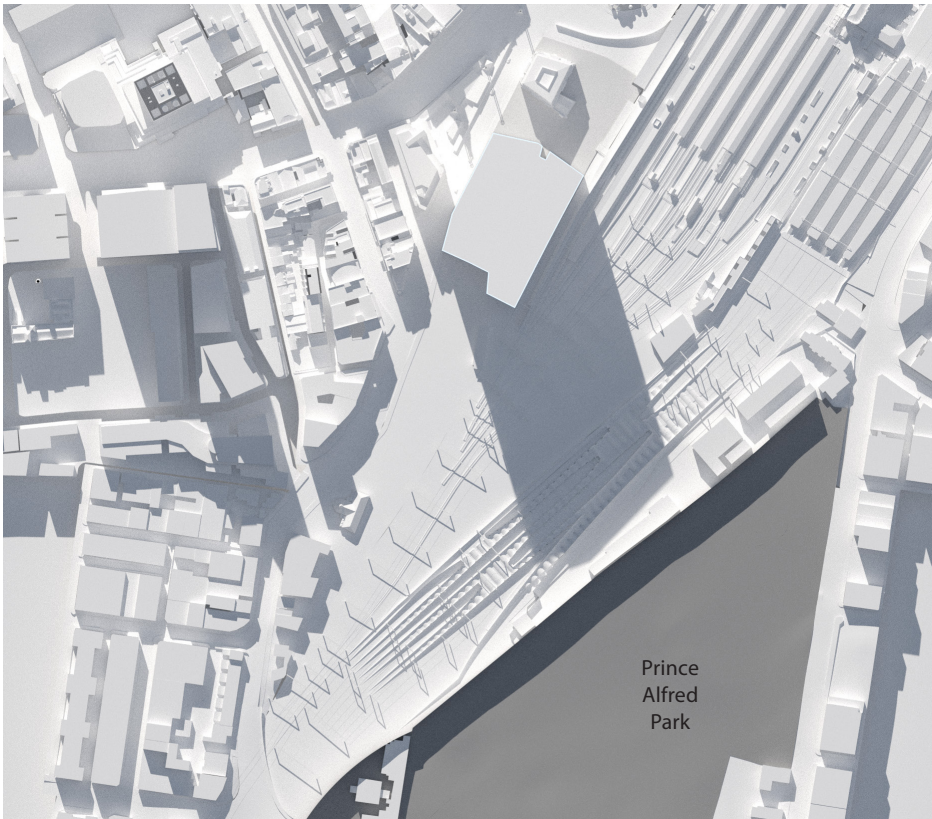
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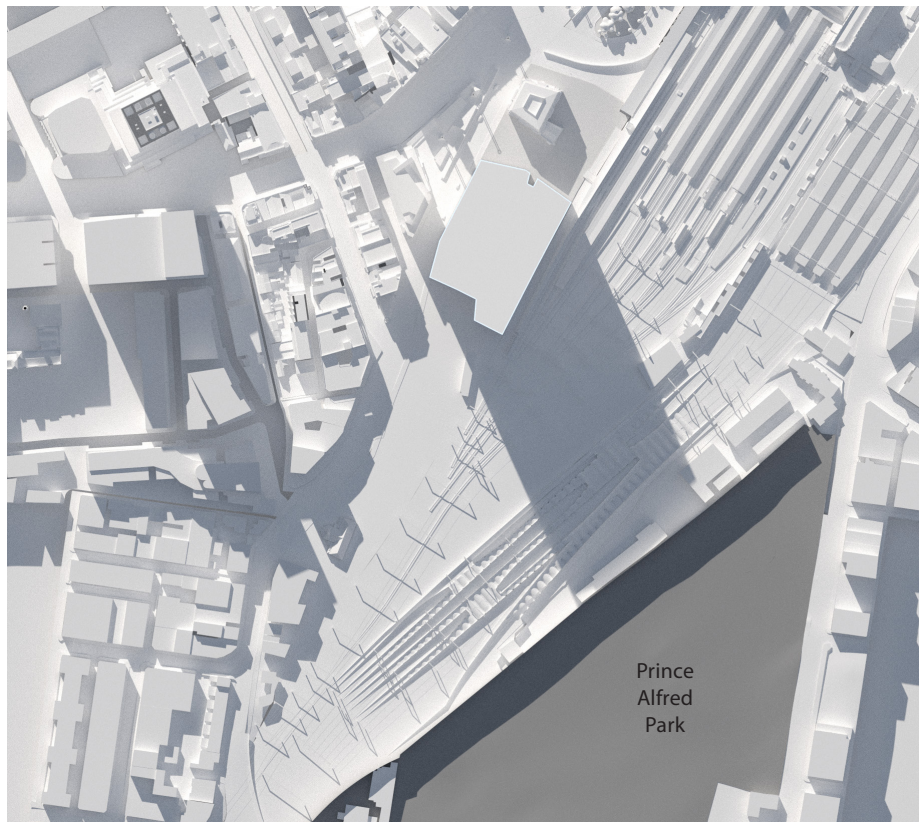
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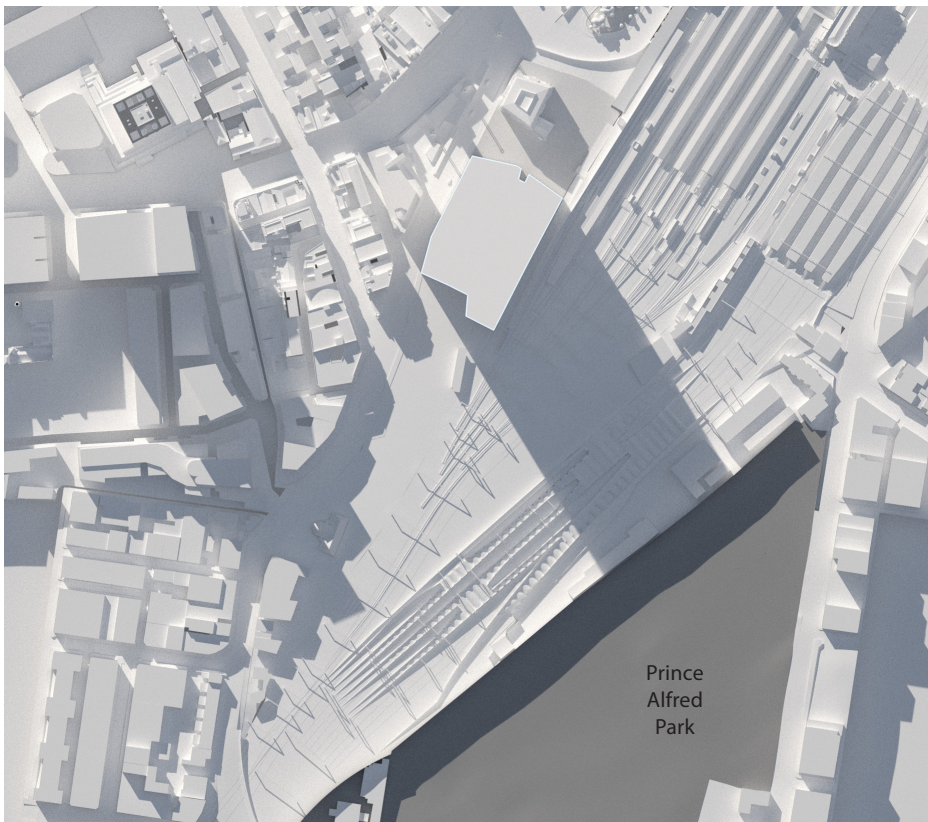
12:30 pm



1:00 pm



1:30 pm



2:00 pm

3.3 Building Envelope

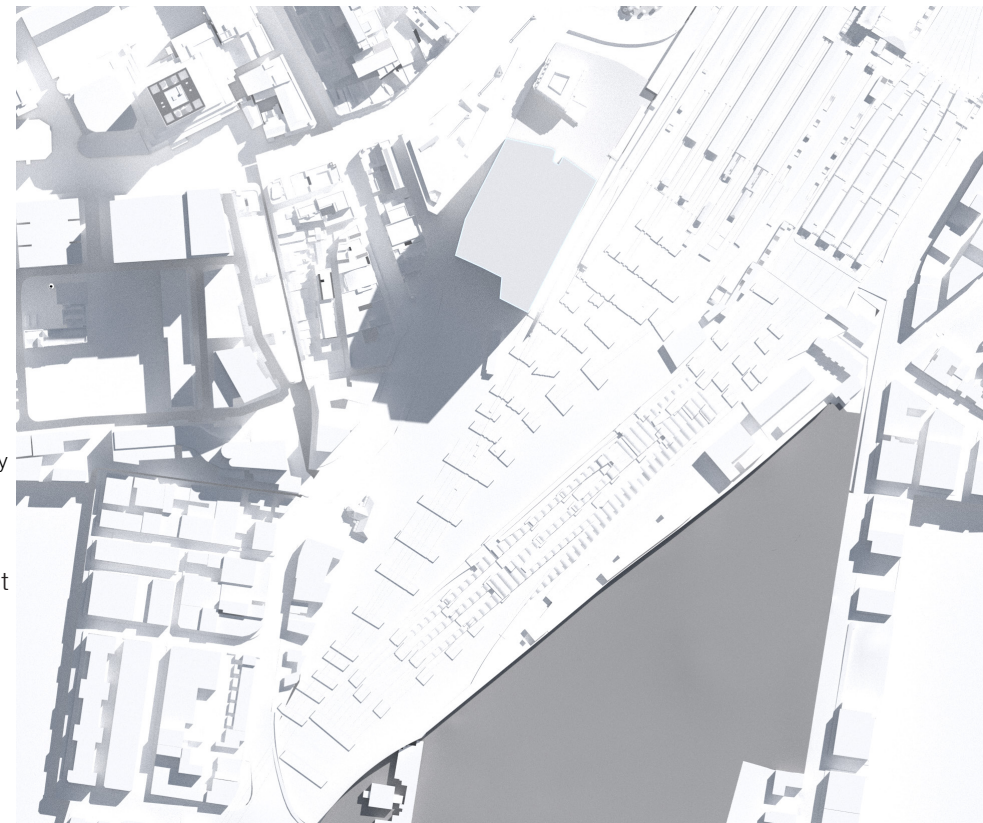
3.3.2 Envelope Impact Analysis

3.3.2.2 Solar Analysis: Spring

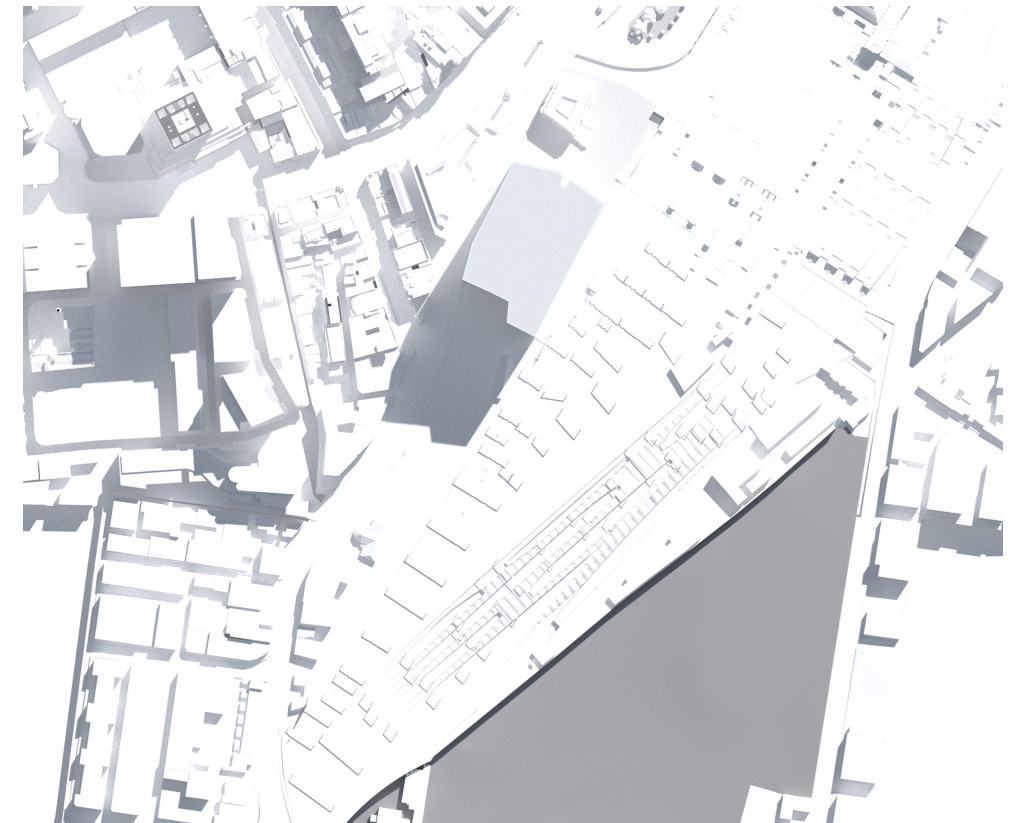
Solar Analysis: September 23rd (Equinox)

The proposed envelope complies with Building envelope height to comply with Sydney (2012) LEP clause 6.18 and 6.19 (h) and the City of Sydney's Draft Central Sydney Planning Strategy: Solar Access Height Contours Map.

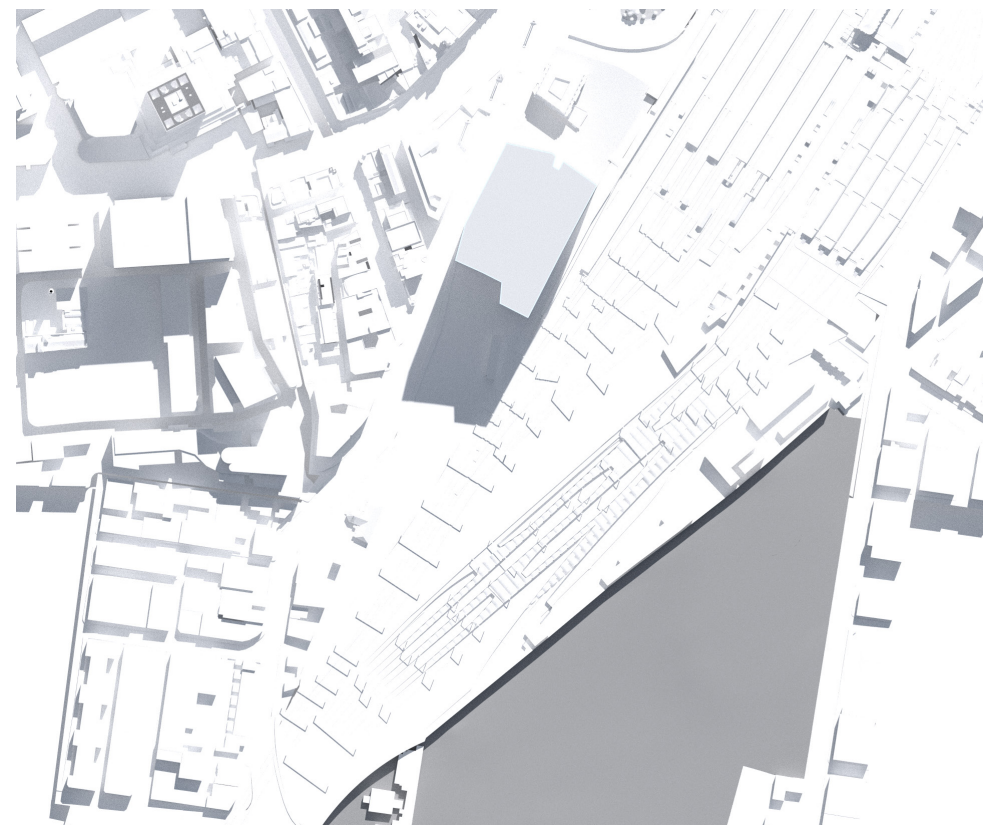
For a large portion of the day the development envelope shadow is a significant distance from Prince Alfred Park.



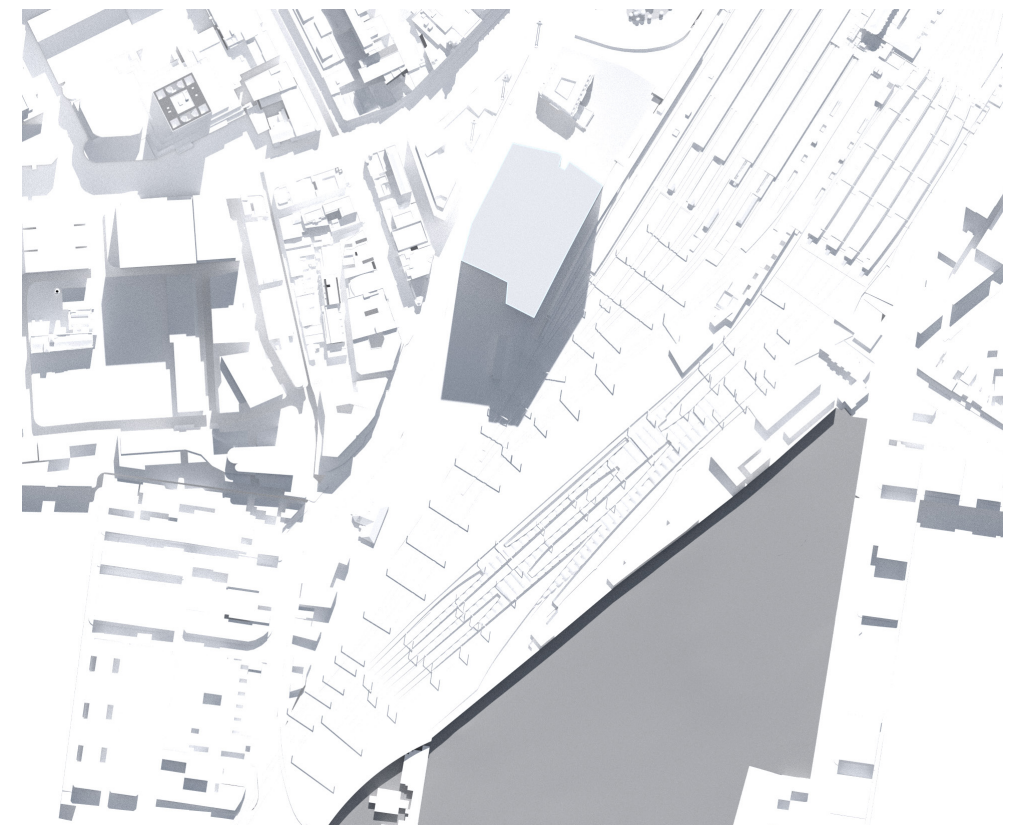
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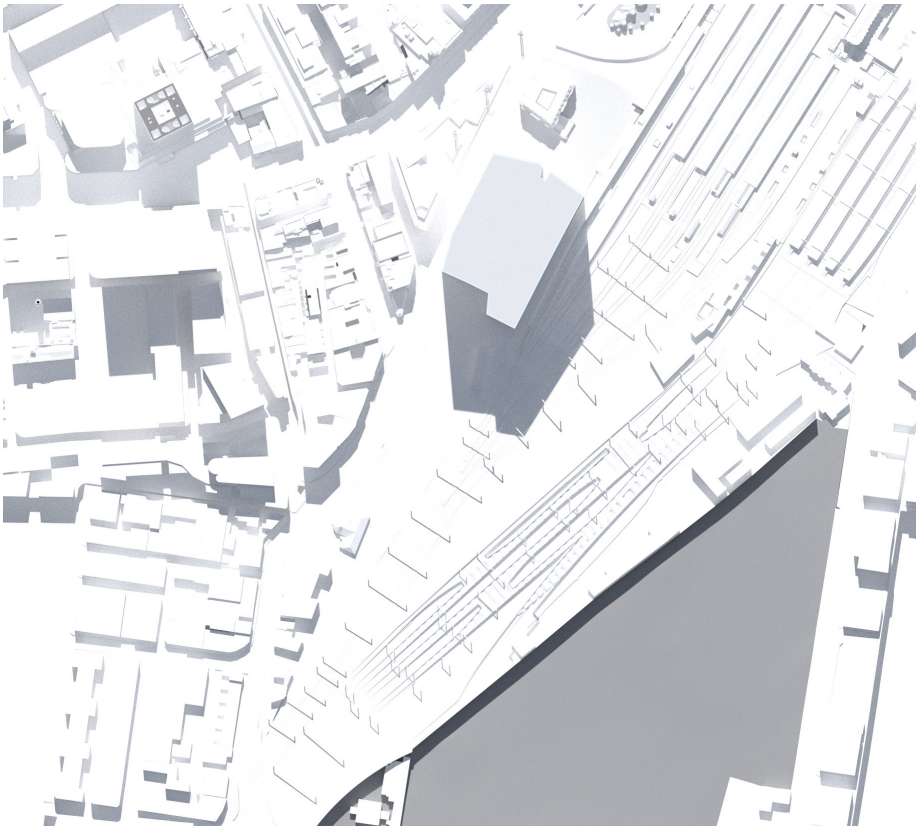
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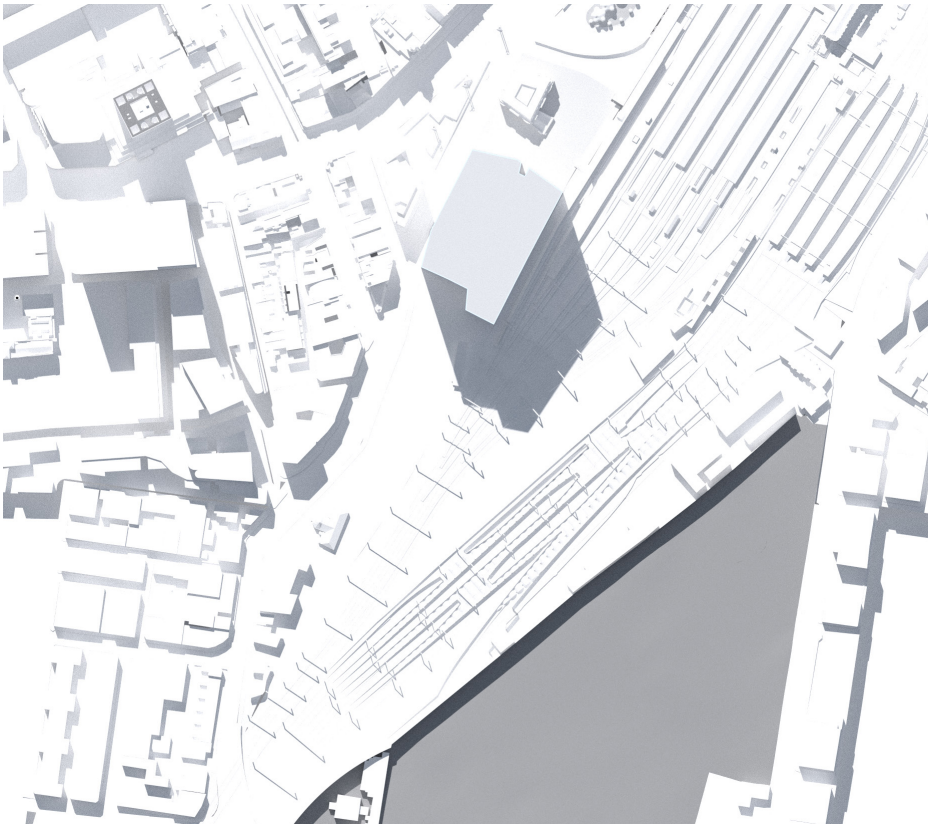
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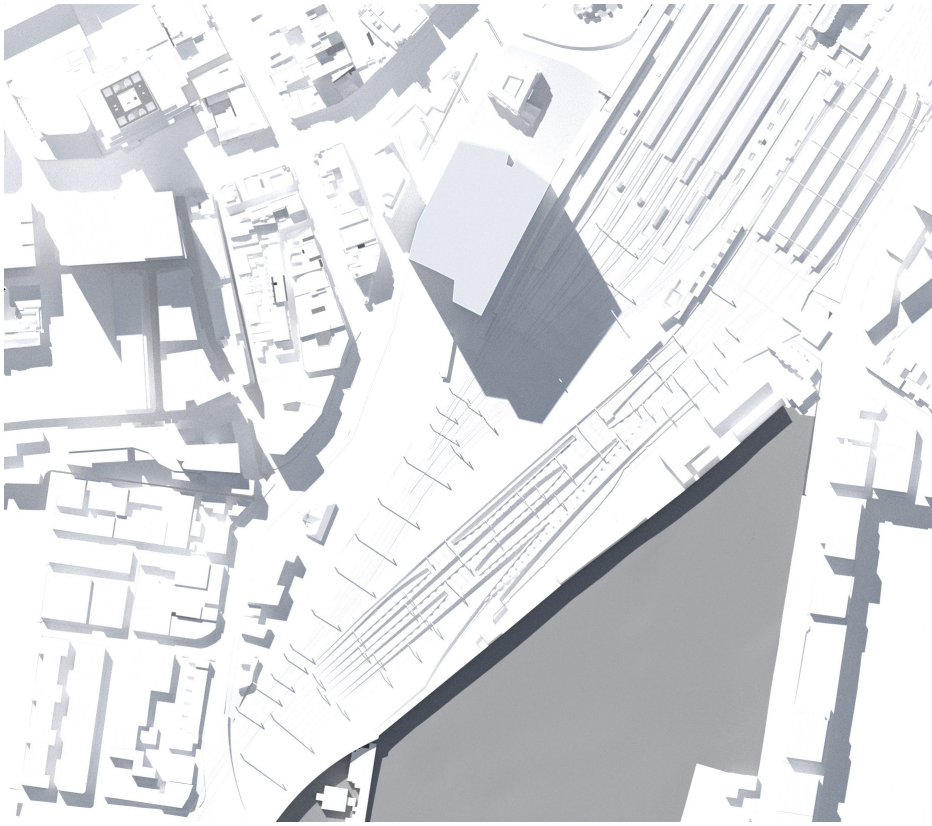
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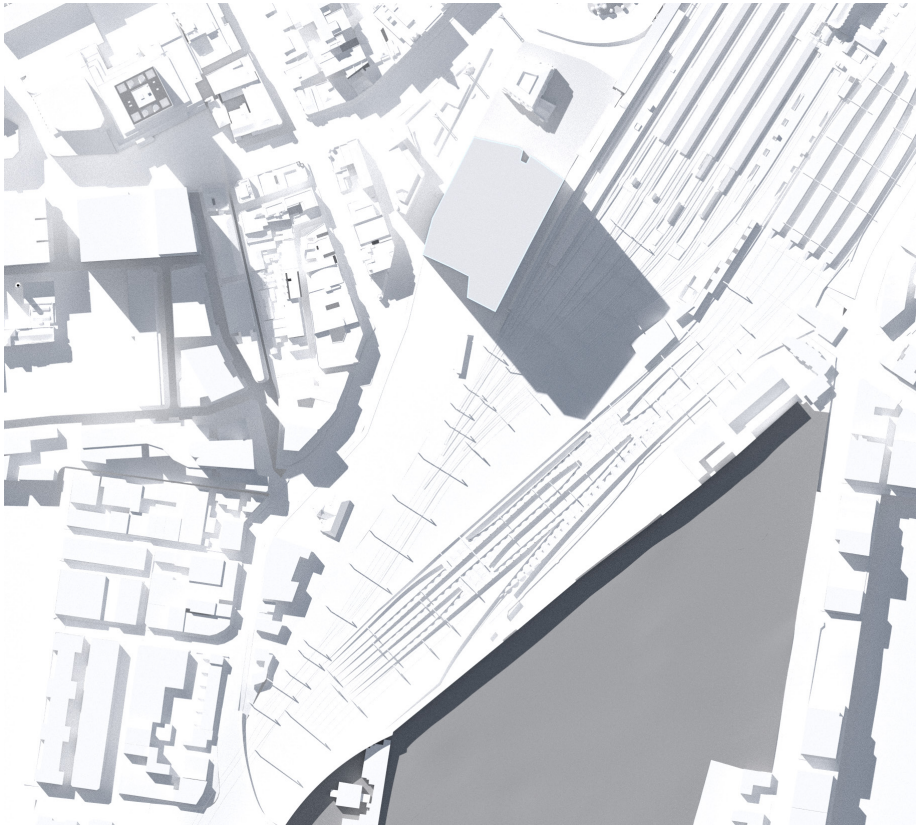
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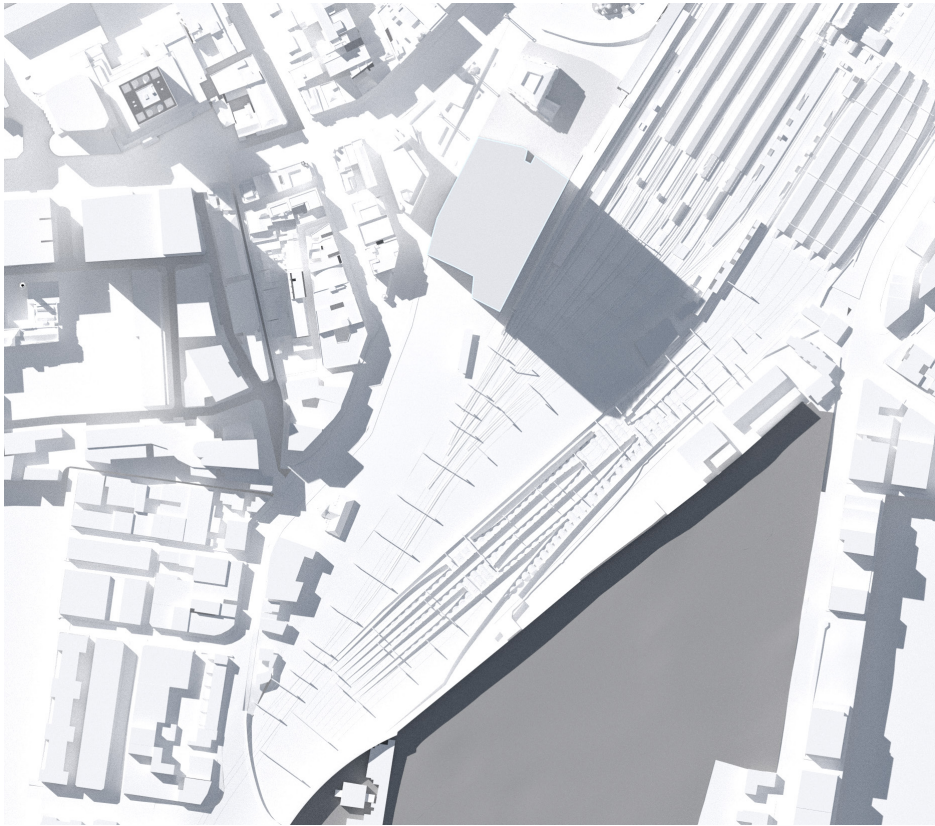
12:30 pm



1:00 pm



1:30 pm



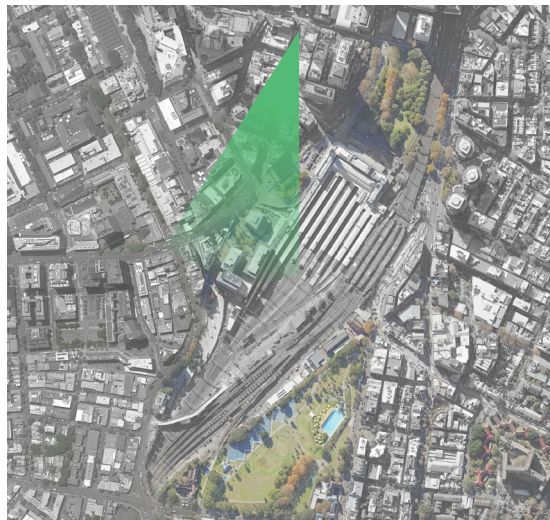
2:00 pm

3.3 Building Envelope

3.3.2 Envelope Impact Analysis

3.3.2.3 George Street View Impact Analysis

The built form shall present as a natural termination to George Street Axis



Key View - George St



3.3 Building Envelope

3.3.2 Envelope Impact Analysis

3.3.2.4 Broadway View Impact Analysis



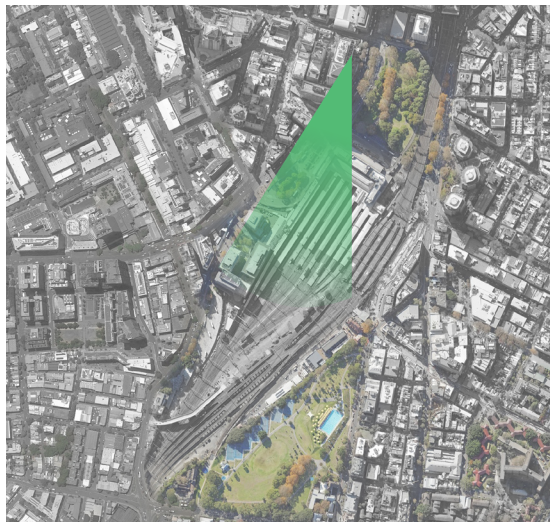
Key View - Broadway



3.3 Building Envelope

3.3.2 Envelope Impact Analysis

3.3.2.5 Pitt Street View Impact Analysis



Key View - Pitt Street



3.3 Building Envelope

3.3.2 Envelope Impact Analysis

3.3.2.6 Wentworth + Goulburn View Impact Analysis



Key View - Wentworth + Goulburn



A

Glossary of terms

Organisations and Stakeholders

Key term	Acronym/ Shortened Term	Description and Conventions
City of Sydney Council	No acronym	Refers directly to the City of Sydney Council. The HDP Site is located on land that is governed by the City of Sydney Council and subject to the City of Sydney LEP 2012. Do not use the City, Council, Sydney Council or other variations.
Department of Planning, Industry and Environment	DPIE	Includes references to the former planning and infrastructure Agency with the Department of Premier and Cabinet, the former Department of Planning and Infrastructure, and the former Department of Planning. Do not use the Department, DPE, Planning and Environment or other variations.
Dexus Funds Management Limited	Dexus	Refers directly to Dexus CPA Pty Limited. When collectively referring to Dexus and Frasers Property, use the Applicant or the Consortium depending on the circumstances.
Frasers Property Australia	Frasers Property	Refers directly to Frasers Property AHL Limited. When collectively referring to Dexus and Frasers Property Australia, use the Applicant or the Consortium depending on the circumstances.
Woods Bagot	No acronym	Refers directly to Woods Bagot. When collectively referring to Skidmore, Owings & Merrill LLP and Woods Bagot Pty Limited, use the Applicant or the Architect depending on the circumstances.
Skidmore, Owings & Merrill	SOM	Refers directly to Skidmore, Owings & Merrill.When collectively referring to Skidmore, Owings & Merrill LLP and Woods Bagot Pty Limited, use the Applicant or the Architect depending on the circumstances.
Arup	No acronym	Refers directly to Arup. When referring to Arup Australia Projects Pty Ltd, use the Applicant or the Transport and Engineering Advisor depending on the circumstances.
Savills	No acronym	Refers directly to Savills. When collectively referring to Savills Project Management Pty Ltd and MG Planning Pty Ltd, use the Applicant or the Planner depending on the circumstances.
MG Planning	No acronym	Refers directly to MG Planning. When collectively referring to Savills Project Management Pty Ltd and MG Planning Pty Ltd, use the Applicant or the Planner depending on the circumstances.
the State	No acronym	Direct reference to the NSW Government as a whole.
Sydney Metro	No acronym;	Refers to Sydney Metro system or agency. Do not use SM, Metro or other variations.
Sydney Trains	No acronym	Refers to Sydney Trains system or agency.
Transport for New South Wales	TfNSW	Refers directly to the Department itself. Do not use Transport, TNSW or other variations.

Project terms

Key term	Acronym/ Shortened Term	Description and Conventions
Block A within the Western Gateway	Block A	Land that includes Part Lot 13,DP1062447; Lot 116, DP1078271A; Lot 117, DP1078271; Part Lot 118, DP1078271 (Airspace) Land located at 8-10 Lee Street, Haymarket. Formerly know as Inwards Parcel Shed
Block A within the Western Gateway	Block B	Land that includes Lot 12, DP1062447; Lot 14, DP1062447; Lot 15, DP1062447; Lot 15, DP1062447 Land located at 14-30 Lee Street, Haymarket.

Block C within the Western Gateway	Block C	Land that includes Part Lot 13, DP1062447; Lot 30, DP877478A Refers to Adina Hotel & Henry Deane Plaza site. Land located at 2 Lee Street, Haymarket. Formerly know as Parcels Post Office.
Applicant, the	No acronym	Used when collectively referring to Dexux and Frasers as the proponents of a rezoning application relating to Block B.
Sydney Light Rail	No acronym.	Official name. Use Sydney Light Rail collectively. When referring to the extension of the Sydney Light Rail use CBD and South East Light Rail extension.
Central Precinct Renewal Project	CPRP	A State Government led program of exploring opportunities to revitalise Central Precinct. The aim of the Central Precinct Renewal Program is to create a precinct with a real sense of place, which connects into surrounding neighbourhoods, and draws on the character and heritage of this landmark location.
Central Station State Significant Precinct	No acronym	The nominate State Significant Precinct comprising an approximately 24 Hectare area including Central Station and surrounding NSW Government owned land along the rail corridor, Goulburn Street Car Park and the strip of land along the Lee Street edge of the Central Precinct, known as the Western Gateway sub-precinct.
Consortium, the	No acronym	Used when collectively referring to Dexux and Frasers as the proponents of a rezoning application relating to Block B.
Draft Central Sydney Planning Strategy	Draft CSPR	Urban planning strategy, prepared by the City of Sydney Council, which supports opportunities for additional height and density balanced with environmental sustainability and urban design excellence. The Draft CSPS proposes amendments to both the Sydney Local Environmental Plan 2012 and Sydney Development Control Plan 2012.
HDP Redevelopment Unsolicited Proposal Stage 2	The USP Stage 2	The <i>Consortium’s</i> proposal to the State Government to redevelop Block B into a large-scale mixed-use development integrating a transport and pedestrian access solution. To be used when referring to the Unsolicited Proposal unless simply the process is being referred to.
Central Walk	No acronym	Refers to the underground paid pedestrian connection, currently under development, that is to be delivered by Sydney Metro City and South West. Once complete, it will be a link between the new station entrance on Chalmers Street, the Eastern Suburbs Railway concourse, suburban platforms 16-23 (via escalators and lifts) and the new Sydney Metro north-south concourse.
Central Walk West	No acronym	Refers to the proposed underground pedestrian connection linking Sydney Metro Central Station to Lee Street. The new pedestrian connection entry proposed to be constructed by TfSNW is located immediately west of Block A.
Draft Design Guide – Western Gateway Sub-precinct	No acronym	A document that sets out specific guidelines to inform and guide future development within the Western Gateway sub-precinct. It comprises a hierarchy of objectives and design guidelines for which future development proposals must consider and demonstrate consistency with.
Indicative Scheme	No acronym	An indicative design has been developed to demonstrate how the site may be developed under the proposed Planning Statement seeking to rezone the land. The Indicative Scheme illustrates how future development may be pursued within Block B and in accordance with the proposed development controls. Future development applications will determine the final form and design of buildings and public spaces
Over Station Development	OSD	The development of airspace over railway or metro stations and corridors.
Planning Statement	No acronym	The application, prepared by <i>Applicant</i> , to amend the local planning controls. The <i>Planning Statement</i> is a suite of documents describing and assessing the proposed amendments to the City of Sydney LEP 2012. The <i>Planning Statement</i> has been prepared for Block B.
Site, the	Site	Land located at 14-30 Lee St, Haymarket. Legally described as Lots 12, 14 and 15 in DP 1062447 (Lot 14 includes the small upper level of Henry Deane Plaza). Forming Block B within the Western Gateway Sub Precinct of the Central Precinct. Currently occupied by three commercial buildings known as SRA house and Henry Deane Place (including Gateway House).
State Significant Development Development Approval	SSD DA	Official name. The <i>SSD DA</i> has been prepared for the Site and seeks development consent.
State Significant Precinct	SSP	The Department of Planning, Industry and Environment has identified that the Central Precinct should be a State Significant Precinct, this includes the Western Gateway as a sub-precinct.
Sub-precincts	No acronym	Definable areas within the Central Precinct SSP due to its unique local character, opportunities and constraints, either current or future. The Western Gateway is a sub-precinct.
Sydney Innovation and Technology Precinct	No acronym	Official name. A State Government Initiative as set out in “The Sydney Innovation and Technology Precinct Panel Report 2018’ The Sydney Innovation and Technology Precinct is located south of the Sydney central business district, surrounded by the suburbs of Redfern, Ultimo, Haymarket, Camperdown, Chippendale, Darlington, Surry Hills and Eveleigh. The NSW Government’s vision for the Precinct is a place where ambitious startups, world class universities and research institutions, high-tech giants and the community collaborate to solve problems, socialise and spark ideas that change the world and support the jobs of the future.

Unsolicited Proposal	USP	Official name. An Unsolicited Proposal is an approach to government from a proponent with a proposal to deal directly with the government over a commercial proposition, where the government has not requested the proposal.
Urban Design Report	No acronym	The Urban Design Report (Woods Bagot + SOM, 2019) prepared to support the <i>Planning Statement</i> .
Urban Design Principles	No acronym	As set out in the Urban Design Report.
Western Forecourt	No acronym	Also known as the Central Station Western Forecourt, The official name given to the area immediately west of the Devonshire Street Tunnel exit and East of Lee Street.
Western Gateway	No acronym	The land along the Lee Street edge of the Central Precinct and that is a sub-precinct of the Central Precinct State Significant Precinct. The Western Gateway includes existing commercial office space and retail around Henry Dean Plaza, YHA Railway Square, Adina Hotel as well as public domain for both lower and upper stratum lots.

Buildings and streets

Key term	Acronym	Description and Conventions
Bus Terminus aka Lee St Layover	No acronym	Official name
Central Metro Station	No acronym	Sydney Metro Central Station
Central Station	No acronym	Central Railway Station
Devonshire Street Tunnel	No acronym	Official name. The covered pedestrian tunnel connecting Chalmers Street and Lee Street.
Goods Line	No acronym	Official name. The Goods Line is a partly elevated urban walkway from Central Station to Darling Harbour that follows the route of a disused railway line.
Mortuary Station	No acronym	Officially named the Regent Street railway station, and formerly known as Mortuary railway station, was previously a railway station on the Rookwood Cemetery railway line.
Railway Square	No acronym	Official name. Major bus stop and below ground uses located between Lee Street and Broadway.
Western Wing Extension of Central Station	No acronym	Official name